



Community Development Department
PO Box 2460
16345 Sixth Street
La Pine, Oregon 97739
Phone: (541) 536-1432, Fax: (541) 536-1462
Email: info@ci.la-pine.or.us

Site Plan Application

File Number # SPR3-25-0003

Fee: Less than 1,000 sq ft	Fee: \$ 1,100.00
Fee: 1,001 to 5,000 sq ft	Fee: \$ 2,000.00
*Fee: 5,001 to 10,000 sq ft	Fee: \$ 3,500.00
Fee: More than 10,000 sq ft	Fee: \$ 4,000.00

PROPERTY OWNER AND APPLICANT INFORMATION

Applicant Name Jesse Bath Phone 541-357-1127 Fax _____
Address 2660 NE Hwy 20 Ste 610-2 City Bend State OR Zip Code 97701
Email Jcbathproperties@gmail
Property Owner BP Holdings Phone 541-357-1127 Fax _____
Address 2660 NE Hwy 20 Ste 610-2 City Bend State OR Zip Code 97701
Email Same

PROPERTY DESCRIPTION

Property Location (address, intersection of cross street, general area) _____
16630 Box way, LaPine, in the industrial Park
Tax lot number: T-15 R-13 Section 14 Tax Lot(s) 221014AB00132
Zoning I Total Land Area 18,769 (Square Ft.) 0.43 (Acres)
Present Land Use None

Describe Project (i.e. type of use, hours of operation, other project characteristics):

Multi Suite Industrial Building. Suited for small business's

PROJECT DESCRIPTION

Please give a brief description of the project:

6,400 Sqft Industrial Building with 4 1,600 Sqft units



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- ☐ Tree and plant sizes (new only).
- ☐ All trees having a six-inch trunk diameter 3' above grade or greater shall be shown on the landscape plan.
- ☐ Location/placement of existing and proposed vegetation to be retained, planted or removed.
- ☐ Approximate location of irrigation lines, and type of irrigation system to be used.

FLOOR PLAN

- ☐ All significant rooms within each structure; label or number rooms, including square footage for each room.
- ☐ Electrical / mechanical equipment areas.

LIGHTING PLAN

- ☐ All exterior light locations.
- ☐ Brochure, illustration, cut sheet or photo for each light fixture type to be used.

By signing this application, the undersigned certifies that he / she has read and understands the submittal requirements stated above. Note: if the applicant makes a misstatement of fact on the application regarding ownership, authority to submit the application, acreage, or any other fact material relied upon in making a decision, the City may upon notice to the applicant and subject to an applicant's right to a hearing declare the application void.

Owner: _____ Date: 7-23-25

Signature

Applicant: _____ Date: 7-23-25

Signature



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PROFESSIONAL SERVICES

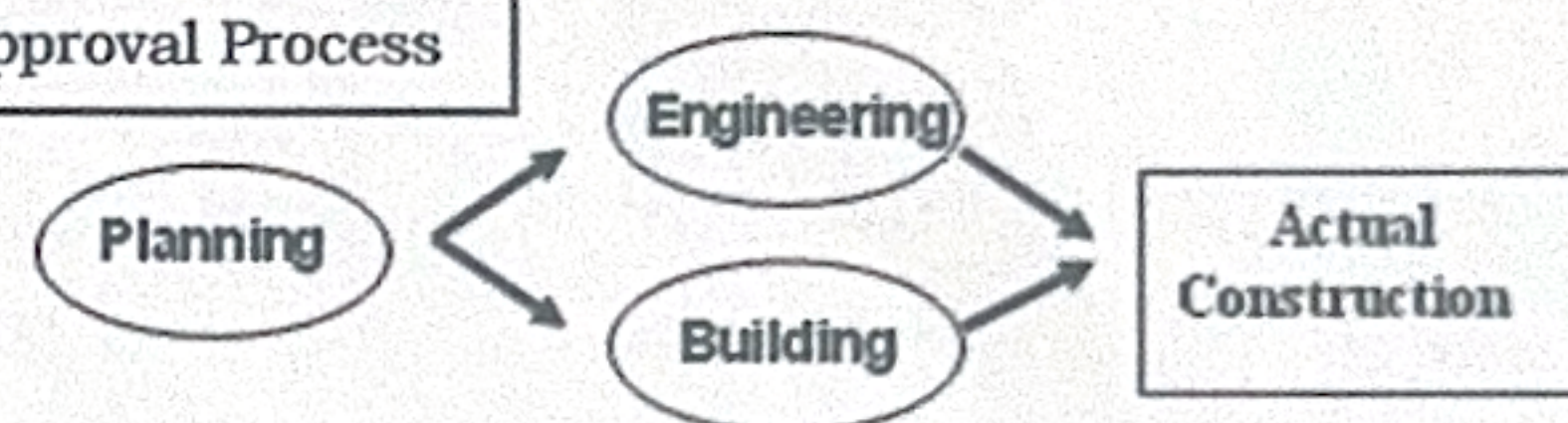
Architect/Designer/Engineer LB engineering Phone 541/317-2934 Fax 1
Address 1902 NE 4th St City Bend State OR Zip Code 97701
Email John Scott@lbeng.com

FOR OFFICE USE ONLY

Date Received: _____

Rec'd By: _____

Approval Process



CHECKLIST

REQUIRED ITEMS TO BE SUBMITTED FOR SITE AND DESIGN REVIEW.

Note: additional information may be required depending on the actual project.

- ☐ Complete Application. The application *must be signed by the property owner and the applicant.*
- ☐ Burden of proof statement, three (3) copies addressing approval criteria.
- ☐ Title Report or Subdivision Guarantee verifying ownership, *including legal description of land.*
- ☐ Fee Schedule (please see attached).
- ☐ Site and Landscape plan; Building Elevations; seven (7) full sized copies of each which must be folded individually, or in sets to 8 ½" X 11" in size and 1 reduced (8 ½ by 11 or 11 by 17) copy.
- ☐ Floor plans, three copies for each building which must be folded individually, or in sets to 8 ½" X 11" in size, plus 1 reduced (8 ½ by 11 or 11 by 17). Include the class of construction.
- ☐ Vicinity map.
- ☐ Trip Generation statement prepared by a professional transportation planner or equivalent. 5 copies, Note: if more than 200 ADT result (or at the discretion of the City Engineer), a Traffic Impact Study may be required.
- ☐ Preliminary Grading and Storm Drainage Plan. 3 copies (11" x 17")
- ☐ Fire Flow Analysis
- ☐ CD or electronic version of submittal items (Word, Jpeg or PDF)



EXIST. LOT LOOKING EAST



EXIST. LOT LOOKING WEST

Project Data

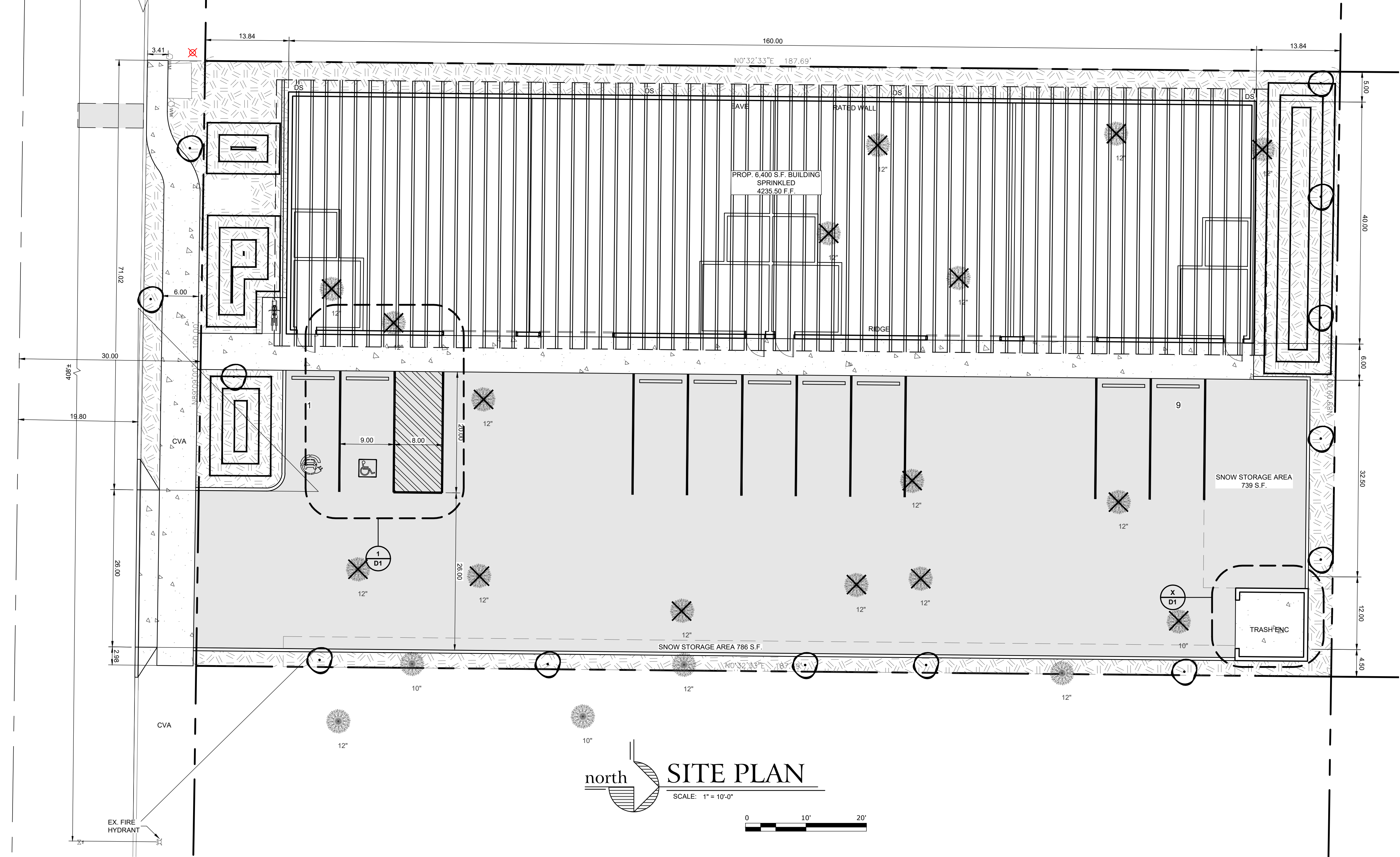
PROJECT NAME:	BATH PROPERTIES
PROJECT ADDRESS:	16630 BOX WAY LAPINE, OR
JURISDICTION:	CITY OF LA PINE
LEGAL DESCRIPTION:	LOT 14, NEWBERRY BUSINESS PARK
LOT AREA:	18,769 S.F.
CURRENT ZONING:	I - INDUSTRIAL
CONSTRUCTION:	TYPE V-B, SPRINKLED
TOTAL BUILDING AREA:	6,400 SQ. FT.
PARKING PROVIDED:	1 / 1,000 = 6 SPACES
PARKING PROVIDED:	9 SPACES INCLUDING 1 HDOP SPACE
LANDSCAPING REQUIRED:	18,769 x 0.10 = 1,877 S.F.
LANDSCAPING PROVIDED:	3,307 S.F.
SNOW REMOVAL AREA REQUIRED:	8,980 x 0.15 = 1,347 S.F.
SNOW REMOVAL AREA PROVIDED:	1,526 S.F.

LEGEND

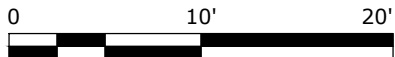
---	EXIST. PROPERTY LINE
---	EXIST. SEWER LINE
---	EXIST. WATER LINE
---	NEW SEWER LINE
---	NEW WATER LINE
---	NEW STORMLINE
[Pattern]	PROP. ASPHALT
[Pattern]	PROP. CONCRETE
[Pattern]	PROP. LANDSCAPE AREAS
[Symbol]	HANDICAP PARKING SPACE
[Symbol]	FUTURE EV CHARGING SPACES (20%)
[Symbol]	PROP. LANDSCAPE TREE
[Symbol]	TREES TO BE REMOVED

BOX WAY

HINKLE WAY



north
SITE PLAN
SCALE: 1" = 10'-0"



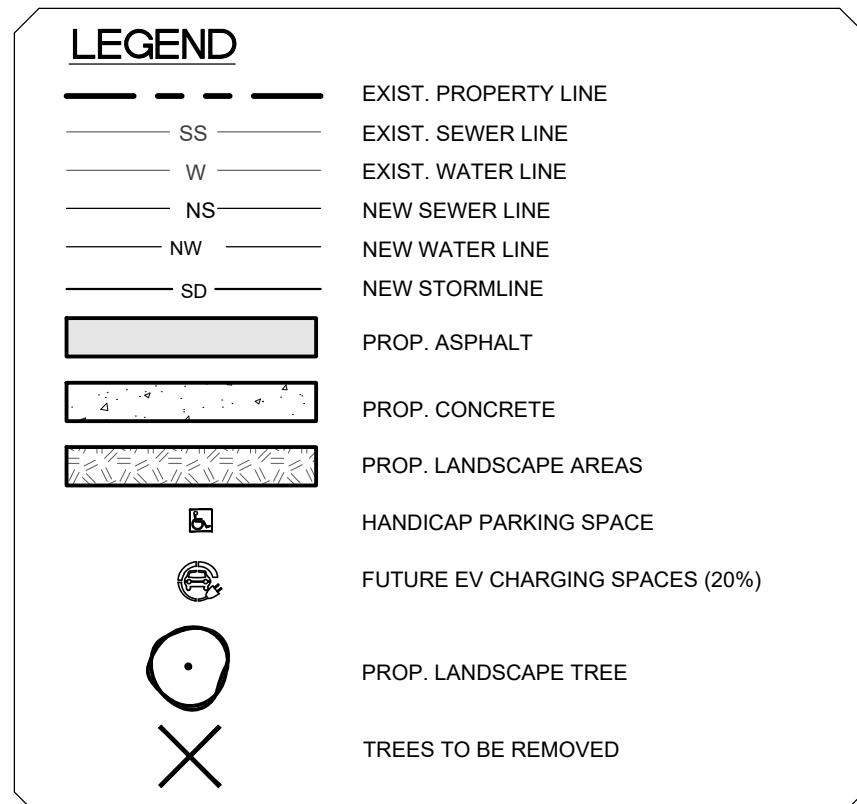
LOT 14, NEWBERRY BUSINESS PARK

16630 BOX WAY
LAPINE OREGON

SITE PLAN

DRAWN BY:	
ISSUE DATE:	
MARK	REVISIONS

C1



DESIGNED FOR THE 25yr 24hr STORM EVENT 2.6"/HR w/ 2.0 INFILTRATION:

DRAINAGE BASIN #1:
 INFLOW AREA: 7,787 S.F., 1,275 C.F. (9,537 GAL.)
 INFILTRATION: 0.2 CFS = 763 CF (5,707 GAL.)
 REQUIRED STORAGE VOLUME: 512 CF (3,829 GAL.)

SWALE:
 PROVIDED STORAGE: 548 C.F. (4,099 GAL.)

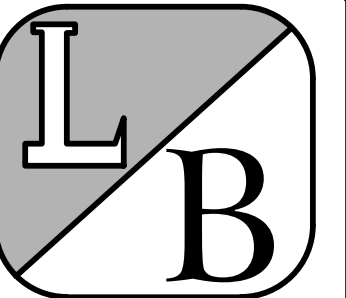
DRAINAGE BASIN #2:
 INFLOW AREA: 7,762 S.F., 1,271 C.F. (9,507 GAL.)
 INFILTRATION: 0.02 CFS = 755 CF (5,647 GAL.)
 REQUIRED STORAGE VOLUME: 516 CF (3,859 GAL.)

SWALE #2:
 PROVIDED STORAGE: 806 C.F. (6,028 GAL.)

TESTING:

SWALE: 548 C.F. (4,099 GAL.)
 SWALE #2: 806 C.F. (6,028 GAL.)

STRUCTURAL PROFESSIONAL
REGISTERED ENGINEER
17035
OREGON
MARK E. BRADY
EXPIRATION DATE: 12/31/2025



ENGINEERING, INC.
1902 NE 4TH ST
BEND, OREGON 97701
(541) 317-2939 FAX. 317-2940

LOT 14, NEWBERRY BUSINESS PARK

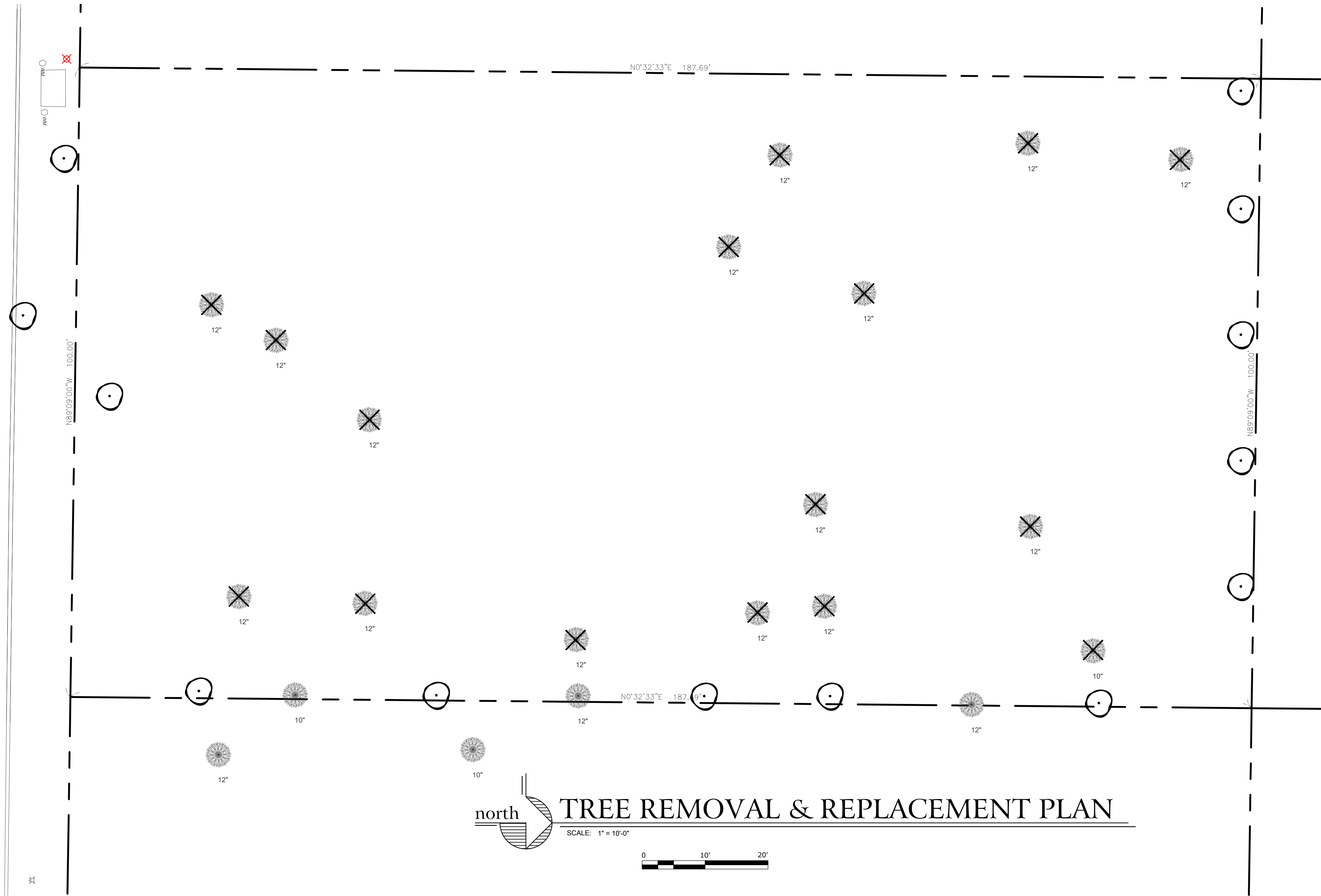
16630 BOX WAY
LAPINE OREGON

GRADING AND UTILITY PLAN

DRAWN BY	
ISSUE DATE	
MARK	REVISIONS

C2

BOX WAY



LEGEND

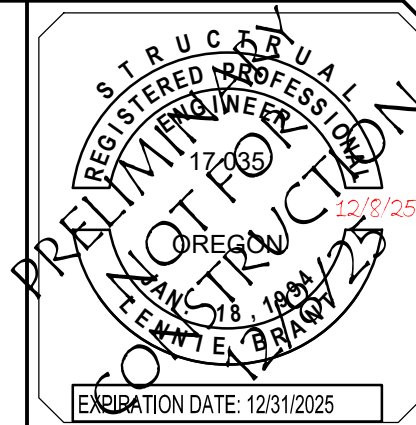
- EXIST. PROPERTY LINE
- PROP. LANDSCAPE TREE
- TREES TO BE REMOVED

north

TREE REMOVAL & REPLACEMENT PLAN

SCALE: 1" = 10'-0"

0 10' 20'



L B

ENGINEERING, INC.

1802 NE 4TH ST
BEND, OREGON 97701
(541) 317-2939 FAX. 317-2940

LOT 14, NEWBERRY BUSINESS PARK

16630 BOX WAY

LAPINE OREGON

TREE REMOVAL
AND REPLACEMENT PLAN

DRAWN BY	
ISSUE DATE	
MARK	REVISIONS

C3

101-24

Landscape Narrative & Planting Schedule

Project: 16630 Box Way

Lot: 14 – Newberry Business Park

Reference: SPR3-25-0003 – Landscape Compliance

Date: December 2025

1. Purpose

This Landscape Narrative and Planting Schedule are submitted in response to the City of La Pine's request for additional landscaping information as part of the Site Plan Review completeness check. The intent of this document is to demonstrate compliance with the City's tree replacement standards, street frontage landscape requirements, and general planting expectations. All proposed plantings correspond to the attached Landscape Plan and Site Plan.

2. Existing Trees & Tree Removal

The site contains several existing lodgepole pine trees as shown on the Landscape Plan legend. Trees identified with an "X" are **proposed for removal** due to conflicts with building footprint, driveway, and parking areas.

Consistent with La Pine Development Code Section 8.0300, all qualifying removed trees will be replaced at a **1:1 ratio**. Replacement tree locations are shown on the Landscape Plan and sized to meet minimum caliper requirements.

3. Street Frontage Landscaping (Box Way)

The Box Way frontage includes a landscaped strip and swale area as depicted on the engineer's plans. The City requires street trees spaced at approximately 30 feet on center. Based on the site's available frontage and existing utilities, **three (3) street trees** are proposed, evenly spaced along Box Way.

Street Trees

- **Acer rubrum 'Franksred'** (Frank's Red Maple)
- **Quantity:** 3
- **Size:** Minimum **1.5" caliper**
- **Spacing:** Approx. **30 feet on center**
- **Installation:** Stake each tree with **two 2" lodgepole pine stakes**, placed outside the rootball.

Groundcover

- **Festuca idahoensis** (Idaho Fescue)
- **1-gallon containers**, triangular spacing at **42" on center**

This combination provides compliant street frontage treatment while maintaining sight distance and swale function.

4. Landscape Swales

Landscape swales are located within the south and southeast portions of the site. Plantings are selected to withstand intermittent moisture while remaining drought-tolerant.

Swale Plantings

- **Prunus virginiana 'Canada Red'** (Canada Red Chokecherry), **1.5" caliper**
- **Festuca idahoensis** (Idaho Fescue), **1-gallon**, triangular spacing at **42" on center**

These plantings provide stabilization, visual consistency, and low-maintenance vegetation appropriate for drainage facilities.

5. Replacement Tree Plantings

Replacement trees required for removed onsite trees are included on the Landscape Plan as "Proposed Landscape Tree."

Tree species and placement align with site constraints and La Pine's preference for native or cold-tolerant species.

Replacement Tree Requirements

- **Quantity:** 1 replacement per removed qualified tree
 - **Minimum Size:** 1.5" caliper
 - **Species:**
 - *Acer rubrum 'Franksred'* (Frank's Red Maple), or
 - A suitable native conifer (if required based on final field conditions)
-

6. Planting Installation Standards

(All plantings installed per industry best practices.)

- Excavate pits to **twice the rootball diameter**; scarify sides of excavation.
 - Set plants centered and plumb in the hole.
 - Backfill using a **50/50 blend** of compost and native soil.
 - Construct a shallow berm around each tree to retain water during establishment.
 - Thoroughly soak at the time of planting.
 - Apply **time-release fertilizer** and **mycorrhizal inoculum** to promote root development.
-

7. Irrigation & Maintenance

All plant materials will be supported by a temporary **drip irrigation system** during their establishment period. Landscape areas will be maintained free of weeds and debris, with any dead or diseased plant materials replaced at the next planting season.

8. Compliance Statement

This planting plan meets all applicable requirements for tree replacement, street frontage landscaping, and landscape coverage as outlined in the City of La Pine Development Code. All plantings have been selected for durability, low water demand, and compatibility with local climate and soil conditions.



ENGINEERING, INC.

Site Development Narrative

PROJECT NAME: Lot 14, Newberry Business Park
OWNER: BP Holdings LLC
21070 Scottsdale Dr
Bend, OR 97701
PROJECT ADDRESS: 16330 Box Way. La Pine OR 97739
ZONING: I - Industrial

PROJECT DESCRIPTION: The applicant is requesting site plan and design review approval to construct a 6,400 s.f. (footprint), sprinkled building, that will be used for tenant lease spaces.

Applicable Procedure, Standards, and Criteria:

La Pine Development Code

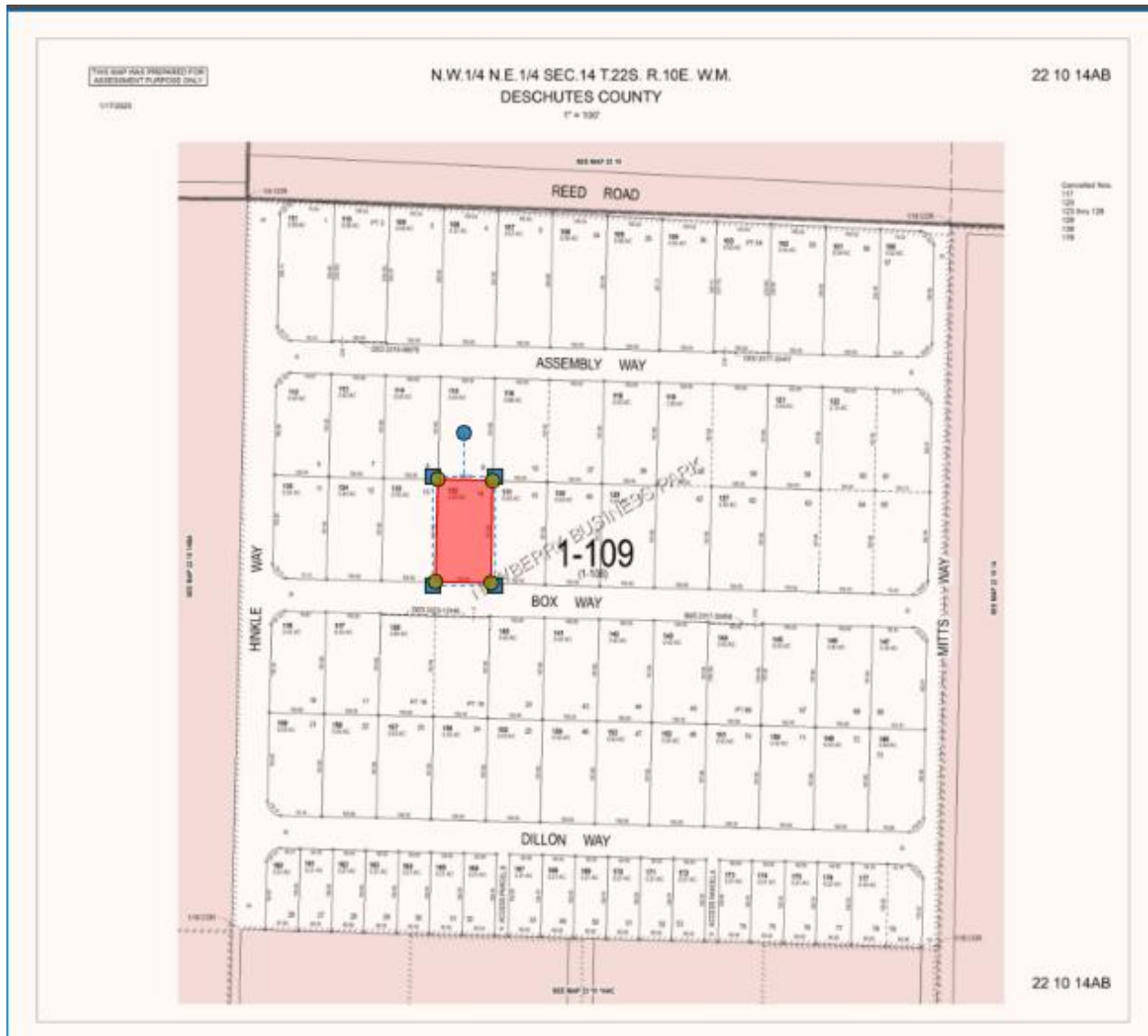
Article 3 – Zoning Districts

- Chapter 15.24 Industrial and Public Facility Zones

Article 5 – Development Standards

- Chapter 15.80 Development Standards, Generally
- Chapter 15.82 Landscaping, Buffering and Fences
- Chapter 15.86 Parking and Loading
- Chapter 15.88 Access and Circulation

The project is located at 16330 Box Way. Being Tax Lot 221014AB00132



CHAPTER 15.24. INDUSTRIAL AND PUBLIC FACILITY ZONES

Sec. 15.24.100. Purpose.

Chapter 15.24 regulates allowed land uses ("uses") and sets forth lot and development standards, including without limitation minimum dimensions, area, density, coverage, structure height, and other provisions that control the intensity, scale, and location of development in the industrial and public facility zones. The regulations of this chapter are intended to implement the City of La Pine Comprehensive Plan.

Sec. 15.24.200. Characteristics of the industrial and public facility zones.

Industrial and public facility zones accommodate a mix of intensive and less intensive uses engaged in manufacturing, processing, warehousing, distribution, and similar activities. Two industrial zoning districts, one for light industrial uses and one for general industrial uses, provide for the full range of planned industrial land uses within the city. Both districts are intended to provide for efficient use of land and public services, provide a high-quality environment for business, offer a range of parcel sizes and locations for industrial site selection, avoid encroachment by incompatible uses, provide transportation options for employees and customers, and facilitate compatibility between dissimilar uses.

A. Light Industrial Zone (LI). The LI zone is intended to allow for a mix of industrial and manufacturing businesses alongside industrial services, research and development, and small-scale retail and professional services.

B. Industrial Zone (I). The I zone allows for the same uses as the LI zone, but also provides suitable locations for more intensive industrial uses, such as those with processing, manufacturing, assembly, packaging, distribution, or other activities.

C. Public Facility Zone (PF). The PF zone is intended to provide areas for large-scale public facility and utility uses that require separation from residential and commercial uses. Additionally, the PF zone accommodates industrial uses that are compatible with large-scale public facilities.

RESPONSE: *The current zoning is I (Industrial) All future tenants uses will comply with this zone.*

Sec. 15.24.300. Use regulations.

Uses may be designated as permitted, limited, conditional, or prohibited in the industrial and public facility zones. As noted in Table 15.24-1, a use may also be subject to special use standards of article 6.

A. Permitted uses (P). Uses allowed outright in the industrial and public facility zones are listed in Table 15.24-1 with a "P."

B. Limited uses (L). Uses allowed in the industrial and public facility zones subject to limitations are listed in Table 15.24-1 with an "L." The limitations are defined below and correspond with the footnote numbers in Table 15.24-1.

1. Eating and drinking establishments in the LI zone. Eating and drinking establishments in the LI zone are limited to 2,500 square feet of gross floor area.

2. Offices in the LI, I, and PF zones. Offices as a primary use are limited to industrial offices (as defined in section 15.14.235) and government offices that do not include a point-of-service facility. All other office uses must be accessory to a permitted industrial use.

3. Retail sales and services in the LI zone. Retail sales and services in the LI zone are limited to 2,500 square feet of gross floor area, except for the following uses:

a. Health and fitness centers may exceed the maximum floor area.

b. Retail sales of heavy equipment may exceed the maximum floor area with a conditional use permit.

c. Retail sales of goods that are displayed outdoors, such as sales of building materials, landscape materials, or garden or farm supplies, may exceed the maximum floor area with a conditional use permit.

4. Automotive wrecking, salvage, and junk yards. The storage or sale of junk requires a special license, see section 15.108.040.

5. General manufacturing and production in the LI zone. Agricultural processing establishments require a conditional use permit. Energy and power generation uses are prohibited. All other general manufacturing and production uses are permitted outright.

6. Warehouse and freight movement in the LI zones. Truck transportation and loading terminals require a conditional use permit. All other warehouse and freight movement uses permitted outright.

7. Community services in the LI zone. Government buildings and services that do not include a point-of-service facility are permitted. All other uses are prohibited.

8. Agriculture in the LI zone. Agriculture uses in the LI zone are limited to large animal veterinary clinics allowed with a conditional use permit.

9. Marijuana facilities in the I zone. Marijuana testing laboratories are permitted outright. Marijuana processing facilities, production facilities, or wholesalers are allowed with a conditional use permit.

10. Retail sales and services in the I and PF zones. Retail sales and services in the I and PF zones are limited to mobile food unit sites.

C. Conditional uses (CU). Uses which are allowed if approved through the conditional use review process are listed in Table 15.24-1 with a "CU." These uses are allowed provided they comply with the conditional use requirements of chapter 15.316, conditional uses. Uses listed with a "CU" that also have a footnote number in the table are subject to the regulations cited in the footnote.

D. Prohibited uses (N). Uses listed in Table 15.24-1 with an "N" are prohibited. Existing uses in categories listed as prohibited may be subject to the regulations of chapter 15.08, non-conforming uses and structures.

Table 15.24-1. Use Regulations in the Industrial and Public Facility Zones

<i>Table 15.24-1. Use Regulations in the Industrial and Public Facility Zones</i>				
<i>Use Category</i>	<i>LI</i>	<i>I</i>	<i>PF</i>	<i>Special Use Standards</i>
<i>Residential Use Categories - None Permitted</i>				
<i>Commercial Use Categories</i>				
Campgrounds and recreational vehicle parks	N	N	N	—
Commercial lodging	N	N	N	—
Commercial parking	N	N	N	—
Commercial recreation	N	N	N	—
Eating and drinking establishments	L (1)	L (10)	N L (10)	Mobile food unit sites subject to Section 15.108.070.
Marijuana dispensary	N	N	N	—
Quick vehicle servicing	P	N	N	—
Office	L (2)	L (2)	L (2)	—
Retail sales and service	L (3)	N	N	—
Vehicle repair	P	N	N	—
Self-service storage	CU(11)	P(11)	P(11)	—
<i>Industrial Use Categories</i>				
Artisanal and light manufacturing	P	P	P	Section 15.108.010
Automotive wrecking, salvage, and junk yards	L (4)	N	L/CU (4)	Section 15.108.040
Industrial service	P	N	N	—
General manufacturing and production	L/CU (5)	P	P	—
Marijuana facilities	N	L/CU (9)	N	Section 15.108.050
Wholesale sales	P	P	P	—
Warehouse and freight movement	L/CU (6)	P	P	—
Waste treatment and recycling	N	N	CU	Section 15.108.100
<i>Institutional Use Categories</i>				
Basic utilities	P	P	P	—
Colleges	N	N	N	—

Community services	L (7)	P	N	—
Daycare centers	P	N	N	—
Medical centers	N	N	N	—
Parks and open areas	CU	CU	CU	—
Religious institutions	N	N	N	—
Schools	N	N	N	—
<i>Other Use Categories</i>				
Agriculture	L/CU (8)	N	N	Section 15.108.080
Forestry	P	P	P	—
Mining	N	CU	CU	Section 15.108.060
Wireless telecommunication facilities	P	P	P	Section 15.108.110

RESPONSE: *All future tenant uses will comply with either the permitted or condition use in this zone.*

Sec. 15.24.400. Development standards.

A. Purpose. The development standards for industrial and public facility zones allow development flexibility, within parameters, that supports the intended characteristics of the specific zone. In addition, the regulations provide guidance to property owners, developers, and neighbors about the limits of what is allowed.

B. Development standards. The development standards for industrial and public facility zones are presented in Table 15.24-2. Development standards may be modified as provided by chapter 15.320, variances. Additional standards may apply to specific zones or uses, see section 15.24.500.

Table 15.24-2. Development Standards in the Industrial and Public Facility Zones			
Standard	LI	I	PF
Minimum setbacks	—	—	—
- Front or street-side yard	20 feet	None	None
- Side yard	10 feet	None	None
- Rear yard	10 feet	None	None

Table 15.24-2. Development Standards in the Industrial and Public Facility Zones			
Standard	LI	I	PF
Maximum building height	45 feet	75 feet	75 feet
Maximum lot coverage	60%	80%	80%
Minimum landscaped area	See 15.24.500.A and chapter 15.82		

RESPONSE: *There are no setback requirements for the Industrial zone, the proposed building setbacks are as follows: North front yard 13.84 feet, East side yard 55.00 feet, West side yard feet and South Rear yard 13.84 feet. The proposed building will cover 34% of the lot, The existing lot is 18,769 s.f.e a minimum of 10% or or 1,877 s.f. of the site will be landscaped. This complies with the Development Standards.*

Sec. 15.24.500. Additional standards.

- A. Landscaping standard. Any portion of a lot developed for industrial uses which are not used for buildings, other structures, parking or loading spaces, or aisles, driveways, sidewalks, and designated storage areas shall be planted and maintained with grass or other all-season groundcover vegetation. Grass shall be kept neatly mowed. Landscaping with trees and shrubs is permitted and encouraged. See additional landscaping standards in article 5.

RESPONSE: *The proposed landscaping of the developed area will comply with this section.*

- B. Screening requirements. All accessory storage of junk, waste, discarded or salvaged material, machinery, or equipment shall not be permitted except within a completely enclosed structure. Or if the lot area devoted to such use is over 200 square feet in area, the owner may have the alternative of enclosing it on all sides, except for an exit and entrance not over 25 feet in width, by a solid fence or wall at least six feet in height and maintained in good condition or by a cyclone or equal-wire fence at least six feet in height and surrounded, except for an exit and entrance not over 25 feet in width, by evergreens at least six feet in height and planted not further apart than six feet so as to form a solid screen. See also chapter 15.82 for additional screening requirements. See additional buffering and fence standards in article 5.

RESPONSE: *No exterior storage is being proposed at this time.*

- C. [C.] Vehicle access. Access driveways and entrances shall be permitted in a number and locations in which sight distance is adequate to allow safe movement of traffic in or out of the driveway or entrance, the free movement of normal highway traffic is not impaired, and the driveway or entrance will not create a hazard or an area of undue traffic congestion on highways to which it has access. The city may require the permit applicant to submit engineering data and/or traffic analyses to support its proposed plan of access driveways and entrances. See additional access and circulation standards in article 5.

RESPONSE: *The access will be from Box Way and will be 26 feet in width, the driveway will be 375 feet Hinkle Way and 811 feet from Mitts Way, and will provide adequate sight distance and allow safe movement of traffic.*

- D. [D.] Emissions. Industrial uses shall comply with all applicable pollution control regulations enacted by the federal and state government and other governmental authorities.

RESPONSE: *Any emissions from future tenants will comply with this section.*

- E. [E.] Noise. Industrial uses shall provide necessary shielding or other protective measures against interference caused by mechanical and nuclear equipment, or uses or processes with electrical apparatus, to nearby residences.

RESPONSE: *Future tenants will comply with this section.*

- G. [F.] Lighting. All exterior lighting shall be so placed and shielded so as not to create a nuisance for adjacent properties.

RESPONSE: *All exterior lighting will comply with this section.*

CHAPTER 15.80. DEVELOPMENT STANDARDS, GENERALLY

Sec. 15.80.010. Purpose.

Article 5 contains development and design standards for the built environment. The standards are intended to protect the public health, safety, and welfare through the provision of landscaping and buffering, parking and loading facilities, multimodal accessibility and interconnectivity, and adequate public facilities.

In interpreting and applying this article, the provisions herein shall be held to be the minimum requirements adopted for the promotion of the public health, safety, comfort, convenience, and general welfare.

Sec. 15.80.020. Applicability.

Any land division or development, and the improvements required therefore, shall be in compliance with the development, design and improvement standards and requirements set forth in this article. Other provisions of this Development Code, other city ordinances, or state statutes or administrative rules may also apply.

Sec. 15.80.030. Exemption - lot size requirements.

- A. The following exemptions to minimum lot size requirements shall apply:
1. Non-conforming lots or aggregate of contiguous lots or parcels held in a single ownership has an area or dimensions which do not meet the lot size or dimensional requirements of the applicable zone, the lot or aggregate holdings may be occupied by a use permitted in the zone subject to the other requirements of the zone; providing, however, residential use shall be limited to single-family dwelling unit or to the number of dwelling units consistent with the equivalent densities of the zone.
 2. Any parcel of land or portion thereof, which is to be dedicated to a public, semi-public or public utility for a park, school, road, canal, railroad, utility or other public use shall be exempt from the minimum lot size requirements of this chapter and the applicable zone.
- B. For all other lot size requirements in all other zones, applicants may propose approval of exceptions or variances in accordance with the application requirements in article 8.

RESPONSE: *No exemptions are being requested.*

Sec. 15.80.040. Exemption - yard or setback requirements.

The following exemptions to yard or setback requirements are authorized for a lot or use in any zone:

- A. If there is a lot where there are buildings on abutting lots, and the buildings are within 100 feet of the intervening lot, and the buildings have front yards less than the required front yard for the applicable zone, the depth of the front yard for the subject lot need not exceed the average depth of the front yards of the abutting lots.
- B. If there is a building on only one abutting lot within 100 feet with a front yard less than the required front yard for the zone, the front yard of the subject lot need not exceed a depth one-half way between the depth of the yard on the abutting lot and the required front yard of the applicable zone.
- C. Architectural features such as cornices, eaves, sunshades, canopies, gutters, chimneys and flues may project into a required yard two feet, provided that the projection is not closer than three feet to a property line, and, drainage or snowdrift does not flow onto abutting properties or right-of-way, and, fumes from woodstoves are not directed to other properties. Steps, terraces, platforms, patios, decks and porches having no roof covering, and fences not interfering with vision clearance requirements or drainage requirements may be permitted in required yards, except as otherwise limited or provided for by this chapter, or as otherwise approved by the city.

RESPONSE: *No exceptions are being requested.*

Sec. 15.80.050. Supplementary height regulations.

The maximum height limitations shall not apply to:

- A. The following principal structures: Church, college, farm structure (other than a farm dwelling), hospital, radio or television tower, exhaust stack, emergency services structure, or public utility structure which is a permitted use and is located in any zone, provided it shall conform to the setback and yard requirements of the zone where it is located plus one additional foot horizontally for each foot over 45 feet in height.
- B. The following appurtenances attached to or part of a principal or accessory structure: Church spire, belfry, cupola, dome, monument, smoke-stack, derrick, conveyor, flag pole, mast, antenna, aerial, roof tank; ventilating air conditioning and similar building service equipment; roof structure, chimney and/or parapet wall, provided it shall be set back in conformance with the setback and yard requirements plus one foot horizontally for each foot in which it exceeds 45 feet in height above ground level. The principal or accessory structure to which it is attached may conform to setback and yard requirements with no additional setback provided the principal or accessory structure conforms to the height limitations of the zone.

RESPONSE: *No structure is being proposed that will exceed the height regulations.*

Sec. 15.80.060. Restrictions on the use of metal shipping containers.

Except as specified below, metal shipping containers shall not be placed on-site:

- A. In residential zones, no metal shipping containers shall be utilized as a dwelling at any time, or as storage structures for greater than 30 days.
- B. In commercial zones, metal shipping containers shall not be placed on-site, with the exception of short-term use for construction or relocations (30 days or less), or in the case of construction; 30 days after a certificate of occupancy has been issued.
- C. In industrial zones, metal shipping containers are permitted for storage uses.

RESPONSE: *No shipping containers are being proposed on-site.*

CHAPTER 15.82. LANDSCAPING, BUFFERING AND FENCES

Sec. 15.82.010. Landscaping and buffering requirements.

The following minimum landscape requirements are established for all developments subject to site plan approval, unless approved otherwise by the reviewing authority:

A. Exemption. The provisions of this section may be exempted for uses existing on or before the effective date of this Development Code that are a permitted use in a specific zone in an existing building or buildings on a lot or parcel of land of the scale that there is no remaining room for landscaping; this exemption shall also apply to the exterior remodeling and/or expansion of not more than 25 percent of the total square footage of all enclosed structures on a lot or parcel existing under a unit ownership on or before the effective date of this Development Code.

B. Area required. Except as approved otherwise by the city, the following minimum percent of a parcel area shall be landscaped for the following uses:

1. Duplexes and triplexes: 25 percent.
2. Multi-family developments containing four or more units and commercial residential mixed uses (CRMX): 20 percent.
3. Commercial uses including mixed use commercial (CMX): 15 percent.
4. Industrial uses. A minimum five-foot landscaped buffer along any adjoining public right-of-way of a collector or arterial street or highway, which may be computed toward an overall requirement of ten percent.
5. Minimum area requirements may include landscaping around buildings, in parking and loading areas, outdoor recreational use areas, screening and buffering areas, and surface water drainage areas.

C. Landscaping defined. Required landscaping may include, but is not limited to, a combination of any of the following materials: living plant material such as trees, shrubs, groundcover, flowers and lawn (including native vegetation); and nonliving materials such as benches, walkways and courtyards, consisting of brick, decorative rock or other decorative materials. The total amount of nonliving materials (including bark dust, chips, aggregate, or other non-plant ground covers) shall not exceed more than 50 percent of the required landscape area.

D. Existing vegetation. Existing site vegetation shall be utilized to the maximum extent possible consistent with building placement and the applicable proposed landscape plan.

E. Parking lots. Parking lots with space for ten or more vehicles must be landscaped in accordance with the following minimum requirements:

1. In commercial and residential developments, parking areas shall be divided into bays, and between or at the end of each parking bay a curbed planter containing at least 16 square feet may be required.
2. If required, each planter shall contain at least one tree or shrub and ground cover.

3. The areas shall be designed to be protected from being damaged by vehicles using the parking area.

4. Unless sidewalks are provided adjacent to a structure, customer or resident parking areas should be separated from the exterior wall of a commercial or residential structure by a minimum five-foot strip of landscaping.

5. Where a parking, loading or driveway area serving a multi-family, commercial, industrial or government use abuts a public right-of-way of a collector or arterial street or a local street across

from a residential zone, or abuts a residential zone, a screen planting or other approved landscaped planter strip may be required between the parking area and the right-of-way without encroaching into a clear vision area or sidewalk.

F. Buffering and screening.

1. Purpose. The purpose of buffering and screening requirements are to reduce the impacts of a proposed use on adjacent uses and zones which provide for different types of uses. The city may waive or reduce the requirements where existing topography or vegetation is appropriate or otherwise negates the effectiveness or intended purpose or benefits of the buffering and screening.

2. Where any permitted principal and/or accessory use in a commercial or industrial zone abuts any land zoned RSF, RMF, RMP or TA the following buffer and screening shall be required. These requirements shall apply in instances where such use is being newly developed on vacant land, expanded in floor area by 50 percent or greater, or removed and a new use developed.

3. Within commercial zones. A buffer strip at least ten feet wide shall be provided and maintained along the entire length of a side or rear yard where it abuts an RSF, RMF, RMP, or TA zone. Buffer strips shall not be used for parking, storage of vehicles, equipment, or materials, nor for any other use incompatible with their purpose as a visual, noise, dust, and pollution barrier. The buffer strip shall contain suitable screening, defined as either of the following:

a. A solid fence or wall, architecturally compatible with existing structures in the area, no less than five feet nor more than eight feet in height; or

b. A sight-obscuring planting of evergreens, not less than four feet in height at the time of planting and of a variety that will maintain full, dense growth from the ground up to a height of not less than six feet upon maturity, planted at a spacing of the lesser of eight feet or the diameter of a mature specimen of the species being planted.

c. Areas of the buffer strip not covered with a fence, wall, or screening plantings, shall be planted with appropriate ground cover vegetation, including native species. Xeriscape methods are highly encouraged.

d. Installation and maintenance of the buffer and screening shall be the responsibility of the owner of the property on which the "C" type zone permitted use is located. Installation must be completed prior to issuance of a certificate of use and occupancy by the city. Fences or walls must be maintained in safe and structurally sound condition. Dead or diseased plants shall be removed and replaced in a timely manner. Grass shall be kept neatly mowed.

4. Within industrial zones. A buffer strip at least 30 feet wide shall be provided and maintained along the entire length of a side or rear yard where it abuts any RSF, RMF, RMP, or TA zoned land. Buffer strips shall not be used for parking, storage of vehicles, equipment, or materials, nor for any other use incompatible with their purpose as a visual, noise, dust, and pollution barrier. The buffer shall meet the following standards:

a. The buffer shall be planted with evergreens capable of obtaining and maintaining a dense growth to a full height and a full canopy diameter of no less than 12 feet. The minimum height at the time of planting shall be six feet. Plants shall be situated in two rows within the buffer strip, each row being located at least ten feet from the edge of the buffer strip. Plants in each row shall be spaced no more than 20 feet center-to-center and the two rows shall be situated in an alternating pattern so that the trees in one row are located centrally between the trees in the other row. Plants shall be allowed to obtain a minimum height of 12 feet and shall not be trimmed below that height thereafter.

b. Installation and maintenance of the buffer and screening shall be the responsibility of the owner of the property on which the industrial use is located. Installation must be completed prior to issuance of a certificate of use and occupancy by the city. Dead or diseased plants shall be removed and replaced in a timely manner. Xeriscape methods and use of native species is highly encouraged.

c. A property owner may not sell, lease, or otherwise transfer property if such action results in a reduction of a separation distance for a commercial or light manufacturing use below the minimum required in this section. Likewise, a property owner may not remove or alter natural vegetation or landforms serving upon a waiver from the city as buffer and screening for a commercial or light manufacturing use if such action results in the natural buffer and screening being less effective than as required in this and other sections of this Development Code.

5. A buffer or screening area may only be occupied by screening utilities and landscaping materials, but the same may be located within the required yard or setback requirements provided vision clearance requirements are complied with.

6. In lieu of the foregoing requirements, an applicant may provide for landscaping and screening, including plantings, fences, walls, walks and other features designed to afford the same degree of buffering as the standards above. A plan and specifications for an alternative shall be reviewed and approved by the review authority.

G. Plant material installation standards. Except as otherwise approved by the city, the following standards shall apply to plant materials and the installation thereof as provided in accordance with the provisions of this section:

1. Landscape plant materials shall be properly guyed and staked, and shall not interfere with vehicular or pedestrian traffic or parking and loading.

2. Trees shall be a minimum size of six feet in height and be fully branched at the time of planting.

3. Shrubs shall be supplied in one-gallon containers or six-inch burlap balls with a minimum spread of 12 inches.

4. Rows of plants should be staggered to provide for more effective coverage.

H. Maintenance and plant survival. All landscaping approved or required as a part of a development plan shall be continuously maintained, including necessary watering, weeding, pruning and replacement of plant materials. Except where the applicant proposes landscaping consisting of drought-resistant plantings and materials that can be maintained and can survive without irrigation, landscaped areas shall be irrigated. If plantings fail to survive, it is the responsibility of the property owner to replace them.

I. Retention of trees. As part of a landscaping plan, the developer shall also provide a tree plan identifying the location and diameter breast height (DBH) of all trees on the property. Existing mature trees with at least a 10-inch DBH shall be retained to the furthest extent possible. Development shall conform to the natural environment to incorporate existing mature trees. Where trees must be removed for proposed development, they shall be replaced at a 1:1 ratio on the subject lot or parcel with trees of similar species that are native to the region, and with a DBH of at least five inches or larger. Replacement trees shall meet the maintenance and plant survival criteria of this code section. Any trees planted must maintain a 20-foot radius for spacing. The 20-foot radius spacing shall be the determining factor in how many trees are replaced on the subject property. If more trees must be planted than what the 20-foot radius spacing would allow, then the number of trees replaced shall be reduced to meet the spacing standard.

RESPONSE: *The existing lot is 18,769 s.f. 10% of the developed area is 1,877 s.f., a minimum of 10% of the site will be landscaped. The site does not a but a residential zone. There are 15 trees 10-inches or larger being removed, based on the 20-foot radius spacing 13 trees will be replaced. This complies with the Development Standards.*

Sec. 15.82.020. Fences and walls.

The yard and setback requirements of this Development Code shall not be deemed to restrict any otherwise lawful fence, wall, or sign, provided that no fence, wall, or sign shall be located on any right-of-way of a public road.

- A. *Materials.* Fences and walls shall not be constructed of nor contain any material that could cause bodily harm, such as barbed wire, broken glass, spikes, or any other hazardous or dangerous materials, except as provided below.
 - 1. Barbed wire fences intended to contain or restrict cattle, sheep, horses or other livestock, are permitted in any zone where the keeping of livestock is permitted.
 - 2. Electric fences are permitted in any zone where the keeping of livestock is permitted, provided the following standards are met:
 - a. The fence product shall be listed by a State of Oregon approved testing laboratory.
 - b. The fence shall be installed and used in accordance with the testing laboratory listing.
 - c. Electrical permits and inspections shall be required for the installation.
 - d. Warning signs which notify individuals of a dangerous fence shall be posted on the fence, at intervals not to exceed 50 feet. The statement, DANGER - Electrified Fence, or an equivalent statement, shall be on the warning signs.
 - e. The fence must be located outside any front yard setback and required landscaping, buffering or screening areas.
- B. *Standards.*
 - 1. Every fence shall be maintained in a condition of reasonable repair and shall not be allowed to become and remain in a condition of disrepair including noticeable leaning, missing sections, broken supports, non-uniform height, and uncontrolled growth of vegetation.
 - 2. All required swimming pool and hot tub fencing shall be a minimum of four feet in height and be equipped with a self-locking gate that closes automatically.
 - 3. Fences within a front or street side yard shall also conform to the clear vision requirements at intersections, which further restrict the use or height of sight-obscuring fences.
 - 4. In no instance shall a fence extend beyond the property line including into a public right-of-way. It is the responsibility of the property owner to determine the property line.

5. Within residential and commercial zones, fences within the required front yard setback may not exceed four feet in height except that one incidental garden structure (e.g., arbor or gate) not exceeding eight feet in height and six feet in width is allowed within the required front yard provided it does not encroach into a required clear vision area. All other fences in all zones shall not exceed seven feet in height.
6. Other provisions of this Development Code, or the requirements of the roadway authority, may limit allowable height of a fence or wall below the height limits of this section.

RESPONSE: *No fences are being proposed at this time.*

CHAPTER 15.86. PARKING AND LOADING

Sec. 15.86.010. Applicability.

Off-street loading and vehicle and bicycle parking spaces shall be provided in accordance with the specifications of this chapter in all zones whenever any new use is established, an existing use is enlarged, or an existing use of land or structure is changed to a new use. Such new, enlarged, or changed use shall fully comply with the specifications of this chapter prior to being given a certificate of use and occupancy.

Sec. 15.86.020. Off-street loading.

- A. Every commercial and industrial use which requires the receipt or distribution of material or merchandise by trucks with a 40-foot or longer wheelbase at a frequency of one or more vehicles per week shall provide off-street loading spaces in sufficient number to adequately serve the number and frequency of vehicle shipping and receiving projected for the use. The applicant shall provide supporting evidence of the projected shipping and receiving and how the number of spaces to be provided will be adequate.
- B. Where an off-street loading space is required, it shall be large enough to accommodate the largest vehicle that is expected to serve the use without obstructing vehicles or pedestrian traffic on adjacent streets and driveways. Each off-street loading space shall not be less than 12 feet wide by 55 feet long unless otherwise approved by the city through site design review.
- C. Off-street loading space(s) shall also have adequate adjacent area for vehicle maneuvering so that vehicles using the space(s) are not required to back-up onto or back-up from a public street or alley to use the space. Where parking areas are prohibited between a building and the street, loading areas are also prohibited.
- D. Exceptions and adjustments. The city, through site design review, may approve a loading area adjacent to or within a street right-of-way where it finds that loading and unloading operations are short in duration (i.e., less than one hour), infrequent, do not obstruct traffic during peak traffic hours, do not interfere with emergency response services, and are acceptable to the applicable roadway authority.

Sec. 15.86.030. Off-street parking - required.

- A. *Location of off-street loading and parking spaces.* Except as otherwise permitted by this Development Code, required off-street loading and parking spaces shall be located on the same lot with the principal use they are intended to serve. In no case shall a required loading space be part of the area used to satisfy the parking requirements and vice versa. Also, in no case shall the required loading or parking space(s) of one use be used to satisfy the loading or parking space requirements of another use.
- B. *Encroachment or reduction.* A required loading or parking space shall not be encroached upon by a structure, storage, or other use, nor shall the number of spaces be reduced without replacement of a commensurate number of spaces in accordance with this section unless a special exception or variance has been approved.
- C. *Calculations of amounts of required and allowed parking.*

1. When computing parking spaces based on floor area, parking structures and non-leasable floor spaces, such as storage closets, mechanical equipment rooms, and similar spaces, are not counted.
 2. The number of parking spaces is computed based on the primary uses on the site except as stated in subsection 3, below. When there are two or more separate primary uses on a site, the minimum and maximum parking for the site is the sum of the required or allowed parking for the individual primary uses. For shared parking, see subsection I below.
 3. When more than 20 percent of the floor area on a site is in an accessory use, the required or allowed parking is calculated separately for the accessory use. An example would be a 10,000 square foot building with a 7,000 square foot warehouse and a 3,000 square foot accessory retail area. The minimum and maximum parking would be computed separately for the retail and warehouse uses.
- D. *Use of required parking spaces.* Except as otherwise provided by this section, required parking spaces must be available for residents, customers, or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for shared parking pursuant to subsection I.
- E. *Improvement of parking areas.* Motorized vehicle parking is allowed only on streets with an improved shoulder of sufficient width; within garages, carports, and other approved structures; and on driveways or parking lots that have been developed in conformance with this Development Code.
- F. *Minimum number of off-street automobile parking spaces.* Except as required for Americans with Disabilities Act compliance under subsection L, off-street parking shall be provided pursuant to one of the following three standards:
1. The standards in Table 15.86-1;
 2. A standard from Table 15.86-1 for a use that the planning official determines is similar to the proposed use. For uses not specified in the table, the city shall determine parking based on submission of technical data from applicant or city sources; or
 3. Subsection (H), parking exceptions, which includes a parking demand analysis option.

<i>Table 15.86-1. Automobile Parking Spaces by Use</i>	
<i>Use Categories</i>	<i>Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.)</i>
<i>Industrial Categories¹</i>	
Industrial service	One space per employee on the largest shift plus one space for each 10,000 sq. ft. for visitors up to ten additional spaces
Manufacturing and production	One space per 1,000 sq. ft. of floor area; or as required by conditional use permit review
Warehouse and freight movement	0.5 space per 1,000 sq. ft. of floor area; or as required by conditional use permit review
Waste-related	Per conditional use permit review
Wholesale sales, e.g., building materials, heavy equipment, agricultural supplies, etc.	One space per 1,000 sq. ft.
Marijuana wholesaler/production facility/processing facility/testing laboratories	Four plus one additional space per 2,000 sq. ft. gross floor area

RESPONSE: *There are a number of uses in the industrial category, for parking we are using one space per 1,000 sq. ft. of floor area, the proposed building is 6,400 sq. ft. / 1,000 = 6.4 spaces, we are providing 9 parking spaces.*

- G. *Maximum number of off-street automobile parking spaces.* The following standards for maximum number of automobile parking spaces promote efficient use of land and compact development patterns.
1. *Applicability.* Developments subject to site plan review must conform to the maximum parking standards.
 2. *Standards.* Unless otherwise approved by the city through site plan review, the maximum number of off-street automobile parking spaces allowed for a commercial development equals the minimum number of required spaces, pursuant to Table 15.86-1 times a factor of 2.0. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements.

RESPONSE: *6.4 spaces are required x 2 equals 12.8 spaces, we are providing 9 spaces, not exceeding the maximum parking*

- H. *Exceptions and reductions to off-street parking.* An applicant may propose a parking standard that is different than the standards under subsections F or G, for review and action by the planning official through a Type II procedure. The applicant's proposal shall consist of a written request and a parking analysis prepared by a qualified professional. The parking analysis, at a minimum, shall assess the average parking demand and available supply for existing and proposed uses on the subject site; opportunities for shared parking with other uses in the vicinity; existing public parking in the vicinity; transportation options existing or planned near the site, such as frequent bus service, carpools, or private shuttles; and other relevant factors. The number of required off-street parking spaces may also be reduced through the provision of shared parking, pursuant to subsection I.
- I. *Shared parking.* Required parking facilities for two or more uses, structures, or parcels of land may be satisfied by the same parking facilities used jointly, to the extent that the owners or operators show that the need for parking facilities does not materially overlap (e.g., uses primarily of a daytime versus nighttime nature; weekday uses versus weekend uses), and, provided that the right of joint use is evidenced by a recorded deed, lease, contract, or similar written instrument establishing the joint use. Shared parking requests shall be subject to review and approval through site plan review.

RESPONSE: *No shared parking is being proposed.*

- J. *Parking stall design and minimum dimensions.* Where a new off-street parking area is proposed, or an existing off-street parking area is proposed for expansion, the entire parking area shall be improved in conformance with this Development Code. At a minimum the parking spaces and drive aisles shall be paved with asphalt, concrete, or other city-approved materials, provided the Americans with Disabilities Act requirements are met, and shall conform to the minimum dimensions in Table 15-86-2 and the figures below. All off-street parking areas shall contain wheel stops, perimeter curbing, bollards, or other edging as required to prevent vehicles from damaging buildings or encroaching into walkways, landscapes, or the public right-of-way. Parking areas shall also provide for surface water management.

<i>Table 15.86-2. Parking Stall Dimensions</i>					
<i>Parking Angle</i>	<i>Stall Width</i>	<i>20' Stall</i>	<i>Aisle Width (*one way)</i>	<i>Curb Length</i>	<i>Bay Width</i>
0°	9'-0"	9.0	12.0	22.0	30.0
	9'-6"	9.5	12.0	22.0	31.0
	10'-0"	10.0	12.0	22.0	32.0
45°	9'-0"	19.8	13.0	12.7	52.5
	9'-6"	20.1	13.0	13.4	53.3
	10'-0"	20.5	13.0	14.1	54.0
60°	9'-0"	21.0	18.0	10.4	60.0
	9'-6"	21.2	18.0	11.0	60.4
	10'-0"	21.5	18.0	11.9	61.0

70°	9'-0"	21.0	19.0	9.6	61.0
	9'-6"	21.2	18.5	10.1	60.9
	10'-0"	21.2	18.0	10.6	60.4
90°	9'-0"	20.0	24.0	9.0	64.0
	9'-6"	20.0	24.0	9.5	64.0
	10'-0"	20.0	24.0	10.0	64.0
*24' minimum for two-way traffic					

- K. *Adjustments to parking area dimensions.* The dimensions in subsection (J) are minimum standards. The city planning official, through a Type II procedure, may adjust the dimensions based on evidence that a particular use will require more or less maneuvering area.
- L. *Americans with Disabilities Act (ADA).* Parking shall be provided consistent with ADA requirements, including, but not limited to, the minimum number of spaces for automobiles, van-accessible spaces, location of spaces relative to building entrances, accessible routes between parking areas and building entrances, identification signs, lighting, and other design and construction requirements.

RESPONSE: *All proposed parking is 90° and is 9' x 20', 1 ADA space has been provided.*

Sec. 15.86.040. Drive-up and drive-through uses and facilities.

- A. *Purpose.* Where drive-up or drive-through uses and facilities are allowed, they shall conform to all of the following standards, which are intended to calm traffic, provide for adequate vehicle queuing space, prevent automobile turning movement conflicts, and provide for pedestrian comfort and safety.
- B. *Standards.* Drive-up and drive-through facilities (i.e., driveway queuing areas, customer service windows, teller machines, kiosks, drop-boxes, or similar facilities) shall meet all of the following standards:
1. The drive-up or drive-through facility shall orient to and receive access from a driveway that is internal to the development and not a street, as generally illustrated.
 2. The drive-up or drive-through facility shall not be oriented to street corner.
 3. The drive-up or drive-through facility shall not be located within 20 feet of a street right-of-way.
 4. Drive-up and drive-through queuing areas shall be designed so that vehicles will not obstruct any street, fire lane, walkway, bike lane, or sidewalk.
 5. All drive up and drive thru facilities must be permanently affixed to the ground and be connected to the city water and sewer in accordance with the standards of chapter 15.90.

RESPONSE: *No drive-through facility is being proposed.*

Sec. 15.86.050. Bicycle parking.

- A. *Exemptions.* This section does not apply to single-family and duplex housing, home occupations, and agricultural uses. The planning official may exempt other uses upon finding that, due to the nature of the use or its location, it is unlikely to have any patrons or employees arriving by bicycle.
- B. *Standards.* Bicycle parking spaces shall be provided with new development and, where a change of use occurs, at a minimum, shall follow the standards in Table 15.86-3. Where an application is subject to conditional use permit approval or the applicant has requested a reduction to an automobile-parking standard, the city may require bicycle parking spaces in addition to those in Table 15.86-3.

<i>Table 15.86 -3. Minimum Required Bicycle Parking Spaces</i>	
<i>Use</i>	<i>Minimum Number of Spaces</i>
Industrial	2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater
Other uses	2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater

RESPONSE: *2 bike parking spaces are being provided for the building if additional spaces are required for the tenants they will be designated inside of the tenant warehouse space.*

- C. *Design.* Bicycle parking shall consist of staple-design steel racks or other city-approved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle. At a minimum, bicycle parking facilities shall be consistent with the following design guidelines:
1. All bicycle parking shall be within 100 feet from a building entrance and located within a well-lit and clearly visible area;
 2. Bicycle parking shall be convenient and easy to find. Where necessary, a sign shall be used to direct users to the parking facility;
 3. Each bicycle parking space shall be at least two feet by six feet with a vertical clearance of six feet;
 4. An access aisle of at least five feet shall be provided in each bicycle parking facility;
 5. Bicycle parking facilities shall offer security in the form of either a lockable enclosure in which the bicycle can be stored or a stationary object, i.e., a "rack," upon which the bicycle can be locked. Structures that require a user-supplied lock shall accommodate both cables and U-shaped locks and shall permit the frame and both wheels to be secured (removing the front wheel may be necessary).
Note: businesses may provide long-term, employee parking by allowing access to a secure room within a building.
- D. *Hazards.* Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall be located so as to not conflict with the vision clearance standards of section 15.88.040.

Sec. 15.86.060. Snow storage areas.

- A. *Purpose.* The purpose of these standards is to ensure that adequate space is be provided within a development for storage of snow in winter months in order to accommodate space needed for access, circulation, and off-street parking.
- B. *Applicability.* Snow storage standards apply to all subdivisions and to developments subject to site plan review.
- C. *Standards.*
1. *Minimum area.* Snow storage areas must be designated on a site plan. The areas must total a minimum of 15 percent of the area to be cleared, including all access drives, parking areas, and walkways.
 2. *Location.* Snow storage is not permitted on landscaped areas, except where these areas are limited to grass or rock cover. Snow storage may be permitted in parking areas, provided that the site can still accommodate enough parking spaces to meet minimum off-street parking requirements in winter months. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements. It is encouraged that snow storage areas be located away from public view and that additional impervious surface areas are not created for the sole purpose of snow storage.

3. *Exceptions and adjustments.* The city may reduce or eliminate the required snow storage areas if a snow removal plan is presented which provides a continuous guarantee of removal.

RESPONSE: *15% of the parking areas, access drives and walkways is 1,347 sq. ft. we are providing 1,526 sq. ft. for snow storage.*

CHAPTER 15.88. ACCESS AND CIRCULATION

Sec. 15.88.010. Purpose.

Chapter 15.88 contains standards for vehicular and pedestrian access, circulation, and connectivity. The standards promote safe, reasonably direct, and convenient options for walking and bicycling, while accommodating vehicle access to individual properties, as needed.

Sec. 15.88.020. Applicability.

Chapter 15.88 applies to new development and changes in land use necessitating a new or modified street or highway connection. Except where the standards of a roadway authority other than the city supersede city standards, chapter 15.88 applies to all connections to a street or highway, and to driveways and walkways.

Sec. 15.88.030. Vehicular access and circulation.

- A. *Purpose and intent.* Section 15.88.030 implements the street access guidelines of the City of La Pine Transportation System Plan. It is intended to promote safe vehicle access and egress to properties, while maintaining traffic operations in conformance with adopted standards. "Safety," for the purposes of this chapter, extends to all modes of transportation.
- B. *Permit required.* Vehicular access to a public street (e.g., a new or modified driveway connection to a street or highway) requires an approach permit approved by the applicable roadway authority.
- C. *Traffic study requirements.* The city, in reviewing a development proposal or other action requiring an approach permit, may require a traffic impact analysis, pursuant to section 15.90.080, to determine compliance with this Development Code.
- D. *Approach and driveway development standards.* Access management restrictions and limitations consist of provisions managing the number of access points and/or providing traffic and facility improvements that are designed to maximize the intended function of a particular street, road or highway. The intent is to achieve a balanced, comprehensive program which provides reasonable access as new development occurs while maintaining the safety and efficiency of traffic movement. Intersections, approaches and driveways shall conform to access spacing guidelines in the City of La Pine Transportation System Plan and the roadway authority's engineering standards. In the review of all new development, the reviewing authority shall consider the following techniques or considerations in providing for or restricting access to certain transportation facilities.
 1. Access points to arterials and collectors may be restricted through the use of the following techniques:
 - a. Restricting spacing between access points based on the type of development and the speed along the serving collector or arterial.
 - b. Sharing of access points between adjacent properties and developments.
 - c. Providing access via a local order of street; for example, using a collector for access to an arterial, and using a local street for access to a collector.
 - d. Constructing frontage or marginal access roads to separate local traffic from through traffic.

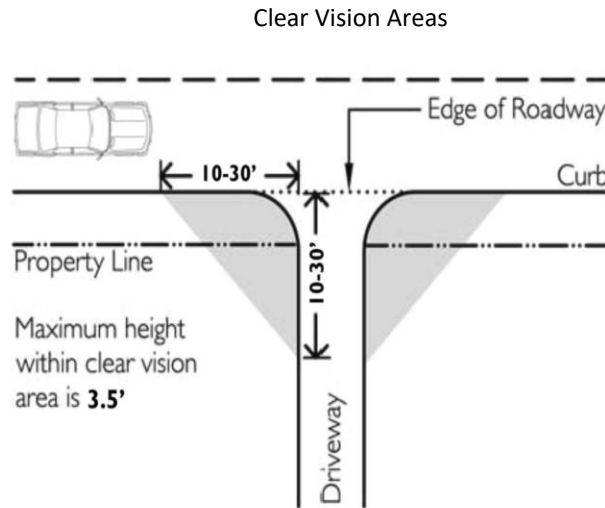
- e. Providing service drives to prevent overflow of vehicle queues onto adjoining roadways.
- 2. Consideration of the following traffic and facility improvements for access management:
 - a. Providing of acceleration, deceleration and right-turn-only lanes.
 - b. Offsetting driveways to produce T-intersections to minimize the number of conflict points between traffic using the driveways and through traffic.
 - c. Installation of median barriers to control conflicts associated with left turn movements.
 - d. Installing side barriers to the property along the serving arterial or collector to restrict access width to a minimum.
- E. *ODOT approval.* Where a new approach onto a state highway or a change of use adjacent to a state highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The city may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case the city will work cooperatively with the applicant and ODOT to avoid unnecessary delays.
- F. *Other agency approval.* Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.
- G. *Exceptions and adjustments.* The city may approve adjustments to the spacing standards of subsections above, where an existing connection to a city street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance.
- H. *Joint use access easement and maintenance agreement.* Where the city approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use of and cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed, defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the city for its records, but the city is not responsible for maintaining the driveway or resolving any dispute between property owners.

RESPONSE: *There is a proposed 26 foot wide driveway access to Box Way which has a local street classification, does not require ODOT review or approval. No joint access is being proposed*

Sec. 15.88.040. Clear vision areas (visibility at intersections).

- A. In all zones, a clear vision area shall be maintained on the corners of all property at the intersection of two streets or a street and a railroad. A clear vision area shall contain no planting, wall, structure, private signage, or temporary or permanent obstruction exceeding 3½ feet in height, measured from the top of the curb or, where no curb exists, from the established street centerline grade, except that trees exceeding this height may be located in this area provided all branches and foliage are removed to a height of eight feet above the grade.
- B. A clear vision area shall consist of a triangular area on the corner of a lot at the intersection of two streets or a street and a railroad (see Figure 18.88-1). Where lot lines have rounded corners, the specified distance is measured from a point determined by the extension of the lot lines to a point of intersection. The third side of the triangle is the line connecting the ends of the measured sections of the street lot lines. The following measurements shall establish clear vision areas within the city:
 - 1. In an agricultural, forestry or industrial zone, the minimum distance shall be 30 feet; or at intersections including an alley, ten feet.
 - 2. In all other zones, the minimum distance shall be in relationship to street and road right-of-way widths as follows:

<i>Right-of-Way Width</i>	<i>Clear vision</i>
80 feet or more	20 feet
Less than 80 feet	30 feet



RESPONSE: *Clear vision areas are shown on the site plan.*

5.88.050. Pedestrian access and circulation.

- A. *Purpose and intent.* This section implements the pedestrian access and connectivity policies of City of La Pine Transportation System Plan and the requirements of the Transportation Planning Rule (OAR 660-012). It is intended to provide for safe, reasonably direct, and convenient pedestrian access and circulation.
- B. *Standards.* New subdivisions, multi-family developments, planned developments, commercial developments and institutional developments shall conform to all of the following standards for pedestrian access and circulation:
 1. *Continuous walkway system.* A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.
 2. *Safe, direct, and convenient.* Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, recreational areas, playgrounds, and public rights-of-way conforming to the following standards:
 - a. The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel.
 - b. The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The city may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.

- c. Vehicle/walkway separation. Except as required for crosswalks, per subsection d., below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the city may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.
- d. Crosswalks. Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrasting material). The crosswalk may be part of a speed table to improve driver-visibility of pedestrians.
- e. Walkway construction. Walkway surfaces may be concrete, or other city-approved durable surface meeting ADA requirements. Walkways shall be not less than six feet in width, except that the city may require wider; sidewalks in developments where pedestrian traffic warrants walkways wider than six feet.
- f. Multi-use pathways. Multi-use pathways, where approved, shall be ten feet wide and constructed of asphalt, concrete or other city-approved durable surface meeting ADA requirements consistent with the applicable city engineering standards.

RESPONSE: *A 6-foot-wide continuous ADA accessible concrete walkway is proposed along the face of the building connecting all entrances to the public right of way. No crosswalks are proposed.*

CHAPTER 15.90. PUBLIC FACILITIES

Sec. 15.90.010. Public facilities improvement.

Minor betterment, improvements, replacement or reconstruction of existing public facilities such as sewer and water lines, stormwater drainage facilities, sidewalks and other pedestrian ways or facilities, bikeways and similar public facilities within rights-of-ways and easements for the purposes existing on or before the effective date of this chapter, or on contiguous publicly-owned property designated, intended or utilized to support the facilities, or the facilities that are set forth within an adopted public facilities plan or other capital improvement plan duly adopted on or before the effective date of this ordinance, are exempt from permit requirements, unless specifically set forth otherwise.

Sec. 15.90.020. Developer responsibility for streets and other public facilities.

- A. *Duties of developer.* It shall be the responsibility of the developer to construct all streets, curbs, sidewalks, sanitary sewers, storm sewers, water mains, electric, telephone and cable television lines necessary to serve the use or development in accordance with the specifications of the city and/or the serving entity.
- B. *Over-sizing.* The city may require as a condition of development approval that sewer, water, or storm drainage systems serving new development be sized to accommodate future development within the area as projected by the applicable facility master plan, and the city may authorize other cost-recovery or cost-sharing methods as provided under state law.
- C. *Inadequate existing streets.* Whenever existing streets, adjacent to, within a tract or providing access to and/or from a tract, are of inadequate width and/or improvement standards, additional right-of-way and/or improvements to the existing streets may be required.

- D. *Half streets.* Half streets, while generally not acceptable, may be approved where essential to the reasonable development of a proposed land development, and when the city finds it will be practical to require dedication and improvement of the other half of the street when the adjoining property is developed. Whenever a half street exists adjacent to a tract of land proposed for development, the other half of the street shall be dedicated and improved.

Sec. 15.90.030. Sewer and water.

- A. *Sewer and water plan approval.* Development permits for sewer and water improvements shall not be issued until the public works director has approved all sanitary sewer and water plans in conformance with city standards.
- B. *Inadequate facilities.* Development permits may be restricted or rationed by the city where a deficiency exists in the existing water or sewer system that cannot be rectified by the development and which, if not rectified, will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of domestic water and sewerage treatment systems. The city may require water booster pumps, sanitary sewer lift stations, and other critical facilities be installed with backup power.

Sec. 15.90.040. Stormwater.

- A. *Accommodation of upstream drainage.* Culverts and other drainage facilities shall be large enough to accommodate existing and potential future runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the city engineer.
- B. *Effect on downstream drainage.* Where it is anticipated by the city engineer that the additional runoff resulting from the development will overload an existing drainage facility, the city shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with city standards.

Sec. 15.90.050. Utilities.

- A. *General provision.* The developer of a property is responsible for coordinating the development plan with the applicable utility providers and paying for the extension and installation of utilities not otherwise available to the subject property.
- B. *Underground utilities.* All new electrical, telephone or other utility lines shall be underground unless otherwise approved by the city.
- C. *Subdivisions.* In order to facilitate underground placement of utilities, the following additional standards apply to all new subdivisions:
1. The developer shall make all necessary arrangements with the serving utility to provide the underground services. Care shall be taken to ensure that no above ground equipment obstructs vision clearance areas for vehicular traffic.
 2. The city reserves the right to approve the location of all surface-mounted facilities.
 3. All underground utilities installed in streets must be constructed and approved by the applicable utility provider prior to the surfacing of the streets.
 4. Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.

- D. *Exception to undergrounding requirement.* The city may grant exceptions to the undergrounding standard where existing physical constraints, such as geologic conditions, streams, or existing development conditions make underground placement impractical.

RESPONSE: *The site is located in a developed industrial subdivision with a 60-foot right of way, 40-foot-wide street, water, sewer, storm drainage and franchise utilities installed. The proposed building will not place an undue burden on the existing facilities. We will be installing a 6-foot-wide sidewalk along the property frontage.*

Sec. 15.90.060. Public street/highway improvement.

The following public streets and highway improvement activities are permitted outright in all zones and are exempt from the permit requirements of this Development Code.

- A. Installation of additional and/or passing lanes, including pedestrian ways and/or bikeways, within a public street or highway right-of-way existing as of the effective date of this chapter, unless such adversely impacts on-street parking capacities and patterns.
- B. Reconstruction or modification of public roads and highways, not including the addition of travel lanes, where no removal or displacement of buildings would occur, and/or no new land parcels result.
- C. Temporary public road and highway detours that will be abandoned and restored to original condition or use at such time when no longer needed.
- D. Minor betterment of existing public roads and highway related facilities such as maintenance yards, weigh stations, waysides, and, rest areas within a right-of-way existing as of the effective date of this Development Code. In addition, also exempt are contiguous public-owned property utilized to support the operation and maintenance of public roads and highways provided such is not located within a duly designated residential zone, or adjacent to or across the street from a lot or parcel within such a zone.
- E. The construction, reconstruction, or modification of a public street or highway that is identified as a priority project in a transportation system plan (TSP) or the state transportation improvement plan (STIP) that was duly adopted on or before the effective date of this chapter.
- F. The design, construction, operation, and maintenance of a tourist-oriented or public wayside.

RESPONSE: *Streets and utilities are fully developed; we will be installing a 6-foot-wide sidewalk along the property frontage.*

Sec. 15.90.070. Design of streets and other public facilities.

- A. *Traffic circulation system.* The overall street system shall ensure an adequate traffic circulation system with intersection angles, grades, tangents and curves appropriate for the traffic to be carried considering the terrain of the development and the area. An analysis of the proposed traffic circulation system within the land division, and as such system and traffic generated therefrom affects the overall City of La Pine transportation, will be required to be submitted with the initial land division review application. The location, width and grade of streets shall be considered in their relationship to existing and planned streets, to topographical conditions, to public convenience and safety and to the proposed use or development to be served thereby.
- B. *Street location and pattern.* The proposed street location and pattern shall be shown on the development plan, and the arrangement of streets shall:
 - 1. Provide for the continuation or appropriate projection of existing principal streets in surrounding areas; or

2. Conform to a plan for the general area of the development approved by the city to meet a particular situation where topographical or other conditions make continuance or conformance to existing streets impractical; and
 3. Conform to the adopted La Pine Transportation System Plan as may be amended.
- C. *Access ways.* The city, in approving a land use application with conditions, may require a developer to provide an access way where the creation of a cul-de-sac or dead-end street is unavoidable and the access way connects the end of the street to another street, a park, or a public access way. Where an access way is required, it shall be not less than ten feet wide and shall contain a minimum six-foot-wide paved surface or other all-weather surface approved by the city. Access ways shall be contained within a public right-of-way or public access easement, as required by the city.
- D. *Future street extensions.* Where necessary to give access to or permit future subdivision or development of adjoining land, streets shall be extended to the boundary of the proposed development or subdivision. Where a subdivision is proposed adjacent to other developable land, a future street plan shall be filed by the applicant in conjunction with an application for a subdivision in order to facilitate orderly development of the street system. The plan shall show the pattern of existing and proposed future streets from the boundaries of the proposed land division and shall include other divisible parcels within 600 feet surrounding and adjacent to the proposed subdivision. The street plan is not binding, but is intended to show potential future street extensions with future development. The plan must demonstrate, pursuant to city standards, that the proposed development does not preclude future street connections to adjacent development land. Wherever appropriate, street stubs shall be provided to allow access to future abutting subdivisions and to logically extend the street system into the surrounding area. Street ends shall contain turnarounds constructed to Uniform Fire Code standards, as the city deems applicable, and shall be designed to facilitate future extension in terms of grading, width, and temporary barricades.
- E. *Minimum right-of-way and roadway widths.* Unless otherwise approved in the tentative development plan, street, sidewalk and bike rights-of-way and surfacing widths shall not be less than the minimum widths in feet set forth in the La Pine Transportation System Plan, and shall be constructed in conformance with applicable standards and specifications set forth by the city.
- F. *Sidewalks.* Unless otherwise required in this chapter or other city ordinances or other regulations, or as otherwise approved by the commission, sidewalks shall be required as specified in the La Pine Transportation System Plan. In lieu of these requirements, however, the city may approve a development without sidewalks if alternative pedestrian routes and facilities are provided.
- G. *Bike lanes.* Unless otherwise required in this chapter or other city ordinances or other regulations, bike lanes shall be required as specified in the La Pine Transportation System Plan, except that the planning commission may approve a development without bike lanes if it is found that the requirement is not appropriate to or necessary for the extension of bicycle routes, existing or planned, and may also approve a development without bike lanes in the streets if alternative bicycle routes and facilities are provided.
- H. *Culs-de-sac.* A cul-de-sac street shall only be used where the city determines that environmental or topographical constraints, existing development patterns, or compliance with other applicable city requirements preclude a street extension. Where the city determines that a cul-de-sac is allowed, all of the following standards shall be met:
1. The cul-de-sac shall not exceed a length of 400 feet, except where the city through a Type II procedure determines that topographic or other physical constraints of the site require a longer cul-de-sac. The length of the cul-de-sac shall be measured along the centerline of the roadway from the near side of the intersecting street to the farthest point of the cul-de-sac.
 2. A cul-de-sac shall terminate with a circular turn around with a minimum radius of 45 feet of paved driving surface and a 50 foot right-of-way and meeting the Uniform Fire Code.
 3. The cul-de-sac shall provide, or not preclude the opportunity to later install, a pedestrian and bicycle access way between it and adjacent developable lands.

- I. *Marginal access streets.* Where a land development abuts or contains an existing or proposed arterial street, the city may require marginal access streets, reverse frontage lots with suitable depth, screen-plantings contained in a non-access reservation strip along the rear or side property line or other treatments deemed necessary for adequate protection of residential properties and the intended functions of the bordering street, and to afford separation of through and local traffic.
- J. *Streets adjacent to railroad right-of-way.* Whenever a proposed land development contains or is adjacent to a railroad right-of-way, provisions may be required for a street approximately parallel to the ROW at a distance suitable for the appropriate use of land between the street and the ROW. The distance shall be determined with consideration at cross streets of the minimum distance required for approach grades to a future grade separation and to provide sufficient depth to allow screen planting or other separation requirements along the ROW.
- K. *Reserve strips.* Reserve strips or street plugs controlling access to streets will not be approved unless deemed necessary for the protection of public safety and welfare and may be used in the case of a dead-end street planned for future extension, and in the case of a half street planned for future development as a standard, full street.
- L. *Alignment.* All streets, as far as practicable, shall be in alignment with existing streets by continuations of the center lines thereof. Necessary staggered street alignment resulting in intersections shall, wherever possible, leave a minimum distance of 200 feet between the center lines of streets of approximately the same direction, and in no case shall the off-set be less than 100 feet.
- M. *Intersection angles.* Streets shall be laid out to intersect at angles as near to right angles as practicable, and in no case shall an acute angle be less than 80 degrees unless there is a special intersection design approved by the city engineer or other duly designated city representative as applicable. Other streets, except alleys, shall have at least 50 feet of tangent adjacent to the intersection, and the intersection of more than two streets at any one point will not be approved.
- N. *Curves.* Centerline radii of curves should not be less than 500 feet on major arterials, 300 feet on minor arterials, 200 feet on collectors or 100 feet on other streets and shall be on an even ten feet. Where existing conditions, particularly topography, make it otherwise impractical to provide building sites, the city may accept steeper grades and sharper curves than provided for herein in this subsection.
- O. *Street grades.* Street grades shall not exceed eight percent on arterials, ten percent on collectors and 12 percent on all other streets including private driveways entering upon a public street or highway; however, for streets at intersections, and for driveways entering upon a public street or highway, there should be a distance of three or more car lengths (approximately 50 feet) where the grade should not exceed six percent to provide for proper stopping distance during inclement weather conditions.
- P. *Street names.* Except for the extension of existing streets, no street names shall be used which will duplicate or be confused with the name of an existing street in the city or within a radius of six miles of the city or within the boundaries of a special service district such as fire or ambulance. Such street names shall be approved by the Deschutes County street name coordinator.
- Q. *Street name signs.* Street name signs shall be installed at all street intersections by the developer in accordance with applicable city, county or state requirements. One street sign shall be provided at the intersection of each street, and two street signs shall be provided at four-way intersections.
- R. *Traffic control signs.* Traffic control signs shall be provided for and installed by the developer as required and approved by the appropriate city, county and/or state agency or department.
- S. *Alleys.* Alleys are not necessary in residential developments, but may be required in commercial and industrial developments unless other permanent provisions for access to off-street parking and loading facilities are approved by the city.

- T. *Curbs.* Curbs shall be required on all streets in all developments, and shall be installed by the developer in accordance with standards set forth by the city unless otherwise approved by the city. Approval of streets without curbs shall be at the discretion of the city engineer, and shall be so determined during the tentative plan land division review process on the basis of special circumstances to the development.
- U. *Street lights.* Street lights may be required and, if so required, shall be installed by the developer in accordance with standards set forth by the city and the serving utility company. Streets lights, if required, shall include one fixture and be located at the intersection of streets.
- V. *Utilities.* The developer shall make necessary arrangements with the serving utility companies for the installation of all proposed or required utilities, which may include electrical power, natural gas, telephone, cable television and the like.
- W. *Drainage facilities.* Drainage facilities shall be provided as required by the city in accordance with all applicable city and Oregon Department of Environmental Quality standards.
- X. *Gates.* Except where approved as part of a master planned development, private streets and gated drives serving more than two dwellings (i.e., where a gate limits access to a development from a public street), are prohibited.

RESPONSE: *Streets and utilities are fully developed; we will be installing a 6-foot-wide sidewalk along the property frontage.*

Sec. 15.90.080. Traffic impact analysis.

- A. *Purpose.* The purpose of this subsection is [to] coordinate the review of land use applications with roadway authorities and to implement section 660-012-0045(2)(e) of the state Transportation Planning Rule, which requires the city to adopt a process to apply conditions to development proposals in order to minimize impacts and protect transportation facilities. The following provisions also establish when a proposal must be reviewed for potential traffic impacts; when a traffic impact analysis must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; the required contents of a traffic impact analysis; and who is qualified to prepare the analysis.
- B. *When a traffic impact analysis is required.* The city or other road authority with jurisdiction may require a traffic impact analysis (TIA) as part of an application for development, a change in use, or a change in access. A TIA shall be required where a change of use or a development would involve one or more of the following:
 1. A change in zoning or a plan amendment designation;
 2. Operational or safety concerns documented in writing by a road authority;
 3. An increase in site traffic volume generation by [300] average daily trips (ADT) or more;
 4. An increase in peak hour volume of a particular movement to and from a street or highway by [20] percent or more;
 5. An increase in the use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by ten vehicles or more per day;
 6. Existing or proposed approaches or access connections that do not meet minimum spacing or sight distance requirements or are located where vehicles entering or leaving the property are restricted, or such vehicles are likely to queue or hesitate at an approach or access connection, creating a safety hazard;
 7. A change in internal traffic patterns that may cause safety concerns; or
 8. A TIA required by ODOT pursuant to OAR 734-051.

- C. *Traffic impact analysis preparation.* A professional engineer registered by the State of Oregon, in accordance with the requirements of the road authority, shall prepare the traffic impact analysis.
- D. *Waiver or deferral.* The city may waive or allow deferral of standard street improvements, including sidewalk, roadway, bicycle lane, undergrounding of utilities, and landscaping, as applicable, where one or more of the following conditions in [subsections] 1 through 4 is met. Where the city agrees to defer a street improvement, it shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future:
1. The standard improvement conflicts with an adopted capital improvement plan.
 2. The standard improvement would create a safety hazard.
 3. It is unlikely due to the developed condition of adjacent property that the subject improvement would be extended in the foreseeable future, and the improvement under consideration does not by itself significantly improve transportation operations or safety.
 4. The improvement under consideration is part of an approved partition in the [RL or RM] and the proposed partition does not create any new street.

RESPONSE: *The proposed building has a projected ADT of 31 trips and does not increase any particular movement from a street or highway by 20 percent or more, there will not be more than 10 trips per day by vehicles exceeding 20,000 pounds. Box way is a straight and relatively flat road, the proposed access drive will not create a safety hazard.*

CHAPTER 15.94. IMPROVEMENT PROCEDURES AND GUARANTEES

Sec. 15.94.010. Improvement procedures.

Improvements to be installed by the developer, either as a requirement of this chapter, conditions of approval or at the developer's option as proposed as a part of the subject development proposal, shall conform to the following requirements:

- A. *Plan review and approval.* Improvement work shall not be commenced until plans therefor have been reviewed and approved by the city or a designated representative thereof. The review and approval shall be at the expense of the developer.
- B. *Modification.* Improvement work shall not commence until after the city has been notified and approval therefore has been granted, and if work is discontinued for any reason, it shall not be resumed until after the city is notified and approval thereof granted.
- C. *Improvements as platted.* Improvements shall be designed, installed and constructed as platted and approved, and plans therefore shall be filed with the final plat at the time of recordation or as otherwise required by the city.
- D. *Inspection.* Improvement work shall be constructed under the inspection and approval of an inspector designated by the city, and the expenses incurred therefore shall be borne by the developer. Fees established by the city council for such review and inspection may be established in lieu of actual expenses. The city, through the inspector, may require changes in typical sections and details of improvements if unusual or special conditions arise during construction to warrant such changes in the public interest.
- E. *Utilities.* Underground utilities, including, but not limited to, electric power, telephone, water mains, water service crossings, sanitary sewers and storm drains, to be installed in streets, shall be constructed by the developer prior to the surfacing of the streets.

- F. *As built plans.* As built plans for all public improvements shall be prepared and completed by a licensed engineer and filed with the city upon the completion of all such improvements. A copy of the as built plans shall be filed with the final plat of a subdivision or other development by and at the cost of the developer. The plans shall be completed and duly filed within 30 days of the completion of the improvements.

Sec. 15.94.020. Completion or assurance of improvements.

- A. *Agreement for improvements.* Prior to final plat approval for a subdivision, partition, PUD or other land development, or the final approval of a land use or development pursuant to applicable zoning provisions, where public improvements are required, the owner and/or developer shall either install required improvements and repair existing streets and other public facilities damaged in the development of the property, or shall execute and file with the city an agreement between him/herself and the city specifying the period in which improvements and repairs shall be completed and, providing that if the work is not completed within the period specified, that the city may complete the work and recover the full costs thereof, together with court costs and attorney costs necessary to collect the amounts from the developer. The agreement shall also provide for payment to the city for the cost of inspection and other engineer services directly attributed to the project.
- B. *Bond or other performance assurance.* The developer shall file with the agreement, to ensure his/her full and faithful performance thereof, one of the following, pursuant to approval of the city attorney and city manager, and approval and acceptance by the city council:
1. A surety bond executed by a surety company authorized to transact business in the State of Oregon in a form approved by the city attorney.
 2. A personal bond co-signed by at least one additional person together with evidence of financial responsibility and resources of those signing the bond sufficient to provide reasonable assurance of the ability to proceed in accordance with the agreement.
 3. Cash deposit.
 4. Such other security as may be approved and deemed necessary by the city council to adequately ensure completion of the required improvements.
- C. *Amount of security required.* The assurance of full and faithful performance shall be for a sum approved by the city as sufficient to cover the cost of the improvements and repairs, including related engineering, inspection and other incidental expenses, plus an additional 20 percent for contingencies.
- D. *Default status.* If a developer fails to carry out provisions of the agreement, and the city has unreimbursed costs or expenses resulting from the failure, the city shall call on the bond or other assurance for reimbursement of the costs or expenses. If the amount of the bond or other assurance deposit exceeds costs and expenses incurred by the city, it shall release the remainder. If the amount of the bond or other assurance is less than the costs or expenses incurred by the city, the developer shall be liable to the city for the difference plus any attorney fees and costs incurred.

RESPONSE: *A bond or cash deposit will be provided for the sidewalk improvements.*

Sec. 15.94.030. Building and occupancy permits.

- A. *Building permits.* No building permits shall be issued upon lots to receive and be served by sanitary, sewer and water service and streets as improvements required pursuant to this chapter unless the improvements are in place, serviceable and approved by the city, with the service connections fees paid, and accepted by the city.

- B. *Sale or occupancy.* All improvements required pursuant to this chapter and other applicable regulations or approval conditions shall be completed, in service and approved by the city, and accepted by the city council, prior to sale or occupancy of any lot, parcel or building unit erected upon a lot within the subdivision, partitioning, PUD or other development.

RESPONSE: *All services are installed to the site.*

Sec. 15.94.040. Maintenance surety bond.

Prior to sale and occupancy of any lot, parcel or building unit erected upon a lot within a subdivision, partitioning, PUD or other development, and as a condition of acceptance of improvements, the city will require a one-year maintenance surety bond in an amount not to exceed 20 percent of the value of all improvements, to guarantee maintenance and performance for a period of not less than one year from the date of acceptance.

RESPONSE: *All streets and services are installed.*

Sec. 15.94.050. Engineering/special services for review.

With regard to any development proposal for which the city deems it necessary to contract for engineering and/or other special technical services for the review thereof or for the design of facility expansions to serve the development, the developer may be required to pay all or part of the special services. In such cases, the choice of the contract service provider shall be at the discretion of the city, and the service provider shall perform the necessary services at the direction of the city. The costs for the services shall be determined reasonable, and an estimate of the costs shall be provided to the developer prior to contracting therefore [therefor].

RESPONSE: *Any additional contract review services will be paid for by the developer.*

CHAPTER 15.312. SITE PLAN REVIEW

Sec. 15.312.010. Purpose.

- A. The purpose of the site plan review provisions of this section [chapter] is to ensure that development within the city complies with standards and limitations set forth within the applicable zone, by other city standards and requirements and by applicable county, state and federal regulations.
- B. This broad purpose is furthered by the following specific purposes of site plan review:
1. To implement the goals and policies of the comprehensive plan.
 2. To foster development that is designed, arranged and constructed in a manner that provides a safe, efficient and aesthetically pleasing community asset.
 3. To encourage originality and creativity in site design, architecture and landscape design.
 4. To ensure that the arrangement of all functions, uses and improvements of a development reflect the natural amenities, capabilities and limitations of its site and adjacent areas.
 5. To encourage development where the various structures, use areas and site elements are integrated in a manner that is visually harmonious within the development and the surrounding area.

6. To encourage development and landscape design that complements the natural landscape and setting, improves the general appearance of the community and enhances specific elements of the man-made environment, both presently and historically.

Sec. 15.312.020. Applicability.

The following uses and development shall be subject to the provisions of this section:

- A. All new construction or new development except for: single-family residences (including manufactured dwellings, mobile homes, modular homes), duplexes, accessory dwelling units and related accessory structures unless provided otherwise in this chapter.
- B. An exterior alteration or modification to an existing nonresidential use or structure, which is subject to site plan review and/or is subject to regulation under the provisions of this chapter, except for painting, replacement of roofing and siding, and other normal maintenance and upkeep requirements which are not subject to regulation under the provisions of this chapter or any other applicable city, county, state and/or federal regulations.
- C. Any alteration or modification of site improvements, such as the landscaping, parking and/or loading facilities and areas, in conjunction with an existing nonresidential use which is subject to site plan review and/or is subject to regulation under the provisions of this chapter.

Sec. 15.312.025. Site plan approval required.

Site plan review and approval, as specified by this chapter, shall be required prior to the following:

- A. Site clearance activities such as grading, excavation or filling for any use or development requiring a permit pursuant to this Development Code.
- B. The issuance of a building or development permit for any use or development requiring city approval pursuant to this Development Code.

Sec. 15.312.030. Procedure type.

- A. Site plan review applications are subject to Type II review in accordance with the procedures in article 7 unless elevated to a Type III review at the discretion of the planning official.
- B. Pre-application conference. Prior to applying for site plan approval, applicants should and may meet with the city planning official, building official and public works director, or designees thereof, and present a preliminary plan which shall contain, in an approximate manner, the information required on a site plan review application.
 1. The purpose of the preliminary site plan review is to enable the applicant to obtain advice from the city as to the intent, standards, criteria and provisions of this chapter, this Development Code, other city ordinances, standards and regulations, and state and federal rules and regulations which may be pertinent to the proposal.
 2. Information presented for preliminary discussion shall be considered confidential if so requested by the applicant.

Sec. 15.312.040. Submittal requirements.

A property owner or authorized representative thereof may initiate a request for site plan review by filing an application with the city using forms prescribed by the city together with the required filing fee in accordance with the Type II application requirements in article 7. In addition to the information required for a Type II review (see article 7), the applicant shall submit that which is listed below.

- A. *Requirements for information to be submitted.* Information provided on the site plan shall conform to the following. The number of copies required shall be as specified on the application form.
1. Drawings depicting the proposal shall be presented on sheets not larger than 24 inches by 36 inches in the number of copies directed by the city.
 2. To facilitate public reviews and notice, at least one copy of the proposal shall be provided on a sheet of paper not larger than 11 inches by 17 inches.
 3. Drawings shall be at a scale sufficiently large enough to enable all features of the design to be clearly discerned and shall include a north arrow and scale.
 4. The city may require that the drawing, development plan or other information be provided to the city on computer disk in a format adaptable to the city's computer systems.

RESPONSE: *Site plan is on 26 x 36 inch sheets, and 11 x 17 will be provided. Drawings are at a 1" = 10' scale, with North arrow shown.*

- B. *Site analysis diagram.* If required by the city planning official, this element of the site plan, which may be in schematic or free hand form to scale, shall indicate the following site characteristics:
1. Location and species of existing trees greater than six inches in diameter when measured four feet above the natural grade, and an indication of which trees are proposed to be removed.
 2. On sites that contain steep slopes, potential geological hazard or unique natural features that may affect the proposed development, the city may require contours mapped at two-foot intervals.
 3. Natural drainage ways, depths of any ground water tables less than 12 feet, any areas of surface water accumulations and any other significant natural features.
 4. The location and width of all public and private streets, drives, sidewalks, pathways, rights-of-way, and easements on the site and adjoining the site, and all buildings, utilities, retaining walls, and other man-made features, both existing and proposed.
 5. Natural features, including trees, riparian habitat and stream channels and structures on-site or on adjoining properties that have or may have a visual or other significant relationship with the site and the proposed development thereon.

RESPONSE: *Locations of existing trees are shown on the site plan, the existing lot is flat with no natural drainage ways. All existing streets, right of ways and utilities are shown on plans. All natural features are shown on site plan.*

- B. *Site photographs.* Photographs depicting the site and its relationship to adjoining sites and the general area are extremely valuable, should be provided, and may be required by the city planning official.

RESPONSE: *Street view photographs are shown on site plan.*

- D. *Site development plan.* The site plan shall indicate the following:
1. Legal description of the property.
 2. Boundary dimensions and site area.
 3. Location and sizes of existing and proposed utilities, including water lines, sewer lines, hydrants, etc.
 4. Location of all existing and proposed structures, including distances from the property lines.
 5. Area of the site to be covered by structures, existing and proposed, and the percentage of site coverage thereby.
 6. All external dimensions of existing and proposed buildings and structures.

7. Location of building entrances and exits.
8. Access drives, parking and circulation areas, including their dimensions.
9. Service areas and delivery circulation plan for such uses as the loading and delivery of goods.
10. Locations, descriptions and dimensions of easements as may be applicable.
11. Grading and drainage plans and calculations, including spot elevations and contours at intervals close enough to convey their meaning.
12. Location of areas to be landscaped, including designated landscape material/plant types and sizes.
13. Outdoor recreation and/or play areas.
14. Pedestrian and bicycle circulation, including existing and proposed on-site and off-site sidewalks.
15. Location of mechanical equipment not enclosed within a building, garbage disposal areas, utility appurtenances and similar structures.
16. Exterior lighting and fencing.
17. Location, size and method of illumination of signs.
18. Provisions for handicapped persons.
19. Other site elements which will assist in the evaluation of site development.
20. Location, names, surface and right-of-way widths and improvement standards of all existing and proposed streets within or adjacent to the proposed development.
21. Location of areas designated for snow storage, in accordance with the requirements of section 18.86.060 [15.86.060], and calculations of the area required by the minimum standard and the proposed area.
22. Information necessary to demonstrate compliance with [the] fire code, including, but not limited to, fire flow, apparatus access, and hydrant spacing.

RESPONSE: *Site plan indicates all applicable provisions of this section.*

- E. *Accompanying written summary.* In addition to the foregoing site development plan requirements, a written summary of the proposal should be provided and may be required showing the following, (unless such is shown on the site development plan):
 1. *Commercial and nonresidential development.* For commercial and nonresidential development:
 - a. The square footage contained in the site area to be developed.
 - b. The percentage of the area to be covered by structures when developed.
 - c. The percentage of the area to be covered by parking areas and the total number of parking spaces.
 - d. The total square footage of all landscaped areas, including the percentage consisting of natural materials and the percentage of hard surfaced areas such as courtyards.
 - e. Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the city planning official). A traffic impact analysis may be required in accordance with section 15.90.080.

RESPONSE: *Written Narrative has been provided. A trip generation letter has been provided.*

2. *Residential development.* For residential development:
 - a. The total square footage of the lot or parcel and in the structures in the development.
 - b. The number of dwelling units in the development (include the units by the number of bedrooms in each unit, for example, ten one-bedroom, 25 two-bedroom and the like).
 - c. Percentage of lot coverage by structures, way areas, recreation areas and landscaping.
 - e. [d.] Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the city planning official). A traffic impact analysis may be required in accordance with section 15.90.080.

RESPONSE: *This is not a residential application.*

- F. *Landscape plan.* If required by the city planning official, a landscape plan shall be submitted and shall indicate the following.
 1. The size, species and locations of plant materials to be retained or placed on-site.
 2. The layout of irrigation facilities.
 3. Location and design details of walkways, plazas, courtyards and similar areas.
 4. Location, type and intensity of outdoor lighting.
 5. Location and design details of proposed fencing, retaining walls and trash collection areas.
 6. Other information as deemed appropriate by the review authority. An arborist's report may be required for sites with mature trees that are to be retained and protected.

RESPONSE: *A Landscape plan has been submitted.*

- G. *Architectural drawings.* This element of the site plan review, if required by the city planning official, shall indicate the following:
 1. A plan specifying the building footprint and dimensions, including all points of access. Floor plans of interior spaces to the extent required to clarify access functions and the relationship of the spaces to decks, porches, balconies and stairs or other features shown on the building elevations. The floor plans shall be provided for all building floors and shall include appropriate dimensions.
 2. Exterior elevations showing building heights, windows, doors, exterior light fixtures, stairways, balconies, decks and other architectural details. These elevations shall be provided for every exterior wall surface, including those which are completely or partially concealed from view by overlapping portions of the structure. Existing and finished grades at the center of all walls shall be shown with elevations of floors indicated and a dimension showing compliance with height limitations.
 3. Location and type of exterior light fixtures, including the lamp types and the levels of illumination that they provide.
 4. Location, size and method of illumination of all exterior signs.

RESPONSE: *Architectural plans have been submitted.*

- H. *Property survey.* A survey of the property by a licensed land surveyor may be required, and if required the survey shall clearly delineate property boundaries, and show the location of the corners of proposed buildings and other significant features proposed for the site. The requirement for a survey of the exterior boundaries of a site may be waived where it is found that there is a recent survey that can be used to clearly establish the applicant's property boundaries.
- I. *Deed restrictions.* Copies of all existing and proposed restrictions or covenants, including those for roadway access control.

- J. *Narrative.* A written narrative addressing the applicable criteria listed [in] section 15.312.050 for residential development and sections 15.312.050 and 15.312.060 for nonresidential development.
- K. *Other information as determined by the city planning official.* The city planning official may require studies or exhibits prepared by qualified professionals to address specific site features or project impacts (e.g., traffic, noise, environmental features, natural hazards, etc.), as necessary to determine a proposal's conformance with this Development Code.

RESPONSE: *Property has been surveyed for topographic features.*

Sec. 15.312.050. Approval criteria - all residential and nonresidential.

To ensure that the stated purposes of the site plan review process are met, the review authority shall be governed by the criteria below as they evaluate and render a decision on a proposal.

- A. *Statement of intent.*
 - 1. The site plan review criteria are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the city with a means of reviewing proposed plans.
 - 2. These criteria provide a clear and objective means of evaluating residential development (and the residential components of a mixed use development) in accordance with ORS 197.
 - 3. The review authority is not authorized as a part of the site plan review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this Development Code.
- B. *Site plan evaluation criteria.* The following criteria shall be used in evaluating all site development plans:
 - 1. The application is complete, in accordance with the applicable procedures in article 7.
 - 2. The application complies with all applicable provisions of the underlying zoning district in article 3, including, but not limited to, setbacks, lot dimensions, density, lot coverage, building height, and other applicable standards.
 - 3. The application complies with the provisions of the any applicable overlay zones in article 4.
 - 4. The proposal complies with all applicable development and design standards of article 5.
 - 5. The application complies with all applicable special use standards in article 6.
 - 6. Adequate public facilities and utilities are available or can be made prior to occupancy to serve the proposed development.
 - 7. The proposed site plan conforms to the standards within the adopted La Pine Transportation System Plan (TSP), as may be amended from time to time, unless other design standards are specifically approved by the city.
 - 8. The proposed site plan conforms to the La Pine Sewer and Water Standards, as may be amended from time to time, unless other design standards are specifically approved by the city. All sewer improvements must comply with Oregon Administrative Rules chapter 340 division 52 requirements, including Appendix A - Sewer Pipelines.
 - 9. The proposed site plan conforms to the Central Oregon Stormwater Manual (COSM), as may be amended from time to time, unless other design standards are specifically approved by the city.
 - 10. All utilities shall be installed underground, unless otherwise specifically approved by the city.

11. The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.

RESPONSE: *The proposed building and site design complies with all development code requirements, La Pine TSP and COSM requirements.*

Sec. 15.312.060. Additional approval criteria - nonresidential development.

In addition to the approval criteria in section 15.312.050, to ensure that the stated purposes of the site plan review process are met, the review authority shall also be governed by the criteria below as they evaluate and render a decision on a nonresidential development proposal.

A. *Statement of intent.*

1. The site plan review criteria for nonresidential development are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the city with a means of reviewing proposed plans.
2. These criteria are not intended to be inflexible requirements, nor are they intended to discourage creativity. The specification of one or more architectural styles is not intended by these criteria.
3. The review authority is not authorized as a part of the design review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this chapter.

B. *Site plan evaluation criteria.* In addition to the approval criteria in section 15.312.050, the following criteria shall be used in evaluating nonresidential site development plans:

1. The arrangement of all functions, uses and improvements has been designed so as to reflect and harmonize with the natural characteristics and limitations of the site and adjacent sites.
2. In terms of setback from streets or sidewalks, the design creates a visually interesting and compatible relationship between the proposed structures and/or adjacent structures.
3. The design incorporates existing features, such as streams, rocks, slopes, vegetation and the like, as part of the overall design.
4. Where appropriate, the design relates or integrates the proposed landscaping/open space to the adjoining landscape/open space in order to create a pedestrian/bike pathway and/or open system that connects several properties or uses.
5. The arrangement of the improvements on the site do not unreasonably degrade the scenic values of the community and the surrounding area in particular.
6. Where appropriate, the design includes a parking and circulation system that encourages a pedestrian and/or bicycle rather than vehicular orientation, including a separate service area for delivery of goods.
7. The design gives attention to the placement of storage, mechanical equipment, utilities or waste collection facilities so as to screen such from view, both from within and from outside the site.

RESPONSE: *The site has been designed to meet all development code requirements for setback and to harmonize with the natural characteristics, there are no existing streams or rock features. Building has been laid out in an orientation that works with the existing lot dimensions. A trash enclose is being provided for waste collection facilities, any mechanical units will be screened from view.*

C. *Landscape design evaluation criteria.* The following criteria shall be used in evaluating landscape plans:

1. The overall design substantially complements the natural environment of the city and the character of the site and the surrounding area.
2. The design acknowledges the growing conditions for this climatic zone, and the unique requirements that its specific site location makes upon plant selection.
3. Provision has been made for the survival and continuous maintenance of the landscape and its vegetation.
4. The design contributes to the stabilization of slopes and the protection of other natural features and resources where applicable.

RESPONSE: *A landscape plan has been provided that meet these requirements.*

Sec. 15.312.070. Conditions of approval.

In addition to the standards and conditions set forth in a specific zone (if found to be necessary and supported with adequate findings), additional conditions may be imposed by the city which are found to be necessary to avoid a detrimental impact on adjoining properties, the general area or the city as a whole, and to otherwise protect the general welfare and interests of the surrounding area. The conditions may include, but are not limited to, the following:

- A. Limiting the manner in which the use is conducted, including restricting the time an activity may take place, and restrictions to minimize environmental impacts such as noise, vibration, air or water pollution, glare and odor.
- B. Establishing a special setback or other open space requirements, and increasing the required lot size or other dimensional standards.
- C. Limiting the height, size or location of a building or other structure or use.
- D. Increasing street width and/or requiring improvements to public streets and other public facilities serving the proposed use, even including those off-site but necessary to serve the subject proposal.
- E. Designating the size, number, improvements, location and nature of vehicle access points and routes, and requiring pedestrian and/or bicycle ways.
- F. Limiting or otherwise designating the number, size, location, height and lighting of signs and outdoor or security lighting, and the intensity and/or direction thereof.
- G. Requiring screening, fencing or other improvements or facilities deemed necessary to protect adjacent or nearby properties, and establishing requirements or standards for the installation and maintenance thereof.
- H. Protecting and preserving existing trees, other vegetation and water, scenic, historic, archaeological, unique, landmark or other natural or man-made significant resources.

RESPONSE: *Any additional conditional of approval will be addressed at time of issuance.*

INCOMPLETE LETTER

October 23, 2025

Jesse Bath
2660 NE Hwy 20 Suite 610-2
Bend, OR 97701

SUBJECT: SPR3-25-0003, BP Holdings, 16630 Box Way, La Pine, OR 97739

Dear Applicant,

The City of La Pine received a Site Plan Review application on September 24, 2025 for the above referenced property. In reviewing the application against the requirements of the La Pine Development Code and State Statue, City staff has determined the application to be **INCOMPLETE** as of October 23, 2025.

To ensure a complete application is reviewed, staff requests the following additional information that relates to La Pine Development Code and State Statue.

The narrative submitted with the application is severely lacking in addressing the applicable criteria, resulting in a lack of evidence towards compliance. Due to the limited amount of information provided for staff to review, a full in-depth completeness review has not been completed. Criteria has been listed below for your convenience, but due to the insufficient nature of the application, the staff responses are limited in terms of guidance towards compliance.

PART III, CITY OF LA PINE DEVELOPMENT CODE

ARTICLE 5 - DEVELOPMENT STANDARDS

CHAPTER 15.82. - LANDSCAPING, BUFFERING AND FENCES

Sec. 15.82.010. - Landscaping and buffering requirements.

(***)

- 1. Retention of trees. As part of a landscaping plan, the developer shall also provide a tree plan identifying the location and diameter breast height (DBH) of all trees on the property. Existing mature trees with at least a 10-inch DBH shall be retained to the furthest extent possible. Development shall conform to the natural environment to incorporate existing mature trees. Where trees must be removed for proposed development, they shall be replaced at a 1:1 ratio on the subject lot or parcel with trees of similar species that are native to the region, and with a DBH of at least 5-inches or larger. Replacement trees shall meet the maintenance and plant*

survival criteria of this code section. Any trees planted must maintain a 20-foot radius for spacing. The 20-foot radius spacing shall be the determining factor in how many trees are replaced on the subject property. If more trees must be planted than what the 20-foot radius spacing would allow, then the number of trees replaced shall be reduced to meet the spacing standard.

STAFF COMMENT: The narrative failed to address the above criteria. Please address this in an updated landscape plan. *There are 15 trees 10-inches or larger being removed, based on the 20-foot radius spacing 13 trees will be replaced.*

CHAPTER 15.86. - PARKING AND LOADING

Sec. 15.86.010. - Applicability.

Off-street loading and vehicle and bicycle parking spaces shall be provided in accordance with the specifications of this chapter in all zones whenever any new use is established, an existing use is enlarged, or an existing use of land or structure is changed to a new use. Such new, enlarged, or changed use shall fully comply with the specifications of this chapter prior to being given a certificate of use and occupancy.

Sec. 15.86.020. - Off-street loading.

- A. Every commercial and industrial use which requires the receipt or distribution of material or merchandise by trucks with a 40-foot or longer wheelbase at a frequency of one or more vehicles per week shall provide off-street loading spaces in sufficient number to adequately serve the number and frequency of vehicle shipping and receiving projected for the use. The applicant shall provide supporting evidence of the projected shipping and receiving and how the number of spaces to be provided will be adequate.*
- B. Where an off-street loading space is required, it shall be large enough to accommodate the largest vehicle that is expected to serve the use without obstructing vehicles or pedestrian traffic on adjacent streets and driveways. Each off-street loading space shall not be less than 12 feet wide by 55 feet long unless otherwise approved by the city through site design review.*
- C. Off-street loading space(s) shall also have adequate adjacent area for vehicle maneuvering so that vehicles using the space(s) are not required to back-up onto or back-up from a public street or alley to use the space. Where parking areas are prohibited between a building and the street, loading areas are also prohibited.*
- D. Exceptions and adjustments. The city, through site design review, may approve a loading area adjacent to or within a street right-of-way where it finds that loading and unloading operations are short in duration (i.e., less than one hour), infrequent, do not obstruct traffic during peak traffic hours, do not interfere with emergency response services, and are acceptable to the applicable roadway authority.*

Sec. 15.86.030. - Off-street parking - required.

- A. Location of off-street loading and parking spaces. Except as otherwise permitted by this Development Code, required off-street loading and parking spaces shall be located on the same lot with the principal use they are intended to serve. In no case shall a required loading space be part of the area used to satisfy the parking requirements and vice versa. Also, in no case shall*

the required loading or parking space(s) of one use be used to satisfy the loading or parking space requirements of another use.

- B. Encroachment or reduction. A required loading or parking space shall not be encroached upon by a structure, storage, or other use, nor shall the number of spaces be reduced without replacement of a commensurate number of spaces in accordance with this section unless a special exception or variance has been approved.*
- C. Calculations of amounts of required and allowed parking.*
 - 1. When computing parking spaces based on floor area, parking structures and non-leasable floor spaces, such as storage closets, mechanical equipment rooms, and similar spaces, are not counted.*
 - 2. The number of parking spaces is computed based on the primary uses on the site except as stated in subsection 3, below. When there are two or more separate primary uses on a site, the minimum and maximum parking for the site is the sum of the required or allowed parking for the individual primary uses. For shared parking, see subsection I below.*
 - 3. When more than 20 percent of the floor area on a site is in an accessory use, the required or allowed parking is calculated separately for the accessory use. An example would be a 10,000 square foot building with a 7,000 square foot warehouse and a 3,000 square foot accessory retail area. The minimum and maximum parking would be computed separately for the retail and warehouse uses.*
- D. Use of required parking spaces. Except as otherwise provided by this section, required parking spaces must be available for residents, customers, or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for shared parking pursuant to subsection I.*
- E. Improvement of parking areas. Motorized vehicle parking is allowed only on streets with an improved shoulder of sufficient width; within garages, carports, and other approved structures; and on driveways or parking lots that have been developed in conformance with this Development Code.*
- F. Minimum number of off-street automobile parking spaces. Except as required for Americans with Disabilities Act compliance under subsection L, off-street parking shall be provided pursuant to one of the following three standards:*
 - 1. The standards in Table 15.86-1;*
 - 2. A standard from Table 15.86-1 for a use that the planning official determines is similar to the proposed use. For uses not specified in the table, the city shall determine parking based on submission of technical data from applicant or city sources; or*
 - 3. Subsection (H), parking exceptions, which includes a parking demand analysis option.*
- G. Maximum number of off-street automobile parking spaces. The following standards for maximum number of automobile parking spaces promote efficient use of land and compact development patterns.*

1. *Applicability. Developments subject to site plan review must conform to the maximum parking standards.*
 2. *Standards. Unless otherwise approved by the city through site plan review, the maximum number of off-street automobile parking spaces allowed for a commercial development equals the minimum number of required spaces, pursuant to Table 15.86-1 times a factor of 2.0. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements.*
- H. *Exceptions and reductions to off-street parking. An applicant may propose a parking standard that is different than the standards under subsections F or G, for review and action by the planning official through a Type II procedure. The applicant's proposal shall consist of a written request and a parking analysis prepared by a qualified professional. The parking analysis, at a minimum, shall assess the average parking demand and available supply for existing and proposed uses on the subject site; opportunities for shared parking with other uses in the vicinity; existing public parking in the vicinity; transportation options existing or planned near the site, such as frequent bus service, carpools, or private shuttles; and other relevant factors. The number of required off-street parking spaces may also be reduced through the provision of shared parking, pursuant to subsection I.*
- I. *Shared parking. Required parking facilities for two or more uses, structures, or parcels of land may be satisfied by the same parking facilities used jointly, to the extent that the owners or operators show that the need for parking facilities does not materially overlap (e.g., uses primarily of a daytime versus nighttime nature; weekday uses versus weekend uses), and, provided that the right of joint use is evidenced by a recorded deed, lease, contract, or similar written instrument establishing the joint use. Shared parking requests shall be subject to review and approval through site plan review.*
- J. *Parking stall design and minimum dimensions. Where a new off-street parking area is proposed, or an existing off-street parking area is proposed for expansion, the entire parking area shall be improved in conformance with this Development Code. At a minimum the parking spaces and drive aisles shall be paved with asphalt, concrete, or other city-approved materials, provided the Americans with Disabilities Act requirements are met, and shall conform to the minimum dimensions in Table 15-86-2 and the figures below. All off-street parking areas shall contain wheel stops, perimeter curbing, bollards, or other edging as required to prevent vehicles from damaging buildings or encroaching into walkways, landscapes, or the public right-of-way. Parking areas shall also provide for surface water management.*

Table 15.86-2. Parking Stall Dimensions

<i>Parking Angle</i>	<i>Stall Width</i>	<i>20' Stall</i>	<i>Aisle Width (*one way)</i>	<i>Curb Length</i>	<i>Bay Width</i>
0°	9'-0"	9.0	12.0	22.0	30.0
	9'-6"	9.5	12.0	22.0	31.0
	10'-0"	10.0	12.0	22.0	31.0
45°	9'-0"	19.8	13.0	12.7	52.5
	9'-6"	20.1	13.0	13.4	53.3
	10'-0"	20.5	13.0	14.1	54.0
60°	9'-0"	21.0	18.0	10.4	60.0

	9'-6"	21.2	18.0	11.0	60.4
	10'-0"	21.5	18.0	11.9	61.0
70°	9'-0"	21.0	19.0	9.6	61.0
	9'-6"	21.2	18.5	10.1	60.9
	10'-0"	21.2	18.0	10.6	60.4
90°	9'-0"	20.0	24.0	9.0	64.0
	9'-6"	20.0	24.0	9.5	64.0
	10'-0"	20.0	24.0	10.0	64.0
<i>*24' minimum for two-way traffic</i>					

K. *Adjustments to parking area dimensions. The dimensions in subsection (J) are minimum standards. The city planning official, through a Type II procedure, may adjust the dimensions based on evidence that a particular use will require more or less maneuvering area.*

L. *Americans with Disabilities Act (ADA). Parking shall be provided consistent with ADA requirements, including, but not limited to, the minimum number of spaces for automobiles, van-accessible spaces, location of spaces relative to building entrances, accessible routes between parking areas and building entrances, identification signs, lighting, and other design and construction requirements.*

Sec. 15.86.040. - Drive-up and drive-through uses and facilities.

A. *Purpose. Where drive-up or drive-through uses and facilities are allowed, they shall conform to all of the following standards, which are intended to calm traffic, provide for adequate vehicle queuing space, prevent automobile turning movement conflicts, and provide for pedestrian comfort and safety.*

B. *Standards. Drive-up and drive-through facilities (i.e., driveway queuing areas, customer service windows, teller machines, kiosks, drop-boxes, or similar facilities) shall meet all of the following standards:*

1. *The drive-up or drive-through facility shall orient to and receive access from a driveway that is internal to the development and not a street, as generally illustrated.*
2. *The drive-up or drive-through facility shall not be oriented to street corner.*
3. *The drive-up or drive-through facility shall not be located within 20 feet of a street right-of-way.*
4. *Drive-up and drive-through queuing areas shall be designed so that vehicles will not obstruct any street, fire lane, walkway, bike lane, or sidewalk.*
5. *All drive up and drive thru facilities must be permanently affixed to the ground and be connected to the City Water and Sewer in accordance with the standards of chapter 15.90.*

Sec. 15.86.050. - Bicycle parking.

A. *Exemptions. This section does not apply to single-family and duplex housing, home occupations, and agricultural uses. The planning official may exempt other uses upon finding that, due to the*

nature of the use or its location, it is unlikely to have any patrons or employees arriving by bicycle.

- B. Standards. Bicycle parking spaces shall be provided with new development and, where a change of use occurs, at a minimum, shall follow the standards in Table 15.86-3. Where an application is subject to conditional use permit approval or the applicant has requested a reduction to an automobile-parking standard, the city may require bicycle parking spaces in addition to those in Table 15.86-3.*

Table 15.86-3. Minimum Required Bicycle Parking Spaces	
<i>Use</i>	<i>Minimum Number of Spaces</i>
<i>Muli-family residential (not required for parcels with fewer than 4 dwelling units)</i>	<i>2 bike spaces per 4 dwelling units</i>
<i>Commercial</i>	<i>2 bike spaces per primary use or 1 per 5 vehicles spaces, whichever is greater</i>
<i>Industrial</i>	<i>2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater</i>
<i>Community Service</i>	<i>2 bike spaces</i>
<i>Parks (active recreation areas only)</i>	<i>4 bike spaces</i>
<i>Schools (all types)</i>	<i>2 bike spaces per classroom</i>
<i>Institutional uses and places of worship</i>	<i>2 bike spaces per primary is or 1 per 10 vehicle spaces, whichever is greater</i>
<i>Other uses</i>	<i>2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater</i>

- C. Design. Bicycle parking shall consist of staple-design steel racks or other city-approved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle. At a minimum, bicycle parking facilities shall be consistent with the following design guidelines:*
- 1. All bicycle parking shall be within 100 feet from a building entrance and located within a well-lit and clearly visible area;*
 - 2. Bicycle parking shall be convenient and easy to find. Where necessary, a sign shall be used to direct users to the parking facility;*
 - 3. Each bicycle parking space shall be at least two feet by six feet with a vertical clearance of six feet;*
 - 4. An access aisle of at least five feet shall be provided in each bicycle parking facility;*
 - 5. Bicycle parking facilities shall offer security in the form of either a lockable enclosure in which the bicycle can be stored or a stationary object, i.e., a "rack," upon which the bicycle can be locked. Structures that require a user-supplied lock shall accommodate both cables and U-shaped locks and shall permit the frame and both wheels to be secured (removing the front wheel may be necessary). Note: businesses may provide long-term, employee parking by allowing access to a secure room within a building.*
- D. Hazards. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall be located so as to not conflict with the vision clearance standards of [section 15.88.040](#).*

Sec. 15.86.060. - Snow storage areas.

- A. *Purpose.* The purpose of these standards is to ensure that adequate space is be provided within a development for storage of snow in winter months in order to accommodate space needed for access, circulation, and off-street parking.
- B. *Applicability.* Snow storage standards apply to all subdivisions and to developments subject to site plan review.
- C. *Standards.*
 - 1. *Minimum area.* Snow storage areas must be designated on a site plan. The areas must total a minimum of 15 percent of the area to be cleared, including all access drives, parking areas, and walkways.
 - 2. *Location.* Snow storage is not permitted on landscaped areas, except where these areas are limited to grass or rock cover. Snow storage may be permitted in parking areas, provided that the site can still accommodate enough parking spaces to meet minimum off-street parking requirements in winter months. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements. It is encouraged that snow storage areas be located away from public view and that additional impervious surface areas are not created for the sole purpose of snow storage.
 - 3. *Exceptions and adjustments.* The city may reduce or eliminate the required snow storage areas if a snow removal plan is presented which provides a continuous guarantee of removal.

STAFF COMMENT: The parking and loading criteria was not addressed in the narrative. At first glance, it appears the parking provided for the proposal is severely limited. When planning for parking, it will be important to provide enough parking for all four units separately. Although no use is proposed currently, when a new use is established within the building, or when one business leaves and another takes their place, a zoning permit will be required with the city. Through that zoning permit, staff will be confirming that the building contains enough parking for all four uses combined. If there is insufficient parking, the site will need to be modified to meet the minimum parking standards before the new or change of use can be established. Other incubator buildings previously reviewed typically prepare for the worst case scenario, identifying the uses within the industrial zone that require the most parking, and planning for that.

Response: There are a number of uses in the industrial category, for parking we are using one space per 1,000 sq. ft. of floor area, the proposed building is 6,400 sq. ft. / 1,000 = 6.4 spaces, we are providing 9 parking spaces.

CHAPTER 15.88. - ACCESS AND CIRCULATION

Sec. 15.88.010. - Purpose.

Chapter 15.88 contains standards for vehicular and pedestrian access, circulation, and connectivity. The standards promote safe, reasonably direct, and convenient options for walking and bicycling, while accommodating vehicle access to individual properties, as needed.

Sec. 15.88.020. - Applicability.

Chapter 15.88 applies to new development and changes in land use necessitating a new or modified street or highway connection. Except where the standards of a roadway authority other than the

city supersede city standards, [chapter 15.88](#) applies to all connections to a street or highway, and to driveways and walkways.

Sec. 15.88.030. - Vehicular access and circulation.

- A. *Purpose and intent.* [Section 15.88.030](#) implements the street access guidelines of the City of La Pine Transportation System Plan. It is intended to promote safe vehicle access and egress to properties, while maintaining traffic operations in conformance with adopted standards. "Safety," for the purposes of this chapter, extends to all modes of transportation.
- B. *Permit required.* Vehicular access to a public street (e.g., a new or modified driveway connection to a street or highway) requires an approach permit approved by the applicable roadway authority.
- C. *Traffic study requirements.* The city, in reviewing a development proposal or other action requiring an approach permit, may require a traffic impact analysis, pursuant to [section 15.90.080](#), to determine compliance with this Development Code.
- D. *Approach and driveway development standards.* Access management restrictions and limitations consist of provisions managing the number of access points and/or providing traffic and facility improvements that are designed to maximize the intended function of a particular street, road or highway. The intent is to achieve a balanced, comprehensive program which provides reasonable access as new development occurs while maintaining the safety and efficiency of traffic movement. Intersections, approaches and driveways shall conform to access spacing guidelines in the City of La Pine Transportation System Plan and the roadway authority's engineering standards. In the review of all new development, the reviewing authority shall consider the following techniques or considerations in providing for or restricting access to certain transportation facilities.
 - 1. *Access points to arterials and collectors may be restricted through the use of the following techniques:*
 - a. *Restricting spacing between access points based on the type of development and the speed along the serving collector or arterial.*
 - b. *Sharing of access points between adjacent properties and developments.*
 - c. *Providing access via a local order of street; for example, using a collector for access to an arterial, and using a local street for access to a collector.*
 - d. *Constructing frontage or marginal access roads to separate local traffic from through traffic.*
 - e. *Providing service drives to prevent overflow of vehicle queues onto adjoining roadways.*
 - 2. *Consideration of the following traffic and facility improvements for access management:*
 - a. *Providing of acceleration, deceleration and right-turn-only lanes.*

- b. *Offsetting driveways to produce T-intersections to minimize the number of conflict points between traffic using the driveways and through traffic.*
 - c. *Installation of median barriers to control conflicts associated with left turn movements.*
 - d. *Installing side barriers to the property along the serving arterial or collector to restrict access width to a minimum.*
- E. *ODOT approval. Where a new approach onto a state highway or a change of use adjacent to a state highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The city may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case the city will work cooperatively with the applicant and ODOT to avoid unnecessary delays.*
- F. *Other agency approval. Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.*
- G. *Exceptions and adjustments. The city may approve adjustments to the spacing standards of subsections above, where an existing connection to a city street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance.*
- H. *Joint use access easement and maintenance agreement. Where the city approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use of and cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed, defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the city for its records, but the city is not responsible for maintaining the driveway or resolving any dispute between property owners.*

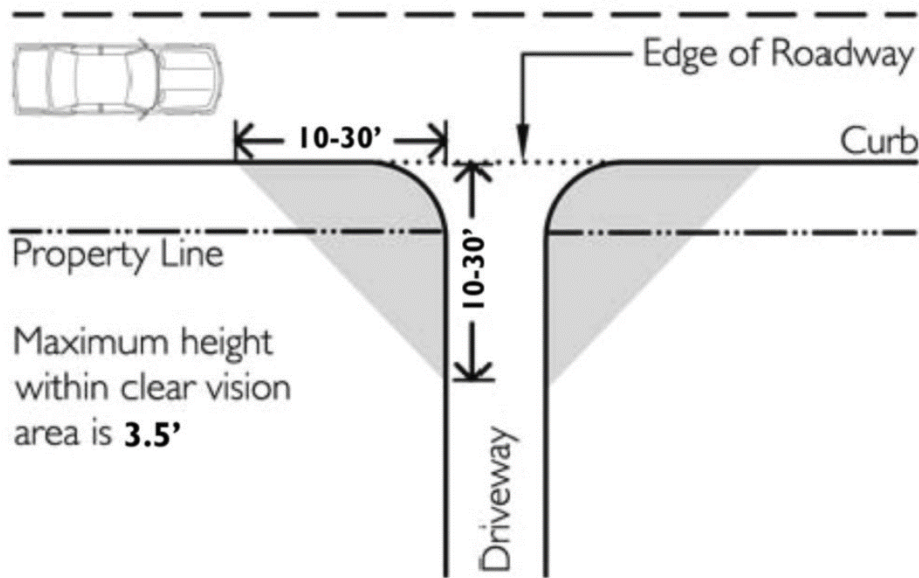
Sec. 15.88.040. - Clear vision areas (visibility at intersections).

- A. *In all zones, a clear vision area shall be maintained on the corners of all property at the intersection of two streets or a street and a railroad. A clear vision area shall contain no planting, wall, structure, private signage, or temporary or permanent obstruction exceeding 3½ feet in height, measured from the top of the curb or, where no curb exists, from the established street centerline grade, except that trees exceeding this height may be located in this area provided all branches and foliage are removed to a height of eight feet above the grade.*
- B. *A clear vision area shall consist of a triangular area on the corner of a lot at the intersection of two streets or a street and a railroad (see Figure 18.88-1). Where lot lines have rounded corners, the specified distance is measured from a point determined by the extension of the lot lines to a point of intersection. The third side of the triangle is the line connecting the ends of the measured sections of the street lot lines. The following measurements shall establish clear vision areas within the city:*
- 1. *In an agricultural, forestry or industrial zone, the minimum distance shall be 30 feet; or at intersections including an alley, ten feet.*

2. In all other zones, the minimum distance shall be in relationship to street and road right-of-way widths as follows:

<i>Right-of-Way Width</i>	<i>Clear vision</i>
<i>80 feet or more</i>	<i>20 feet</i>
<i>Less than 80 feet</i>	<i>30 feet</i>

Clear Vision Areas



Sec. 15.88.050. - Pedestrian access and circulation.

- A. *Purpose and intent.* This section implements the pedestrian access and connectivity policies of City of La Pine Transportation System Plan and the requirements of the Transportation Planning Rule (OAR 660-012). It is intended to provide for safe, reasonably direct, and convenient pedestrian access and circulation.
- B. *Standards.* New subdivisions, multi-family developments, planned developments, commercial developments and institutional developments shall conform to all of the following standards for pedestrian access and circulation:
1. *Continuous walkway system.* A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.
 2. *Safe, direct, and convenient.* Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, recreational areas, playgrounds, and public rights-of-way conforming to the following standards:

- a. *The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel.*
- b. *The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The city may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.*
- c. *Vehicle/walkway separation. Except as required for crosswalks, per subsection d., below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the city may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.*
- d. *Crosswalks. Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrasting material). The crosswalk may be part of a speed table to improve driver-visibility of pedestrians.*
- e. *Walkway construction. Walkway surfaces may be concrete, or other city-approved durable surface meeting ADA requirements. Walkways shall be not less than six feet in width, except that the city may require wider sidewalks in developments where pedestrian traffic warrants walkways wider than six feet.*
- f. *Multi-use pathways. Multi-use pathways, where approved, shall be ten feet wide and constructed of asphalt, concrete or other city-approved durable surface meeting ADA requirements consistent with the applicable city engineering standards.*

STAFF COMMENT: The access and circulation criteria were not addressed in the narrative. Additionally, a pre-development meeting was not requested, where the Fire Marshal would have been able to provide guidance on whether or not the access and circulation plan would comply. Staff recommends reaching out to Clara Butler, with the Oregon State Fire Marshal's Office, to confirm compliance.

After discussion with Clara, Owner has decided to sprinkle building, showed fire hydrants on plan

CHAPTER 15.90. - PUBLIC FACILITIES

Sec. 15.90.010. - Public facilities improvement.

Minor betterment, improvements, replacement or reconstruction of existing public facilities such as sewer and water lines, stormwater drainage facilities, sidewalks and other pedestrian ways or facilities, bikeways and similar public facilities within rights-of-ways and easements for the purposes existing on or before the effective date of this chapter, or on contiguous publicly-owned property designated, intended or utilized to support the facilities, or the facilities that are set forth within an adopted public facilities plan or other capital improvement plan duly adopted on or before the effective date of this ordinance, are exempt from permit requirements, unless specifically set forth otherwise.

Sec. 15.90.020. - Developer responsibility for streets and other public facilities.

- A. *Duties of developer. It shall be the responsibility of the developer to construct all streets, curbs, sidewalks, sanitary sewers, storm sewers, water mains, electric, telephone and cable television lines necessary to serve the use or development in accordance with the specifications of the city and/or the serving entity.*
- B. *Over-sizing. The city may require as a condition of development approval that sewer, water, or storm drainage systems serving new development be sized to accommodate future development within the area as projected by the applicable facility master plan, and the city may authorize other cost-recovery or cost-sharing methods as provided under state law.*
- C. *Inadequate existing streets. Whenever existing streets, adjacent to, within a tract or providing access to and/or from a tract, are of inadequate width and/or improvement standards, additional right-of-way and/or improvements to the existing streets may be required.*
- D. *Half streets. Half streets, while generally not acceptable, may be approved where essential to the reasonable development of a proposed land development, and when the city finds it will be practical to require dedication and improvement of the other half of the street when the adjoining property is developed. Whenever a half street exists adjacent to a tract of land proposed for development, the other half of the street shall be dedicated and improved.*

Sec. 15.90.030. - Sewer and water.

- A. *Sewer and water plan approval. Development permits for sewer and water improvements shall not be issued until the public works director has approved all sanitary sewer and water plans in conformance with city standards.*
- B. *Inadequate facilities. Development permits may be restricted or rationed by the city where a deficiency exists in the existing water or sewer system that cannot be rectified by the development and which, if not rectified, will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of domestic water and sewerage treatment systems. The city may require water booster pumps, sanitary sewer lift stations, and other critical facilities be installed with backup power.*

Sec. 15.90.040. - Stormwater.

- A. *Accommodation of upstream drainage. Culverts and other drainage facilities shall be large enough to accommodate existing and potential future runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the city engineer.*
- B. *Effect on downstream drainage. Where it is anticipated by the city engineer that the additional runoff resulting from the development will overload an existing drainage facility, the city shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with city standards.*

Sec. 15.90.050. - Utilities.

- A. *General provision. The developer of a property is responsible for coordinating the development plan with the applicable utility providers and paying for the extension and installation of utilities not otherwise available to the subject property.*
- B. *Underground utilities. All new electrical, telephone or other utility lines shall be underground unless otherwise approved by the city.*
- C. *Subdivisions. In order to facilitate underground placement of utilities, the following additional standards apply to all new subdivisions:*
 - 1. *The developer shall make all necessary arrangements with the serving utility to provide the underground services. Care shall be taken to ensure that no above ground equipment obstructs vision clearance areas for vehicular traffic.*
 - 2. *The city reserves the right to approve the location of all surface-mounted facilities.*
 - 3. *All underground utilities installed in streets must be constructed and approved by the applicable utility provider prior to the surfacing of the streets.*
 - 4. *Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.*
- D. *Exception to undergrounding requirement. The city may grant exceptions to the undergrounding standard where existing physical constraints, such as geologic conditions, streams, or existing development conditions make underground placement impractical.*

Sec. 15.90.060. - Public street/highway improvement.

The following public streets and highway improvement activities are permitted outright in all zones and are exempt from the permit requirements of this Development Code.

- A. *Installation of additional and/or passing lanes, including pedestrian ways and/or bikeways, within a public street or highway right-of-way existing as of the effective date of this chapter, unless such adversely impacts on-street parking capacities and patterns.*
- B. *Reconstruction or modification of public roads and highways, not including the addition of travel lanes, where no removal or displacement of buildings would occur, and/or no new land parcels result.*
- C. *Temporary public road and highway detours that will be abandoned and restored to original condition or use at such time when no longer needed.*
- D. *Minor betterment of existing public roads and highway related facilities such as maintenance yards, weigh stations, waysides, and, rest areas within a right-of-way existing as of the effective date of this Development Code. In addition, also exempt are contiguous public-owned property utilized to support the operation and maintenance of public roads and highways provided such is not located within a duly designated residential zone, or adjacent to or across the street from a lot or parcel within such a zone.*

- E. *The construction, reconstruction, or modification of a public street or highway that is identified as a priority project in a transportation system plan (TSP) or the state transportation improvement plan (STIP) that was duly adopted on or before the effective date of this chapter.*
- F. *The design, construction, operation, and maintenance of a tourist-oriented or public wayside.*

Sec. 15.90.070. - Design of streets and other public facilities.

- A. *Traffic circulation system. The overall street system shall ensure an adequate traffic circulation system with intersection angles, grades, tangents and curves appropriate for the traffic to be carried considering the terrain of the development and the area. An analysis of the proposed traffic circulation system within the land division, and as such system and traffic generated therefrom affects the overall City of La Pine transportation, will be required to be submitted with the initial land division review application. The location, width and grade of streets shall be considered in their relationship to existing and planned streets, to topographical conditions, to public convenience and safety and to the proposed use or development to be served thereby.*
- B. *Street location and pattern. The proposed street location and pattern shall be shown on the development plan, and the arrangement of streets shall:*
 - 1. *Provide for the continuation or appropriate projection of existing principal streets in surrounding areas; or*
 - 2. *Conform to a plan for the general area of the development approved by the city to meet a particular situation where topographical or other conditions make continuance or conformance to existing streets impractical; and*
 - 3. *Conform to the adopted La Pine Transportation System Plan as may be amended.*
- C. *Access ways. The city, in approving a land use application with conditions, may require a developer to provide an access way where the creation of a cul-de-sac or dead-end street is unavoidable and the access way connects the end of the street to another street, a park, or a public access way. Where an access way is required, it shall be not less than ten feet wide and shall contain a minimum six-foot-wide paved surface or other all-weather surface approved by the city. Access ways shall be contained within a public right-of-way or public access easement, as required by the city.*
- D. *Future street extensions. Where necessary to give access to or permit future subdivision or development of adjoining land, streets shall be extended to the boundary of the proposed development or subdivision. Where a subdivision is proposed adjacent to other developable land, a future street plan shall be filed by the applicant in conjunction with an application for a subdivision in order to facilitate orderly development of the street system. The plan shall show the pattern of existing and proposed future streets from the boundaries of the proposed land division and shall include other divisible parcels within 600 feet surrounding and adjacent to the proposed subdivision. The street plan is not binding, but is intended to show potential future street extensions with future development. The plan must demonstrate, pursuant to city standards, that the proposed development does not preclude future street connections to adjacent development land. Wherever appropriate, street stubs shall be provided to allow access to future abutting subdivisions and to logically extend the street system into the surrounding area. Street ends shall contain turnarounds constructed to Uniform Fire Code*

standards, as the city deems applicable, and shall be designed to facilitate future extension in terms of grading, width, and temporary barricades.

- E. Minimum right-of-way and roadway widths. Unless otherwise approved in the tentative development plan, street, sidewalk and bike rights-of-way and surfacing widths shall not be less than the minimum widths in feet set forth in the La Pine Transportation System Plan, and shall be constructed in conformance with applicable standards and specifications set forth by the city.*
- F. Sidewalks. Unless otherwise required in this chapter or other city ordinances or other regulations, or as otherwise approved by the commission, sidewalks shall be required as specified in the La Pine Transportation System Plan. In lieu of these requirements, however, the city may approve a development without sidewalks if alternative pedestrian routes and facilities are provided.*
- G. Bike lanes. Unless otherwise required in this chapter or other city ordinances or other regulations, bike lanes shall be required as specified in the La Pine Transportation System Plan, except that the planning commission may approve a development without bike lanes if it is found that the requirement is not appropriate to or necessary for the extension of bicycle routes, existing or planned, and may also approve a development without bike lanes in the streets if alternative bicycle routes and facilities are provided.*
- H. Cul-de-sac. A cul-de-sac street shall only be used where the city determines that environmental or topographical constraints, existing development patterns, or compliance with other applicable city requirements preclude a street extension. Where the city determines that a cul-de-sac is allowed, all of the following standards shall be met:*
 - 1. The cul-de-sac shall not exceed a length of 400 feet, except where the city through a Type II procedure determines that topographic or other physical constraints of the site require a longer cul-de-sac. The length of the cul-de-sac shall be measured along the centerline of the roadway from the near side of the intersecting street to the farthest point of the cul-de-sac.*
 - 2. A cul-de-sac shall terminate with a circular turn around with a minimum radius of 45 feet of paved driving surface and a 50 foot right-of-way and meeting the Uniform Fire Code.*
 - 3. The cul-de-sac shall provide, or not preclude the opportunity to later install, a pedestrian and bicycle access way between it and adjacent developable lands.*
- I. Marginal access streets. Where a land development abuts or contains an existing or proposed arterial street, the city may require marginal access streets, reverse frontage lots with suitable depth, screen-plantings contained in a non-access reservation strip along the rear or side property line or other treatments deemed necessary for adequate protection of residential properties and the intended functions of the bordering street, and to afford separation of through and local traffic.*
- J. Streets adjacent to railroad right-of-way. Whenever a proposed land development contains or is adjacent to a railroad right-of-way, provisions may be required for a street approximately parallel to the ROW at a distance suitable for the appropriate use of land between the street and the ROW. The distance shall be determined with consideration at cross streets of the minimum distance required for approach grades to a future grade separation and to provide sufficient depth to allow screen planting or other separation requirements along the ROW.*

- K. *Reserve strips. Reserve strips or street plugs controlling access to streets will not be approved unless deemed necessary for the protection of public safety and welfare and may be used in the case of a dead-end street planned for future extension, and in the case of a half street planned for future development as a standard, full street.*
- L. *Alignment. All streets, as far as practicable, shall be in alignment with existing streets by continuations of the center lines thereof. Necessary staggered street alignment resulting in intersections shall, wherever possible, leave a minimum distance of 200 feet between the center lines of streets of approximately the same direction, and in no case shall the off-set be less than 100 feet.*
- M. *Intersection angles. Streets shall be laid out to intersect at angles as near to right angles as practicable, and in no case shall an acute angle be less than 80 degrees unless there is a special intersection design approved by the city engineer or other duly designated city representative as applicable. Other streets, except alleys, shall have at least 50 feet of tangent adjacent to the intersection, and the intersection of more than two streets at any one point will not be approved.*
- N. *Curves. Centerline radii of curves should not be less than 500 feet on major arterials, 300 feet on minor arterials, 200 feet on collectors or 100 feet on other streets and shall be on an even ten feet. Where existing conditions, particularly topography, make it otherwise impractical to provide building sites, the city may accept steeper grades and sharper curves than provided for herein in this subsection.*
- O. *Street grades. Street grades shall not exceed eight percent on arterials, ten percent on collectors and 12 percent on all other streets including private driveways entering upon a public street or highway; however, for streets at intersections, and for driveways entering upon a public street or highway, there should be a distance of three or more car lengths (approximately 50 feet) where the grade should not exceed six percent to provide for proper stopping distance during inclement weather conditions.*
- P. *Street names. Except for the extension of existing streets, no street names shall be used which will duplicate or be confused with the name of an existing street in the city or within a radius of six miles of the city or within the boundaries of a special service district such as fire or ambulance. Such street names shall be approved by the Deschutes County street name coordinator.*
- Q. *Street name signs. Street name signs shall be installed at all street intersections by the developer in accordance with applicable city, county or state requirements. One street sign shall be provided at the intersection of each street, and two street signs shall be provided at four-way intersections.*
- R. *Traffic control signs. Traffic control signs shall be provided for and installed by the developer as required and approved by the appropriate city, county and/or state agency or department.*
- S. *Alleys. Alleys are not necessary in residential developments, but may be required in commercial and industrial developments unless other permanent provisions for access to off-street parking and loading facilities are approved by the city.*

- T. *Curbs. Curbs shall be required on all streets in all developments, and shall be installed by the developer in accordance with standards set forth by the city unless otherwise approved by the city. Approval of streets without curbs shall be at the discretion of the city engineer, and shall be so determined during the tentative plan land division review process on the basis of special circumstances to the development.*
- U. *Street lights. Street lights may be required and, if so required, shall be installed by the developer in accordance with standards set forth by the city and the serving utility company. Streets lights, if required, shall include one fixture and be located at the intersection of streets.*
- V. *Utilities. The developer shall make necessary arrangements with the serving utility companies for the installation of all proposed or required utilities, which may include electrical power, natural gas, telephone, cable television and the like.*
- W. *Drainage facilities. Drainage facilities shall be provided as required by the city in accordance with all applicable city and Oregon Department of Environmental Quality standards.*
- X. *Gates. Except where approved as part of a master planned development, private streets and gated drives serving more than two dwellings (i.e., where a gate limits access to a development from a public street), are prohibited.*

Sec. 15.90.080. - Traffic impact analysis.

- A. *Purpose. The purpose of this subsection is [to] coordinate the review of land use applications with roadway authorities and to implement section 660-012-0045(2)(e) of the state Transportation Planning Rule, which requires the city to adopt a process to apply conditions to development proposals in order to minimize impacts and protect transportation facilities. The following provisions also establish when a proposal must be reviewed for potential traffic impacts; when a traffic impact analysis must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; the required contents of a traffic impact analysis; and who is qualified to prepare the analysis.*
- B. *When a traffic impact analysis is required. The city or other road authority with jurisdiction may require a traffic impact analysis (TIA) as part of an application for development, a change in use, or a change in access. A TIA shall be required where a change of use or a development would involve one or more of the following:*
 - 1. *A change in zoning or a plan amendment designation;*
 - 2. *Operational or safety concerns documented in writing by a road authority;*
 - 3. *An increase in site traffic volume generation by [300] average daily trips (ADT) or more;*
 - 4. *An increase in peak hour volume of a particular movement to and from a street or highway by [20] percent or more;*
 - 5. *An increase in the use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by ten vehicles or more per day;*

6. *Existing or proposed approaches or access connections that do not meet minimum spacing or sight distance requirements or are located where vehicles entering or leaving the property are restricted, or such vehicles are likely to queue or hesitate at an approach or access connection, creating a safety hazard;*
 7. *A change in internal traffic patterns that may cause safety concerns; or*
 8. *A TIA required by ODOT pursuant to OAR 734-051.*
- C. *Traffic impact analysis preparation. A professional engineer registered by the State of Oregon, in accordance with the requirements of the road authority, shall prepare the traffic impact analysis.*
- D. *Waiver or deferral. The city may waive or allow deferral of standard street improvements, including sidewalk, roadway, bicycle lane, undergrounding of utilities, and landscaping, as applicable, where one or more of the following conditions in [subsections] 1 through 4 is met. Where the city agrees to defer a street improvement, it shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future:*
1. *The standard improvement conflicts with an adopted capital improvement plan.*
 2. *The standard improvement would create a safety hazard.*
 3. *It is unlikely due to the developed condition of adjacent property that the subject improvement would be extended in the foreseeable future, and the improvement under consideration does not by itself significantly improve transportation operations or safety.*
 4. *The improvement under consideration is part of an approved partition in the [RL or RM] and the proposed partition does not create any new street.*

STAFF COMMENT: The public facilities criteria were not addressed in the narrative. Please address this.

RESPONSE: The site is located in a developed industrial subdivision with a 60-foot right of way, 40-foot-wide street, water, sewer, storm drainage and franchise utilities installed. The proposed building will not place an undue burden on the existing facilities. We will be installing a 6-foot-wide sidewalk along the property frontage.

CHAPTER 15.94 - IMPROVEMENT PROCEDURES AND GUARANTEES

Sec. 15.94.010. - Improvement procedures.

Improvements to be installed by the developer, either as a requirement of this chapter, conditions of approval or at the developer's option as proposed as a part of the subject development proposal, shall conform to the following requirements:

- A. *Plan review and approval. Improvement work shall not be commenced until plans therefor have been reviewed and approved by the city or a designated representative thereof. The review and approval shall be at the expense of the developer.*
- B. *Modification. Improvement work shall not commence until after the city has been notified and approval therefore has been granted, and if work is discontinued for any reason, it shall not be resumed until after the city is notified and approval thereof granted.*

- C. *Improvements as platted. Improvements shall be designed, installed and constructed as platted and approved, and plans therefore shall be filed with the final plat at the time of recordation or as otherwise required by the city.*
- D. *Inspection. Improvement work shall be constructed under the inspection and approval of an inspector designated by the city, and the expenses incurred therefore shall be borne by the developer. Fees established by the city council for such review and inspection may be established in lieu of actual expenses. The city, through the inspector, may require changes in typical sections and details of improvements if unusual or special conditions arise during construction to warrant such changes in the public interest.*
- E. *Utilities. Underground utilities, including, but not limited to, electric power, telephone, water mains, water service crossings, sanitary sewers and storm drains, to be installed in streets, shall be constructed by the developer prior to the surfacing of the streets.*
- F. *As built plans. As built plans for all public improvements shall be prepared and completed by a licensed engineer and filed with the city upon the completion of all such improvements. A copy of the as built plans shall be filed with the final plat of a subdivision or other development by and at the cost of the developer. The plans shall be completed and duly filed within 30 days of the completion of the improvements.*

Sec. 15.94.020. - Completion or assurance of improvements.

- A. *Agreement for improvements. Prior to final plat approval for a subdivision, partition, PUD or other land development, or the final approval of a land use or development pursuant to applicable zoning provisions, where public improvements are required, the owner and/or developer shall either install required improvements and repair existing streets and other public facilities damaged in the development of the property, or shall execute and file with the city an agreement between him/herself and the city specifying the period in which improvements and repairs shall be completed and, providing that if the work is not completed within the period specified, that the city may complete the work and recover the full costs thereof, together with court costs and attorney costs necessary to collect the amounts from the developer. The agreement shall also provide for payment to the city for the cost of inspection and other engineer services directly attributed to the project.*
- B. *Bond or other performance assurance. The developer shall file with the agreement, to ensure his/her full and faithful performance thereof, one of the following, pursuant to approval of the city attorney and city manager, and approval and acceptance by the city council:*
 - 1. *A surety bond executed by a surety company authorized to transact business in the State of Oregon in a form approved by the city attorney.*
 - 2. *A personal bond co-signed by at least one additional person together with evidence of financial responsibility and resources of those signing the bond sufficient to provide reasonable assurance of the ability to proceed in accordance with the agreement.*
 - 3. *Cash deposit.*
 - 4. *Such other security as may be approved and deemed necessary by the city council to adequately ensure completion of the required improvements.*

- C. *Amount of security required. The assurance of full and faithful performance shall be for a sum approved by the city as sufficient to cover the cost of the improvements and repairs, including related engineering, inspection and other incidental expenses, plus an additional 20 percent for contingencies.*
- D. *Default status. If a developer fails to carry out provisions of the agreement, and the city has unreimbursed costs or expenses resulting from the failure, the city shall call on the bond or other assurance for reimbursement of the costs or expenses. If the amount of the bond or other assurance deposit exceeds costs and expenses incurred by the city, it shall release the remainder. If the amount of the bond or other assurance is less than the costs or expenses incurred by the city, the developer shall be liable to the city for the difference plus any attorney fees and costs incurred.*

Sec. 15.94.030. - Building and occupancy permits.

- A. *Building permits. No building permits shall be issued upon lots to receive and be served by sanitary, sewer and water service and streets as improvements required pursuant to this chapter unless the improvements are in place, serviceable and approved by the city, with the service connections fees paid, and accepted by the city.*
- B. *Sale or occupancy. All improvements required pursuant to this chapter and other applicable regulations or approval conditions shall be completed, in service and approved by the city, and accepted by the city council, prior to sale or occupancy of any lot, parcel or building unit erected upon a lot within the subdivision, partitioning, PUD or other development.*

Sec. 15.94.040. - Maintenance surety bond.

Prior to sale and occupancy of any lot, parcel or building unit erected upon a lot within a subdivision, partitioning, PUD or other development, and as a condition of acceptance of improvements, the city will require a one-year maintenance surety bond in an amount not to exceed 20 percent of the value of all improvements, to guarantee maintenance and performance for a period of not less than one year from the date of acceptance.

Sec. 15.94.050. - Engineering/special services for review.

With regard to any development proposal for which the city deems it necessary to contract for engineering and/or other special technical services for the review thereof or for the design of facility expansions to serve the development, the developer may be required to pay all or part of the special services. In such cases, the choice of the contract service provider shall be at the discretion of the city, and the service provider shall perform the necessary services at the direction of the city. The costs for the services shall be determined reasonable, and an estimate of the costs shall be provided to the developer prior to contracting therefore [therefor].

STAFF COMMENT: The improvement procedures and guarantees criteria were not addressed in the narrative. Please address this. **RESPONSE:** A bond or cash deposit will be provided for the sidewalk improvements.

ARTICLE 8 - APPLICATIONS AND REVIEWS

CHAPTER 15.312. - SITE PLAN REVIEW

Sec. 15.312.010. - Purpose.

- A. *The purpose of the site plan review provisions of this section [chapter] is to ensure that development within the city complies with standards and limitations set forth within the applicable zone, by other city standards and requirements and by applicable county, state and federal regulations.*
- B. *This broad purpose is furthered by the following specific purposes of site plan review:*
 - 1. *To implement the goals and policies of the comprehensive plan.*
 - 2. *To foster development that is designed, arranged and constructed in a manner that provides a safe, efficient and aesthetically pleasing community asset.*
 - 3. *To encourage originality and creativity in site design, architecture and landscape design.*
 - 4. *To ensure that the arrangement of all functions, uses and improvements of a development reflect the natural amenities, capabilities and limitations of its site and adjacent areas.*
 - 5. *To encourage development where the various structures, use areas and site elements are integrated in a manner that is visually harmonious within the development and the surrounding area.*
 - 6. *To encourage development and landscape design that complements the natural landscape and setting, improves the general appearance of the community and enhances specific elements of the man-made environment, both presently and historically.*

Sec. 15.312.020. - Applicability.

The following uses and development shall be subject to the provisions of this section:

- A. *All new construction or new development except for: single-family residences (including manufactured dwellings, mobile homes, modular homes), duplexes, accessory dwelling units and related accessory structures unless provided otherwise in this chapter.*
- B. *An exterior alteration or modification to an existing nonresidential use or structure, which is subject to site plan review and/or is subject to regulation under the provisions of this chapter, except for painting, replacement of roofing and siding, and other normal maintenance and upkeep requirements which are not subject to regulation under the provisions of this chapter or any other applicable city, county, state and/or federal regulations.*
- C. *Any alteration or modification of site improvements, such as the landscaping, parking and/or loading facilities and areas, in conjunction with an existing nonresidential use which is subject to site plan review and/or is subject to regulation under the provisions of this chapter.*

Sec. 15.312.025. - Site plan approval required.

Site plan review and approval, as specified by this chapter, shall be required prior to the following:

- A. *Site clearance activities such as grading, excavation or filling for any use or development requiring a permit pursuant to this Development Code.*
- B. *The issuance of a building or development permit for any use or development requiring city approval pursuant to this Development Code.*

Sec. 15.312.030. - Procedure type.

- A. *Site plan review applications are subject to Type II review in accordance with the procedures in [article 7](#) unless elevated to a Type III review at the discretion of the planning official.*
- B. *Pre-application conference. Prior to applying for site plan approval, applicants should and may meet with the city planning official, building official and public works director, or designees thereof, and present a preliminary plan which shall contain, in an approximate manner, the information required on a site plan review application.*
 - 1. *The purpose of the preliminary site plan review is to enable the applicant to obtain advice from the city as to the intent, standards, criteria and provisions of this chapter, this Development Code, other city ordinances, standards and regulations, and state and federal rules and regulations which may be pertinent to the proposal.*
 - 2. *Information presented for preliminary discussion shall be considered confidential if so requested by the applicant.*

Sec. 15.312.040. - Submittal requirements.

A property owner or authorized representative thereof may initiate a request for site plan review by filing an application with the city using forms prescribed by the city together with the required filing fee in accordance with the Type II application requirements in [article 7](#). In addition to the information required for a Type II review (see [article 7](#)), the applicant shall submit that which is listed below.

- A. *Requirements for information to be submitted. Information provided on the site plan shall conform to the following. The number of copies required shall be as specified on the application form.*
 - 1. *Drawings depicting the proposal shall be presented on sheets not larger than 24 inches by 36 inches in the number of copies directed by the city.*
 - 2. *To facilitate public reviews and notice, at least one copy of the proposal shall be provided on a sheet of paper not larger than 11 inches by 17 inches.*
 - 3. *Drawings shall be at a scale sufficiently large enough to enable all features of the design to be clearly discerned and shall include a north arrow and scale.*
 - 4. *The city may require that the drawing, development plan or other information be provided to the city on computer disk in a format adaptable to the city's computer systems.*

- B. Site analysis diagram. If required by the city planning official, this element of the site plan, which may be in schematic or free hand form to scale, shall indicate the following site characteristics:*
- 1. Location and species of existing trees greater than six inches in diameter when measured four feet above the natural grade, and an indication of which trees are proposed to be removed.*
 - 2. On sites that contain steep slopes, potential geological hazard or unique natural features that may affect the proposed development, the city may require contours mapped at two-foot intervals.*
 - 3. Natural drainage ways, depths of any ground water tables less than 12 feet, any areas of surface water accumulations and any other significant natural features.*
 - 4. The location and width of all public and private streets, drives, sidewalks, pathways, rights-of-way, and easements on the site and adjoining the site, and all buildings, utilities, retaining walls, and other man-made features, both existing and proposed.*
 - 5. Natural features, including trees, riparian habitat and stream channels and structures on-site or on adjoining properties that have or may have a visual or other significant relationship with the site and the proposed development thereon.*
- C. Site photographs. Photographs depicting the site and its relationship to adjoining sites and the general area are extremely valuable, should be provided, and may be required by the city planning official.*
- D. Site development plan. The site plan shall indicate the following:*
- 1. Legal description of the property.*
 - 2. Boundary dimensions and site area.*
 - 3. Location and sizes of existing and proposed utilities, including water lines, sewer lines, hydrants, etc.*
 - 4. Location of all existing and proposed structures, including distances from the property lines.*
 - 5. Area of the site to be covered by structures, existing and proposed, and the percentage of site coverage thereby.*
 - 6. All external dimensions of existing and proposed buildings and structures.*
 - 7. Location of building entrances and exits.*
 - 8. Access drives, parking and circulation areas, including their dimensions.*
 - 9. Service areas and delivery circulation plan for such uses as the loading and delivery of goods.*
 - 10. Locations, descriptions and dimensions of easements as may be applicable.*

11. *Grading and drainage plans and calculations, including spot elevations and contours at intervals close enough to convey their meaning.*
 12. *Location of areas to be landscaped, including designated landscape material/plant types and sizes.*
 13. *Outdoor recreation and/or play areas.*
 14. *Pedestrian and bicycle circulation, including existing and proposed on-site and off-site sidewalks.*
 15. *Location of mechanical equipment not enclosed within a building, garbage disposal areas, utility appurtenances and similar structures.*
 16. *Exterior lighting and fencing.*
 17. *Location, size and method of illumination of signs.*
 18. *Provisions for handicapped persons.*
 19. *Other site elements which will assist in the evaluation of site development.*
 20. *Location, names, surface and right-of-way widths and improvement standards of all existing and proposed streets within or adjacent to the proposed development.*
 21. *Location of areas designated for snow storage, in accordance with the requirements of section 18.86.060 [15.86.060], and calculations of the area required by the minimum standard and the proposed area.*
 22. *Information necessary to demonstrate compliance with [the] fire code, including, but not limited to, fire flow, apparatus access, and hydrant spacing.*
- E. *Accompanying written summary. In addition to the foregoing site development plan requirements, a written summary of the proposal should be provided and may be required showing the following, (unless such is shown on the site development plan):*
1. *Commercial and nonresidential development. For commercial and nonresidential development:*
 - a. *The square footage contained in the site area to be developed.*
 - b. *The percentage of the area to be covered by structures when developed.*
 - c. *The percentage of the area to be covered by parking areas and the total number of parking spaces.*
 - d. *The total square footage of all landscaped areas, including the percentage consisting of natural materials and the percentage of hard surfaced areas such as courtyards.*

- e. *Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the city planning official). A traffic impact analysis may be required in accordance with [section 15.90.080](#).*
- 2. *Residential development. For residential development:*
 - a. *The total square footage of the lot or parcel and in the structures in the development.*
 - b. *The number of dwelling units in the development (include the units by the number of bedrooms in each unit, for example, ten one-bedroom, 25 two-bedroom and the like).*
 - c. *Percentage of lot coverage by structures, way areas, recreation areas and landscaping.*
 - d. *Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the city planning official). A traffic impact analysis may be required in accordance with [section 15.90.080](#).*
- F. *Landscape plan. If required by the city planning official, a landscape plan shall be submitted and shall indicate the following.*
 - 1. *The size, species and locations of plant materials to be retained or placed on-site.*
 - 2. *The layout of irrigation facilities.*
 - 3. *Location and design details of walkways, plazas, courtyards and similar areas.*
 - 4. *Location, type and intensity of outdoor lighting.*
 - 5. *Location and design details of proposed fencing, retaining walls and trash collection areas.*
 - 6. *Other information as deemed appropriate by the review authority. An arborist's report may be required for sites with mature trees that are to be retained and protected.*
- G. *Architectural drawings. This element of the site plan review, if required by the city planning official, shall indicate the following:*
 - 1. *A plan specifying the building footprint and dimensions, including all points of access. Floor plans of interior spaces to the extent required to clarify access functions and the relationship of the spaces to decks, porches, balconies and stairs or other features shown on the building elevations. The floor plans shall be provided for all building floors and shall include appropriate dimensions.*
 - 2. *Exterior elevations showing building heights, windows, doors, exterior light fixtures, stairways, balconies, decks and other architectural details. These elevations shall be provided for every exterior wall surface, including those which are completely or partially concealed from view by overlapping portions of the structure. Existing and finished grades at the center of all walls shall be shown with elevations of floors indicated and a dimension showing compliance with height limitations.*

3. *Location and type of exterior light fixtures, including the lamp types and the levels of illumination that they provide.*
4. *Location, size and method of illumination of all exterior signs.*
- H. *Property survey. A survey of the property by a licensed land surveyor may be required, and if required the survey shall clearly delineate property boundaries, and show the location of the corners of proposed buildings and other significant features proposed for the site. The requirement for a survey of the exterior boundaries of a site may be waived where it is found that there is a recent survey that can be used to clearly establish the applicant's property boundaries.*
- I. *Deed restrictions. Copies of all existing and proposed restrictions or covenants, including those for roadway access control.*
- J. *Narrative. A written narrative addressing the applicable criteria listed [in] [section 15.312.050](#) for residential development and sections [15.312.050](#) and [15.312.060](#) for nonresidential development.*
- K. *Other information as determined by the city planning official. The city planning official may require studies or exhibits prepared by qualified professionals to address specific site features or project impacts (e.g., traffic, noise, environmental features, natural hazards, etc.), as necessary to determine a proposal's conformance with this Development Code.*

Sec. 15.312.050. - Approval criteria - all residential and nonresidential.

To ensure that the stated purposes of the site plan review process are met, the review authority shall be governed by the criteria below as they evaluate and render a decision on a proposal.

- A. *Statement of intent.*
 1. *The site plan review criteria are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the city with a means of reviewing proposed plans.*
 2. *These criteria provide a clear and objective means of evaluating residential development (and the residential components of a mixed use development) in accordance with ORS 197.*
 3. *The review authority is not authorized as a part of the site plan review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this Development Code.*
- B. *Site plan evaluation criteria. The following criteria shall be used in evaluating all site development plans:*
 1. *The application is complete, in accordance with the applicable procedures in [article 7](#).*

2. *The application complies with all applicable provisions of the underlying zoning district in [article 3](#), including, but not limited to, setbacks, lot dimensions, density, lot coverage, building height, and other applicable standards.*
3. *The application complies with the provisions of the any applicable overlay zones in [article 4](#).*
4. *The proposal complies with all applicable development and design standards of [article 5](#).*
5. *The application complies with all applicable special use standards in [article 6](#).*
6. *Adequate public facilities and utilities are available or can be made prior to occupancy to serve the proposed development.*
7. *The proposed site plan conforms to the standards within the adopted La Pine Transportation System Plan (TSP), as may be amended from time to time, unless other design standards are specifically approved by the city.*
8. *The proposed site plan conforms to the La Pine Sewer and Water Standards, as may be amended from time to time, unless other design standards are specifically approved by the city. All sewer improvements must comply with Oregon Administrative Rules chapter 340 division 52 requirements, including Appendix A - Sewer Pipelines.*
9. *The proposed site plan conforms to the Central Oregon Stormwater Manual (COSM), as may be amended from time to time, unless other design standards are specifically approved by the city.*
10. *All utilities shall be installed underground, unless otherwise specifically approved by the city.*
11. *The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.*

Note— *Compliance with other city codes and requirements, though not applicable land use criteria, may be required prior to issuance of building permits.*

Sec. 15.312.060. - Additional approval criteria - nonresidential development.

In addition to the approval criteria in [section 15.312.050](#), to ensure that the stated purposes of the site plan review process are met, the review authority shall also be governed by the criteria below as they evaluate and render a decision on a nonresidential development proposal.

A. Statement of intent.

1. *The site plan review criteria for nonresidential development are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the city with a means of reviewing proposed plans.*
2. *These criteria are not intended to be inflexible requirements, nor are they intended to discourage creativity. The specification of one or more architectural styles is not intended by these criteria.*

3. *The review authority is not authorized as a part of the design review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this chapter.*
- B. *Site plan evaluation criteria. In addition to the approval criteria in [section 15.312.050](#), the following criteria shall be used in evaluating nonresidential site development plans:*
1. *The arrangement of all functions, uses and improvements has been designed so as to reflect and harmonize with the natural characteristics and limitations of the site and adjacent sites.*
 2. *In terms of setback from streets or sidewalks, the design creates a visually interesting and compatible relationship between the proposed structures and/or adjacent structures.*
 3. *The design incorporates existing features, such as streams, rocks, slopes, vegetation and the like, as part of the overall design.*
 4. *Where appropriate, the design relates or integrates the proposed landscaping/open space to the adjoining landscape/open space in order to create a pedestrian/bike pathway and/or open system that connects several properties or uses.*
 5. *The arrangement of the improvements on the site do not unreasonably degrade the scenic values of the community and the surrounding area in particular.*
 6. *Where appropriate, the design includes a parking and circulation system that encourages a pedestrian and/or bicycle rather than vehicular orientation, including a separate service area for delivery of goods.*
 7. *The design gives attention to the placement of storage, mechanical equipment, utilities or waste collection facilities so as to screen such from view, both from within and from outside the site.*
- C. *Landscape design evaluation criteria. The following criteria shall be used in evaluating landscape plans:*
1. *The overall design substantially complements the natural environment of the city and the character of the site and the surrounding area.*
 2. *The design acknowledges the growing conditions for this climatic zone, and the unique requirements that its specific site location makes upon plant selection.*
 3. *Provision has been made for the survival and continuous maintenance of the landscape and its vegetation.*
 4. *The design contributes to the stabilization of slopes and the protection of other natural features and resources where applicable.*

Sec. 15.312.070. - Conditions of approval.

In addition to the standards and conditions set forth in a specific zone (if found to be necessary and supported with adequate findings), additional conditions may be imposed by the city which are found to be necessary to avoid a detrimental impact on adjoining properties, the general area or the city as a whole, and to otherwise protect the general welfare and interests of the surrounding area. The conditions may include, but are not limited to, the following:

- A. Limiting the manner in which the use is conducted, including restricting the time an activity may take place, and restrictions to minimize environmental impacts such as noise, vibration, air or water pollution, glare and odor.*
- B. Establishing a special setback or other open space requirements, and increasing the required lot size or other dimensional standards.*
- C. Limiting the height, size or location of a building or other structure or use.*
- D. Increasing street width and/or requiring improvements to public streets and other public facilities serving the proposed use, even including those off-site but necessary to serve the subject proposal.*
- E. Designating the size, number, improvements, location and nature of vehicle access points and routes, and requiring pedestrian and/or bicycle ways.*
- F. Limiting or otherwise designating the number, size, location, height and lighting of signs and outdoor or security lighting, and the intensity and/or direction thereof.*
- G. Requiring screening, fencing or other improvements or facilities deemed necessary to protect adjacent or nearby properties, and establishing requirements or standards for the installation and maintenance thereof.*
- H. Protecting and preserving existing trees, other vegetation and water, scenic, historic, archaeological, unique, landmark or other natural or man-made significant resources.*

STAFF COMMENT: The site plan review criteria were not addressed in the narrative. Please address this.

RESPONSE: This section has been addressed

Pursuant to ORS 227.178 the applicant has 180 days from the date of application (September 24, 2025) to respond to this Incomplete Letter. If the application does not respond **in writing** within 180 days, then the application will be deemed void on the 181st day (March 24, 2026). To prevent this application from being deemed void, please respond **in writing** to this Incomplete Letter by submitting one of the following:

- All of the requested information.
- Some of the requested information and written notice that no other information will be provided and the applicant requests staff to deem the application complete and begin their review.
- None of the requested information and written notice that the applicant requests staff to deem the application complete and begin their review.

Please be aware that failure to provide all the requested information may result in an administrative denial or referral to a Public Hearing in front of the La Pine Planning Commission.

This letter does not convey tacit approval or denial of any development on the subject property. If you have any questions, please contact me directly at (541) 668-1135 or bbybee@lapineoregon.gov.

Sincerely,

A handwritten signature in black ink, appearing to read "Brent Bybee". The signature is fluid and cursive, with the first name "Brent" and last name "Bybee" clearly distinguishable.

Brent Bybee, Community Development Director



ENGINEERING, INC.

Traffic Generation Letter

December 8, 2025

Subject: Lot 14, Newberry Business Park – 16330 Box Way.
City, State: La Pine, Oregon

L.B. Engineering, Inc. is submitting for a 6,400 s.f. footprint building, that will be used for General Light Industrial uses.

L.B. Engineering, Inc. has completed the traffic generation letter for the proposed building with the findings presented below.

SITE INFORMATION

The building has a footprint of 6,400 s.f. that will be used for General Light Industrial uses

The site is located at 16330 Box Way, La Pine, OR and is Tax Lot 221014AB00132

The site zoning is I - Industrial

The site will be accessed off of Box Way

TRIP GENERATION

We will be using ITE Trip Generation Manual, 11th Edition, 140 Manufacturing

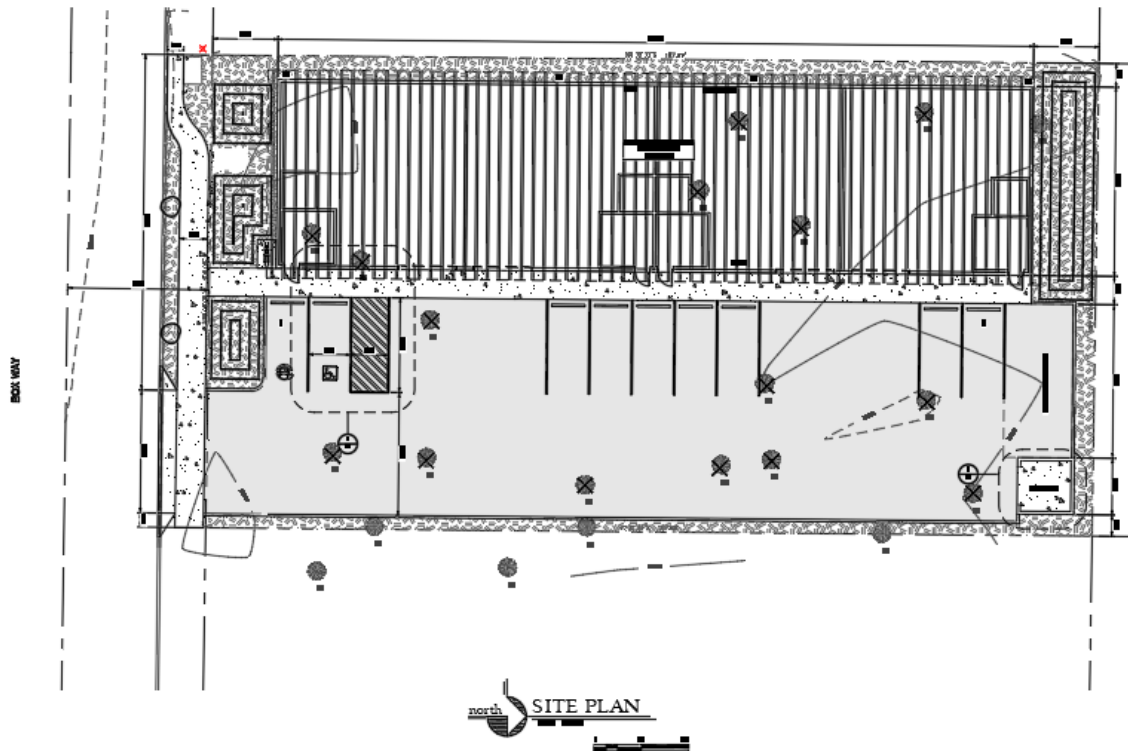
PROPOSED USE

ADT TABLE 1- TRIP GENERATION FOR PROPOSED BUILDING ITE CODE 110 – General Light Industrial per 1000 s.f. (6,400 s.f.)						
ADT TIME STUDY	TRIP RATE	KFS	IN/OUT (PERCENT)	IN	OUT	TOTAL
WEEKDAY A.M. PEAK HOUR	0.74	6.4	88/12	4	1	5
WEEKDAY P.M. PEAK HOUR	0.65	6.4	14/86	1	3	4
TOTAL PEAK TRIPS GENERATED						9
DAILY TRIPS GENERATED	4.87	6.4	50/50	16	15	31

AERIAL



SITE PLAN



REQUIREMENTS FOR A TRAFFIC IMPACT ANALYSIS:

1. A change in zoning or a plan amendment designation;

RESPONSE: *This Land use submittal is for site plan approval with no change in zoning or plan amendment designation.*

2. Operational or safety concerns documented in writing by a road authority;

RESPONSE: *Box Way is low use street, we do not anticipate and operational or safety concerns.*

3. An increase in site traffic volume generation by [300] average daily trips (ADT) or more;

RESPONSE: *This site proposal does not increase ADT by 300 daily trips or more.*

4. An increase in peak hour volume of a particular movement to and from a street or highway by [20] percent or more;

RESPONSE: *The highest classification street is Reed Rd to the North which is a collector street. With only 4 additional PM peak hour trips this will not increase any particular movement by 20 percent or more.*

5. An increase in the use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by ten vehicles or more per day;

RESPONSE: *The tenants are unknown at this time but with the relatively small size of the proposed building it is not anticipated that there would be an increase of vehicles exceeding 20,000 pounds by 10 or more.*

6. Existing or proposed approaches or access connections that do not meet minimum spacing or sight distance requirements or are located where vehicles entering or leaving the property are restricted, or such vehicles are likely to queue or hesitate at an approach or access connection, creating a safety hazard;

RESPONSE: *The proposed approaches meet minimum spacing and sight distance requirements for a local street; there are no anticipated safety concerns.*

7. A change in internal traffic patterns that may cause safety concerns; or

RESPONSE: *The site is vacant so all internal traffic patterns are proposed, there are no anticipated safety concerns.*

8. A TIA required by ODOT pursuant to OAR 734-051.

RESPONSE: *The site does not front an ODOT road classification*

CONCLUSION:

Based on the above responses a Traffic Impact Analysis should not be required to meet the City of La Pine development code.

Respectfully Submitted,

Lennie Brant, SE
President
L. B. Engineering, Inc.



Drawing Index		Drawing Release History		
Drawing Title	Pages	Type	Date	Description
Cover Sheet				
Codes and Loads				
Notes				
Anchor Rod Plan				
Primary Structural				
Secondary Structural				
Covering				
Special Drawings				
Standard Erection Details				
Planograph Details				

A 3D wireframe architectural rendering of a long, low industrial building. The building features a gabled roof with a central ridge. The front facade is composed of vertical panels and includes several large doors of varying sizes. The side walls are also depicted with vertical paneling. The entire structure is shown in a perspective view, highlighting its elongated form and structural details.

The general contractor and/or erector is solely responsible for accurate good quality workmanship in erecting this building in accordance with this drawing, details referenced in this drawing, all applicable VP Buildings erection guides, and industry standards pertaining to proper erection, including the correct use of temporary bracing.

13:44:17

a division of BlueScope Buildings North America, Inc.

Codes and Loads
WHEN MULTIPLE BUILDINGS ARE INVOLVED, SPECIFIC LOAD FACTORS FOR DIFFERING OCCUPANCIES, BUILDING DIMENSIONS, HEIGHTS, FRAMING SYSTEMS, ROOF SLOPES, ETC., MAY RESULT IN DIFFERENT LOAD APPLICATION FACTORS THAN INDICATED BELOW. SEE CALCULATIONS FOR FURTHER DETAILS. WIND LOADS ARE APPLIED TO OVERALL BUILDING ENVELOPE. COMMON WALLS BETWEEN CONNECTED SHAPES ARE NOT SUBJECT TO EXTERNAL WIND LOADS.
City: La Pine County: Deschutes State: Oregon Country: United States

Building Code
Building Code: 2021 International Building Code Structural: 16AISC - ASD Rainfall: I: 4.00 inches per hour
Building Risk/Occupancy Category: II (Standard Occupancy Structure) Cold Form: 16AISI - ASD f'c: 3000.00 psi Concrete

Dead and Collateral Loads
Collateral Gravity: 10.00 psf
Collateral Uplift: 0.00 psf
Material Dead Weight
Roof Covering + Second. Dead Load: Varies
Frame Weight (assumed for seismic):2.50 psf

Wind Load
Wind Speed: Vult: 99.00 (Vasd: 76.69) mph
The 'Envelope Procedure' is Used
Primaries Wind Exposure: C - Kz: 0.882
Parts Wind Exposure Factor: 0.882
Wind Enclosure: Enclosed
Topographic Factor: Kzt: 1.0000
Ground Elevation Factor: Ke: 1.0000
Snow Load
Ground Snow Load: pg: 49.00 psf
Flat Roof Snow: pf: 30.87 psf
Design Snow (Sloped): ps: 28.75 psf
Rain Surcharge: 0.00 psf
Specified Minimum Roof Snow: 20.00 psf (Code)
Exposure Factor: 1 Fully Exposed - Ce: 0.90
Snow Importance: Is: 1.000
Thermal Factor: Heated - Ct: 1.00
Ground / Roof Conversion: 0.70
Unobstructed, Slippery

NOT Windborne Debris Region
Base Elevation: 0/0/0
Site Elevation: 0.0 ft
Primary Zone Strip Width: 2a: 8/0/0
Parts / Portions Zone Strip Width: a: 4/0/0
Velocity Pressure: qz: 18.81, (CzC) 18.81 psf

Roof Live Load
Roof Live Load: 20.00 psf Not Reducible

Seismic Load
Lateral Force Resisting Systems using Equivalent Force Procedure
Mapped MCE Acceleration: Ss: 42.30 %g
Mapped MCE Acceleration: S1: 21.40 %g
Site Class: Stiff soil (D) - Default
Seismic Importance: Ie: 1.0000
Design Acceleration Parameter: Sds: 0.4122
Design Acceleration Parameter: Sd1: 0.3099
Seismic Design Category: C
Seismic Snow Load: 5.75 psf
% Snow Used in Seismic: 20.00
Diaphragm Condition: Flexible
Fundamental Period Height Used: 19/8/0

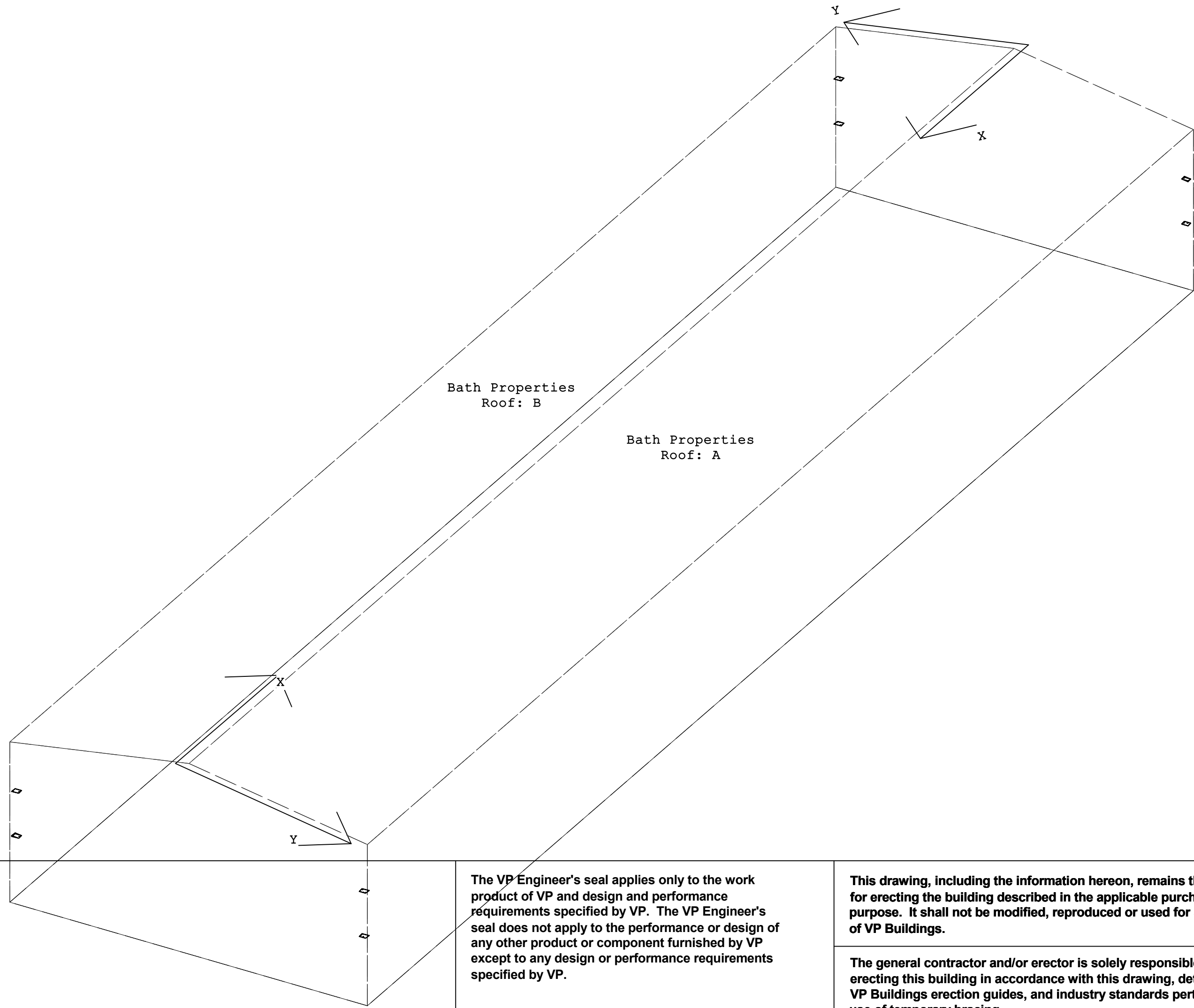
Transverse Direction Parameters
System NOT detailed for Seismic
Redundancy Factor: Rho: 1.00
Fundamental Period: Ta: 0.3035
R-Factor: 3.00
Overstrength Factor: Omega: 2.50
Deflection Amplification Factor: Cd: 3.00
Base Shear: V: 0.1374 x W

Longitudinal Direction Parameters
System NOT detailed for Seismic
Redundancy Factor: Rho: 1.00
Fundamental Period: Ta: 0.1868
R-Factor: 3.00
Overstrength Factor: Omega: 2.50
Deflection Amplification Factor: Cd: 3.00
Base Shear: V: 0.1374 x W

Snow Buildup	Surface	Description	X Location	Y Location	Magnitude
Shape	Roof: A	Unbalanced Snow Load 1, Shifted Left : Roof: A	0.0 ft	11.3 ft	14.4 psf
Bath Properties			0.0 ft	0.0 ft	14.4 psf
			160.0 ft	0.0 ft	14.4 psf
			160.0 ft	11.3 ft	14.4 psf
			0.0 ft	11.3 ft	14.4 psf
			0.0 ft	0.0 ft	14.4 psf
			160.0 ft	0.0 ft	14.4 psf
			160.0 ft	11.3 ft	14.4 psf

Bath Properties	Roof: B	Unbalanced Snow Load 1, Shifted Right : Roof: B			


1. The Snow Buildup loading shown is in addition to the flat or sloped roof snow.
2. The X and Y Location dimensions are from the point of origin of each surface.



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D	VP Buildings			CODES AND LOADS			
	3200 Players Club Circle Memphis TN 38125						
Rev	Date	By	Description	Builder		<div><p>VP BUILDINGS VARCO PRUDEN</p><p>A BlueScope Steel Company VPC Version: 25.1.1</p></div>	Job #
				Customer			Date
				Location			5/7/2025
				Project			Drawn/Check
				Builders PO#			Page
NTS				Filename:		a division of BlueScope Buildings North America, Inc.	
5/13/2025			13:44:17	Bath Propertie			

BUILDER/CONTRACTOR RESPONSIBILITIES

VP Buildings follows the guidelines as outlined in the AISC and MBMA Codes of Standard Practice. VP Buildings standard product specifications, design, fabrication, quality criteria shall govern all work unless stipulated otherwise in the contract documents. In case of discrepancies between VP Buildings structural plans and plans for other trades, VP Buildings structural plans shall govern.

It is the responsibility of the Builder to obtain approvals and permits from all governing agencies and jurisdictions as required. Approval of VP Buildings drawings constitutes the builders acceptance of VP interpretation of the contract purchase order. Unless specific design criteria concerning interface design and details are furnished as part of the contract, VP Buildings design assumptions shall govern.

VP engineers are not Project Engineers or Engineer of Record for the overall project. VP engineering supply sealed engineering design data and drawings for VP supplied material as part of the overall project for use by others to obtain permits, approvals, and coordinate with other trades. All interface and/or compatibility of any materials not furnished by VP are to be considered and coordinated by the builder or A/E firm.

CONSTRUCTION & ERECTION RESPONSIBILITY

The Builder is responsible for construction in strict accordance with VP Buildings "FOR CONSTRUCTION" drawings and all applicable product installation guides. VP is not responsible for work done from any other VP drawings that are not marked "FOR CONSTRUCTION", nor any drawings prepared by others.

As erected field assemblies of members shall be as specified in MBMA Code of Standard Practice (in Canada - CSA S16), which require L/500 tolerance of installed members. Occasional field work including shimming, cutting, coping, and drilling for final fit-up are considered part of erection. Specified field work and field welding conditions indicated on these drawings shall also be included in the erectors scope of work. See Erection Guide for shimming procedure. For building with top riding bridge cranes see Crane Data drawing for column plumb tolerance.

The building erector shall be properly licensed and experienced in erecting metal building systems. The Builder is responsible for having knowledge of, and shall comply with, all OSHA requirements and all other governing site safety criteria. The builder is responsible for designing, supplying, locating and installing temporary supports and bracing during erection of the building. VP bracing is designed for code required loads after building completion and shall not be considered as adequate erection bracing. See Erection Guide.

Shimming of steel buildings during erection may be required to accomodate allowable tolerances during fabrication and erection. Special care should be taken by the building erector to shim connections where key dimensions must be maintained for building performance as even small tolerances can have a significant impact on critical dimensions such as height, clearances and plumbness, especially as the size of the member or building increases. Conditions where shimming should be expected can include but are not limited to large door openings, critical clear height requirements, cranes, buildings greater than 45 feet in height, clear spans greater than 125 feet and adjacent frames with different characteristics (like clear span frames adjacent to an endwall or modular frame). Shims are normally provided by the erector, but may be ordered upon request by contacting your Project Manager.

EXISTING STRUCTURES

VP must be advised of any structure that is within 20 ft. of VP's building. Load effects from snow drifting, wind effects, and seismic separation must be considered for both the new and existing structures. VP has designed the new VP building for these effects. The owner/builder are responsible for employing a Professional Engineer to review and verify the existing structure for all load effects from the adjacent VP building.

BRACING

Tension brace rods work in pairs to balance forces caused by initial tensioning. Care must be taken while tightening brace rods so as not to cause accidental or misalignment of components. All rods must be installed loose and then tightened. Rods should not exhibit excessive sag. For long or heavy rods, or angles it may be necessary to support the rods at mid-bay by suspending them from secondary members.

Bracing for seismic or wind loading of objects or equipment that are not a part of the VP structure must be designed by a qualified professional to deliver lateral loads to primary frames and rod bracing struts. Equipment bracing and suspension connections must not impose torsion or minor axis loads, or cause local distortion in any VP components. VP accepts no responsibility for design or installation of bracing systems not furnished by VP.

FIELD WELDING

All field welding shall be done at the direction of a design professional, and done in accordance with governing requirements (AWS in USA, CWB in Canada) by welders qualified to perform the welding as directed by the applicable welding procedure specification (WPS). A WPS shall be prepared by the contractor for each welding variation specified. The contractor is responsible for any special welding inspection as required by local jurisdiction. Filler metal shall be 70 ksi (480 MPa) tensile strength. For welds in high seismic force resisting system (Seismic Cat D, E or F), minimum Charpy V-Notch toughness shall meet AISC-341 criteria (20 ft-lbs min @ 0Deg F). Interpass temperatures shall not exceed 550Deg F (300Deg C).

DELIVERIES

It is the responsibility of the builder to have adequate equipment available at the job site to unload trucks in a safe and timely manner. The Builder will be responsible for all retention charges from carriers as a result of job site unloading delays.

SIGNAGE

The Builder is responsible for furnishing signs as required by Code and the Building Department, including but not limited to, exits, occupancy limits, floor loading limits, and bulk storage limits. Floor loading signs shall clearly indicate maximum floor live load permitted. Bulk storage facilities shall have signs clearly posted on all loaded walls indicating the type of commodity stored and the maximum storage height. Signs shall be clearly visible when building is fully loaded to design level. Overloading of floors or walls may result in failure.

Claims for damage or shorts MUST be noted on the Bill-of-Lading or delivery receipt and filed against the carrier by the consignee as per VP's Terms of Sales (F.O.B. Plant) under the Uniform Commercial Code. It is critical that damages or shorts be noted on the Bill-of-Lading or you have little recourse with the carrier. Immediately upon delivery of material, material quantities are verified by the Builder against quantities billed on the shipping document. Neither the Manufacturer nor the carrier is responsible for material shortages against quantities billed on the shipping document if such shortages are not noted on the shipping documents upon delivery of material and acknowledged by the carriers agent. For materials concealed in bundles, boxes, or crates, shortages must be reported immediately upon unpacking. Should products get wet, bundled and crated materials must be unpacked and unbundled immediately to provide drainage of trapped moisture. See Erection Guide for proper job site storage procedure.

SEALANTS

Sealants shall be applied in strict accordance with VP details or weather tightness will be compromised. Sealant must be applied in temperatures and weather conditions consistent with labeling.

INDEPENDENT MEZZANINES

Independent mezzanines must be designed by a professional engineer. The engineer must ensure that proper isolation from the VP building has been provided to avoid structural damage due to differential movements, or inadvertently apply loads to the VP structure. VP accepts no responsibility for the design of the independent mezzanine.

FIRE CODE COMPLIANCE

It is the responsibility of the project design professional and builder to comply with local fire code regulations including consideration of, but not limited to, building use and occupancy, all building construction materials, separation requirements, egress requirements, fire protection systems, etc. Builder shall advise VP of any special requirements to be furnished by VP.

FIELD MODIFICATIONS

Modifications to this building from details and instructions contained on these drawings must be approved in writing by VP Buildings engineers, or other licensed structural engineer. This includes, but is not limited to, removal of roof or wall cladding, removing or moving any flange braces or rod braces, cutting of openings for doors, windows or RTU's, correction of fabrication errors, etc. The owner shall not impose loads to this structure beyond what is specified for this building in the contract documents. VP Buildings accepts no responsibility for the consequences of any unauthorized additions, alterations, or added loads to this structure.

If the builder intends to invoice VP Buildings for modifications in excess of \$1000, The builder must notify VP Buildings immediately, and obtain a Work Authorization from VP Buildings prior to proceeding. All final claims must be submitted to VP Buildings with all supporting documentation within 30 days of the building completion. Claims submitted without work authorizations, or after 30 days will not be accepted. Correction of minor misfits, shimming and plumbing, moderate amount of reaming, drilling, chipping / cutting and minor welding are considered by Code of Standard Practice to be part of erection are not subject to claim reimbursement.

CONCRETE/MASONRY/CONVENTIONAL STUD WALLS

The engineer responsible for the design of the wall system is responsible for coordinating with, or specifying to VP Buildings, any wall to steel compatibility issues such as drift and deflection compatibility, special base details, and wall to VP steel connections. All fasteners, sealant and counter flashing of wall systems are to be provided by contractor. The engineer responsible for the wall shall design the anchorage to VP supporting elements consistent with Code required forces.

PANELS

Oil canning is an inherent characteristic of cold formed steel panels. It is the result of several factors that include induced stresses in the raw material delivered to VP, fabrication methods, installation procedures, and post installation thermal forces. Thru fastened panels will exhibit some dimpling when installed, especially when insulation is installed between panels and secondary supports. Dimpling can be minimized by careful installation, taking care not to over drive fasteners.

Roof rumble is a phenomenon that is caused by wind gusts lifting up on the roof panels and then springing back into place. All panels experience this action to some degree, especially with concealed clip Standing Seam panels. Roof rumble noise may be minimized by providing a layer of blanket insulation between the panels and any hard support surface such as steel secondary members, substrates such as plywood, steel decking, or rigid board insulation. A minimum of 3 inch thick blanket is recommended over steel secondary members, or 2 inch over substrates.

Oil canning, dimpling, and roof rumble do not affect the structural integrity or weather tightness of the panels and is not grounds for rejection of panels.

The Standing Seam joint detail is designed with an interlocking feature for ease of installation. However, it is imperative that installed Standing Seam panels be secured to the secondary structural members and properly seamed prior to departure from the job site each day.

SKYLIGHTS

Local building departments may require added fall restraint due to conditions that may affect the skylight structural integrity. It is the responsibility of the builder to determine and provide any added fall restraint under the skylight as may be required by your building department.

RAIN WATER RUNOFF

Drainage systems must be designed by the project professional to comply with code requirements. VP is not responsible for drainage designs, overflow scuppers, down piping, etc. The project professional and contractor are responsible to ensure that primary drains and overflow devices such as scuppers and auxiliary drains are provided as required for the required rain intensity at the building perimeter and at valley conditions to prevent ponding.

STEEL SHOP COAT

The purpose of VP's shop coat is to provide protection for the steel members during transportation, during temporary job site storage and during erection. Standard shop formulation is not designed to perform as a finish coat when exposed to environmental conditions. Members shall be kept free of the ground and properly drained during job site storage. It is the Builder's responsibility to ensure that if a finish coat is being applied over VP shop coat that the painting contractor verifies compatibility between his finish coat and VP's shop coat.

VP BUILDINGS ACCREDITATIONS AND APPROVALS

Fabricator Approvals

IAS AC472 Approvals: (www.iasonline.org/services/metal-building-inspection)
Listed under BlueScope Buildings North America, Inc.
City of Los Angeles, CA #FB00031; City of Houston, TX 767 & 429;
City of Phoenix, AZ C19-02008; Clark County, NV 43 & 833, San Bernardino County, CA 289
State of Utah, City of Richmond, CA.

Design Approvals

IAS AC472 Approvals: (www.iasonline.org/services/metal-building-inspection)
Listed under Varco Pruden Buildings, a Division of BlueScope Buildings North America, Inc.

Canadian CSA A660 Certifications

(www.cwbgroup.org)
Listed under BlueScope Buildings North America, Inc.

Engineering Certifications of Authorization

USA--AL#CA-5589-E; AZ#22225-0; AR#576; FL#30427; GA#PEF007551; ID#C-2470; IL#184-002649;
KS#E-29; KY#4490; LA#EF6722; MS#E-0592; MO#E-2010007736; NC#F-0998; ND#1579PE;
NJ#24GA28318800; NV#20437; OH#05898; OK#CA4170PE; RI#8838; SC#6206; SD#C-1787; TX#F4828;
VA#0411001520; VA#0411001518; WA#4119; WV#C03059-00
CAN--AB#P08900; NB#F0951; NL#D0044; NS#30123; NT#P062; ON#100148796; and YT#PP134

ICC Evaluation Reports (www.icc-es.org)

SSR Roof System - #ESR-2527

State of Florida Product Approvals (www.floridabuilding.org)

Approved Products Listed Under VP Buildings, Inc.

VP TextureClad - See Transamerican Structuroc, Inc.

Dade Co. Product Approval (www.miamidade.gov/buildingcode)

Approved Products Listed Under Varco Pruden Buildings, Inc.

VP TextureClad - See Transamerican Structuroc, Inc.

Underwriter's Laboratory Approvals (Available only when specified in contract)

SSR Roof-UL#TGKX-113; SSR Composite Roof Class 90-UL#TGKX-113A;

SSR Roof w/Super Block; Class 90-UL#TGKX-328;

Panel Rib Roof UL Class 60-UL#TGKX-60; Panel Rib Roof UL Class 90-UL#TGKX-64;

VP SLR II Roof Class 90-UL#TGKX-90, -180, -435, -435A, -176, -238, -238A, -238B

Factory Mutual Approved Assemblies (Available only when specified in contract)

SSR Roof Systems are approved in various type applications and listed in FM Approval Guide.

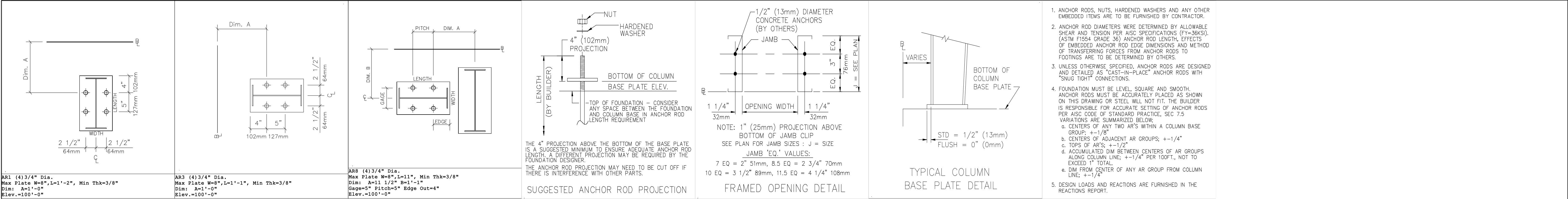
24 Ga SSR (0.0227" Nominal), is available in Class 1-60, 1-75, 1-90. 22Ga SSR (0.0277"

Nominal), is available in Class 1-75, 1-90-, 1-120.

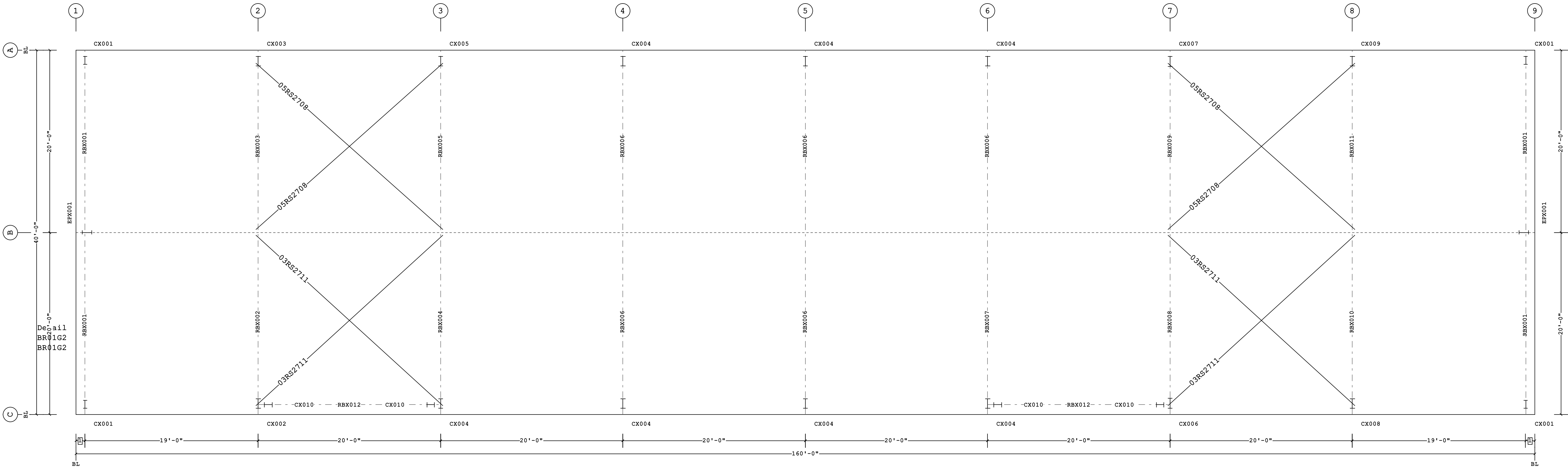
SLR II Roof Systems are approved in various type applications and listed in FM Approval Guide.

24 Ga SLR II (0.0227" Nominal), is available in Class 1-75 and 1-120.

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				Rev	Date	By	Description	Builder	<div><div><div><div></div></div><div>VP BUILDINGS</div><div>VARCO PRUDEN</div></div><div>A BlueScope Steel Company</div><div>VPC Version: 25.1.1</div></div> <div>a division of BlueScope Buildings North America, Inc.</div>			Customer	Baxter Builders LLC		Job #
								Location				La Pine, Oregon		Date	
								Project				Bath Properties		5/7/2025	
								Builders PO#						Drawn/Check	
				NTS								Page			
				5/13/2025				13:44:18		Filename:	Bath Propertie				



Bracing Part Schedule		
Part	Qty	Length
03RS2711	4	27'-11"
05RS2708	4	27'-8"



PRIMARY AND ROOF BRACING PLAN

1 1'-0"
Dimension Key

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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Shape Name = Bath Properties

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D			
VP Buildings 3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			

5/13/2025

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PRIMARY AND ROOF BRACING PLAN

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	

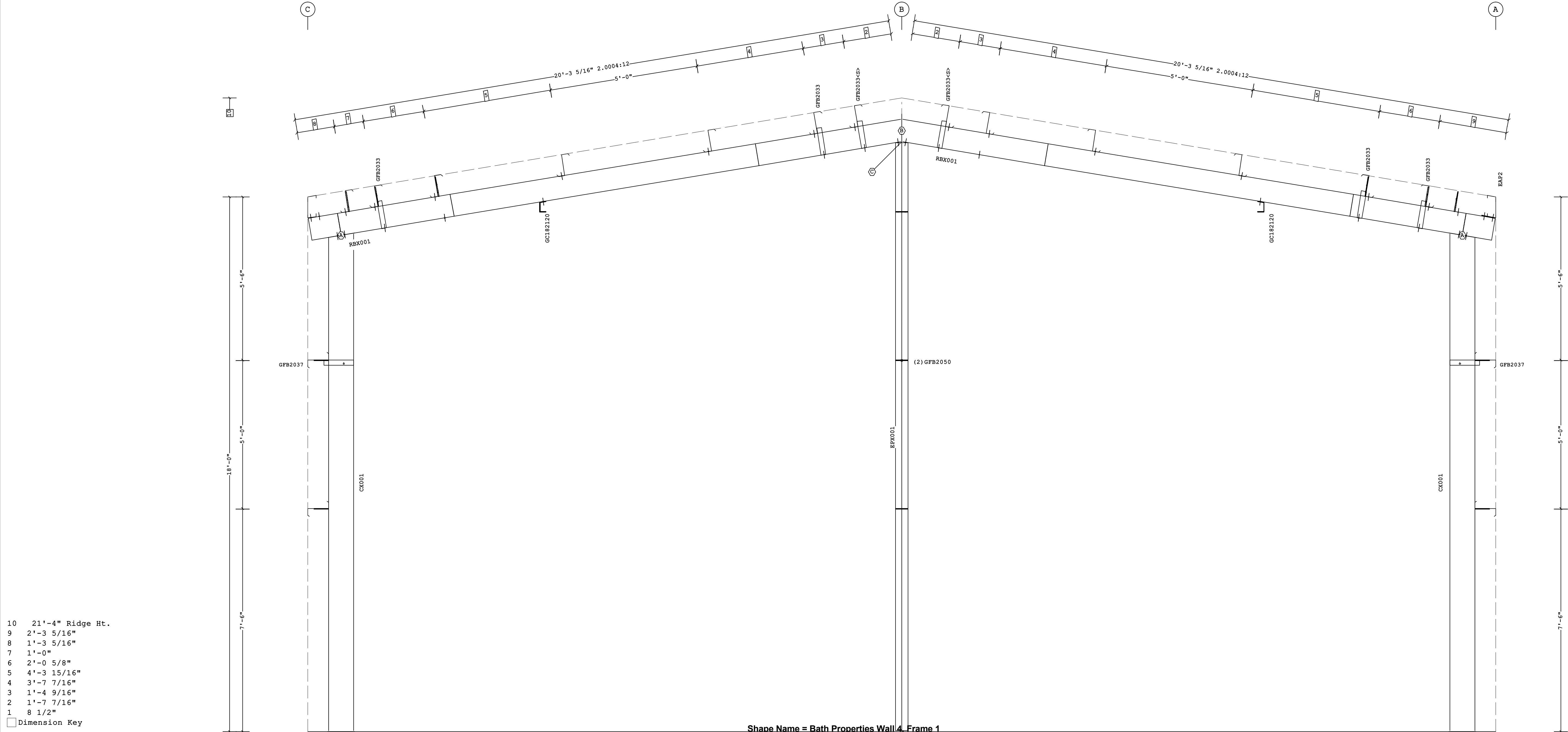


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Job #	
Date	5/7/2025
Drawn/Check	
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Frame Member Schedule										Bolt Connection & Plate Schedule										Frame Clearances									
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth	Approx.Weight	Detail	Id	Qty	Grade	Bolt	Bolt	Plate	Rows	Rows	Washer	PartNo	Horiz. Clearance between members 1 (CX001) and 8 (CX001): 36'-11"									
									Dia.				Length	Thick.	Out	In	Vert. Clearance at member 1 (CX001): 16'-9 5/16"												
CX001	1	5.0000	.1345	.1345	10"	10"	16'-9 5/16"	169#		A	4	A325	1/2"	1 1/2"	3/8"	1	1		49080	Vert. Clearance at member 8 (CX001): 16'-9 5/16"									
RBX001	2-4	5.0000	.1875	.1345	9"	9"	20'-3"	228#		B	6	A325	1/2"	1 1/2"	3/8"	2	1		49080	Vert. Clearance at member 9 (EPX001): 19'-10 1/4"									
RBX001	5-7	5.0000	.1875	.1345	9"	9"	20'-3"	228#		C	4	A325	1/2"	1 1/2"	3/8"	1	1	Yes	49080	Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)									
CX001	8	5.0000	.1345	.1345	10"	10"	16'-9 5/16"	169#		<S> - (2) Washers (095872) req'd at Flange Brace to Secondary.																			
EPX001	9	5.0000	.1345	.1345	1'-0"	1'-0"	19'-10 1/4"	215#	BR25CA																				

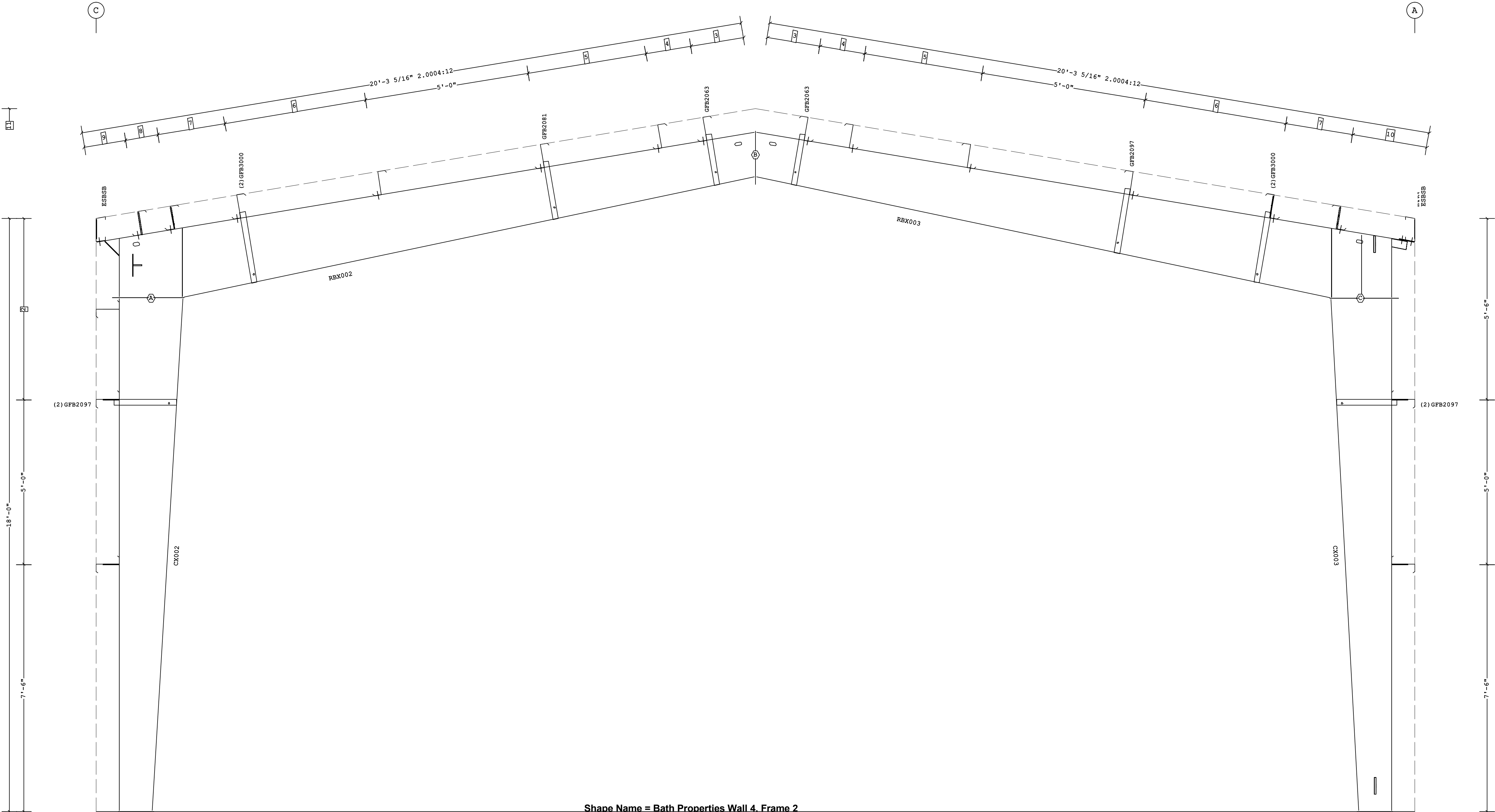


1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise. 2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.		The VP Engineer's seal applies only to the work product of VP and design and performance requirements specified by VP. The VP Engineer's seal does not apply to the performance or design of any other product or component furnished by VP except to any design or performance requirements specified by VP.		This drawing, including the information hereon, remains the property of VP Buildings. It is provided solely for erecting the building described in the applicable purchase order and may be reproduced only for that purpose. It shall not be modified, reproduced or used for any other purpose without prior written approval of VP Buildings. The general contractor and/or erector is solely responsible for accurate good quality workmanship in erecting this building in accordance with this drawing, details referenced in this drawing, all applicable VP Buildings erection guides, and industry standards pertaining to proper erection, including the correct use of temporary bracing.		D VP Buildings 3200 Players Club Circle Memphis TN 38125		FRAME CROSS SECTION AT FRAME LINE(S) 1	
Rev		Date		By		Description		Builder Baxter Builders LLC	
								Customer Bath Properties	
								Location La Pine, Oregon	
								Project Bath Properties	
								Builders PO#	
						NTS		Filename: Bath Propertie	
5/13/2025		13:44:22						a division of BlueScope Buildings North America, Inc.	

Frame Member Schedule							
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth
CX002	1	6.0000	.2500	.1345	1'-0"	1'-11"	15'-7 1/8"
RBX002	2	5.0000	.1875	.1345	2'-1"	1'-4"	20'-3"
RBX003	3	5.0000	.1875	.1345	1'-4"	2'-1"	20'-3"
CX003	4	6.0000	.2500	.1345	1'-0"	1'-10"	15'-6 15/16"
							Approx.Weight
							311#
							362#
							362#
							310#

Bolt Connection & Plate Schedule									
Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo	
A	6	A325	3/4"	2 1/2"	3/8"	2	1	0097284	
B	6	A325	3/4"	2 1/2"	3/8"	1	2	0097284	
C	8	A325	3/4"	2 1/2"	3/8"	3	1	0097284	

Frame Clearances
Horiz. Clearance between members 1 (CX002) and 4 (CX003): 34'-10"
Vert. Clearance at member 1 (CX002): 15'-7 1/8"
Vert. Clearance at member 4 (CX003): 15'-6 15/16"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 11 21'-4" Ridge Ht.
10 2'-3 5/16"
9 1'-3 5/16"
8 1'-0"
7 2'-0 5/8"
6 4'-3 15/16"
5 3'-7 7/16"
4 1'-4 9/16"
3 1'-7 7/16"
2 2 @ 2'-9"
1 8 1/2"

Dimension Key

Shape Name = Bath Properties Wall 4, Frame 2

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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VP Buildings			
3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			

5/13/2025

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FRAME CROSS SECTION AT FRAME LINE(S) 2

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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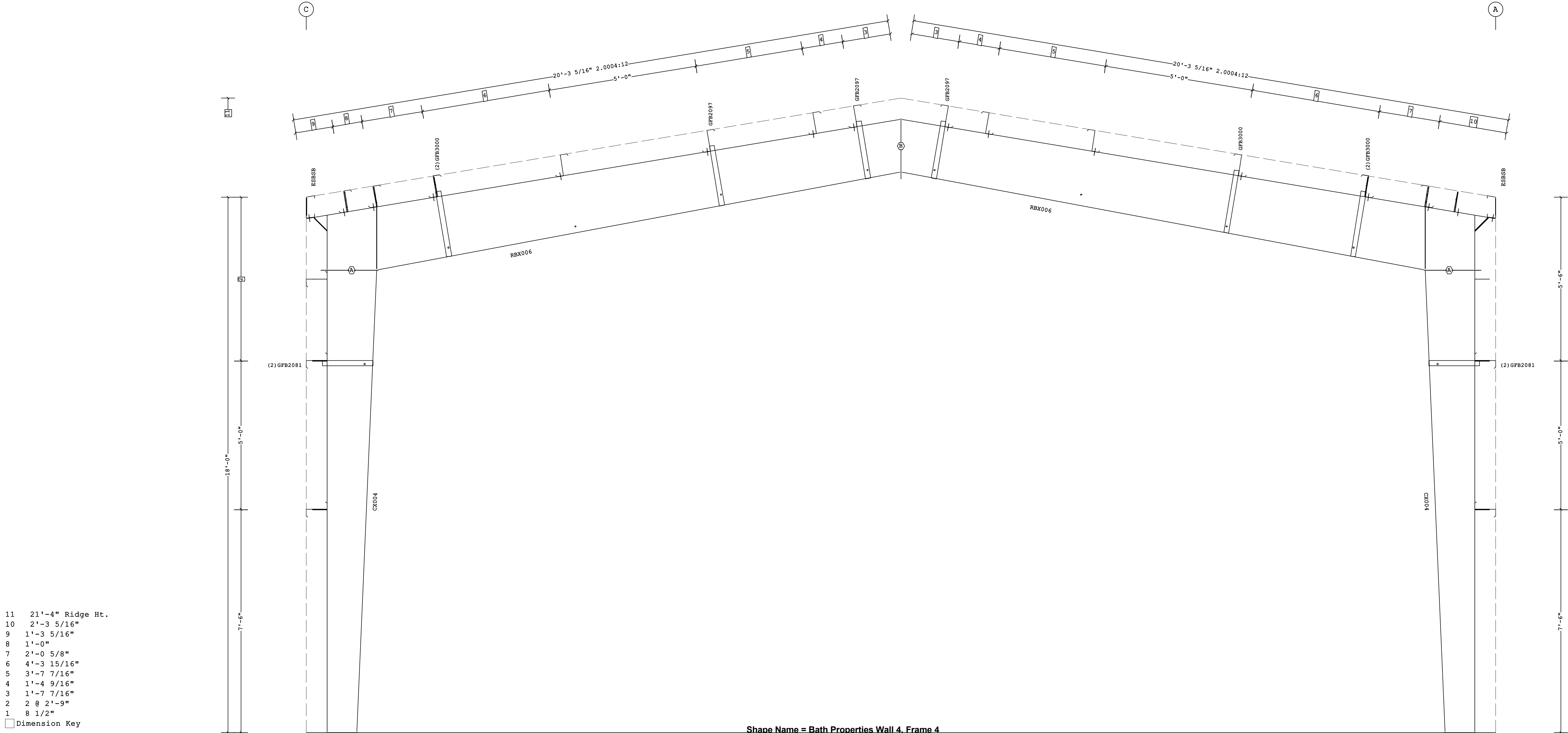
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Job #	
Date	5/7/2025
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Frame Member Schedule							
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth
CX004	1	6.0000	.2500	.1345	1'-0"	1'-8"	15'-6 5/8"
RBX006	2	5.0000	.1875	.1345	2'-1"	1'-9"	20'-3"
RBX006	3	5.0000	.1875	.1345	1'-9"	2'-1"	20'-3"
CX004	4	6.0000	.2500	.1345	1'-0"	1'-8"	15'-6 5/8"
							297#

Bolt Connection & Plate Schedule									
Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo	
A	8	A325	3/4"	2 1/2"	3/8"	3	1	0097284	
B	6	A325	3/4"	2 1/2"	3/8"	1	2	0097284	

Frame Clearances
Horiz. Clearance between members 1 (CX004) and 4 (CX004): 35'-3"
Vert. Clearance at member 1 (CX004): 15'-6 5/8"
Vert. Clearance at member 4 (CX004): 15'-6 5/8"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 11 21'-4" Ridge Ht.
- 10 2'-3 5/16"
- 9 1'-3 5/16"
- 8 1'-0"
- 7 2'-0 5/8"
- 6 4'-3 15/16"
- 5 3'-7 7/16"
- 4 1'-4 9/16"
- 3 1'-7 7/16"
- 2 2 @ 2'-9"
- 1 8 1/2"

Dimension Key

Shape Name = Bath Properties Wall 4, Frame 4

- Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
- Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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VP Buildings			
3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			
5/13/2025 13:44:27			

FRAME CROSS SECTION AT FRAME LINE(S) 4

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



A BlueScope Steel Company
VPC Version: 25.1.1

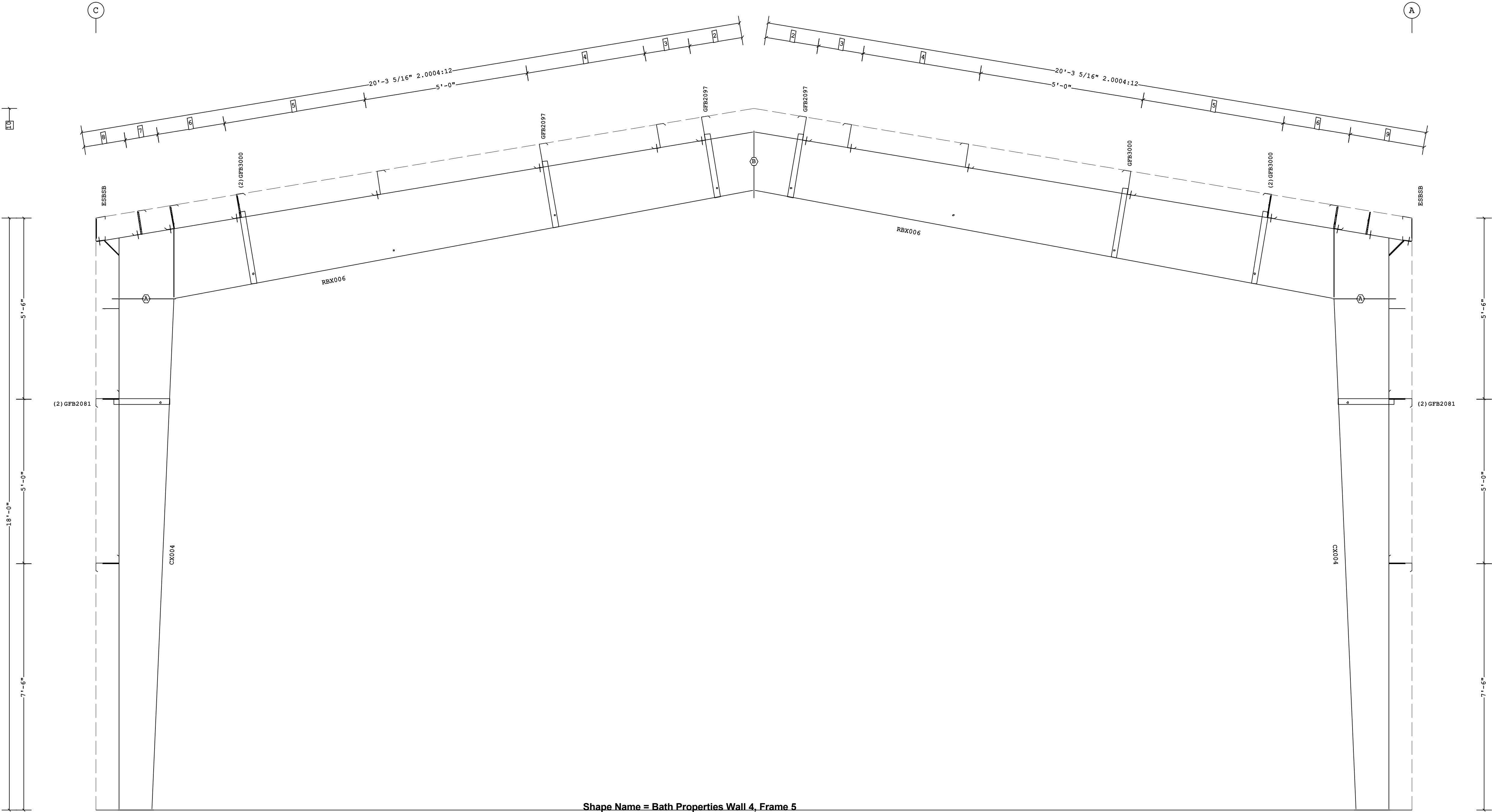
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Job #	
Date	5/7/2025
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Page	

Frame Member Schedule							
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth
CX004	1	6.0000	.2500	.1345	1'-0"	1'-8"	15'-6 5/8"
RBX006	2	5.0000	.1875	.1345	2'-1"	1'-9"	20'-3"
RBX006	3	5.0000	.1875	.1345	1'-9"	2'-1"	20'-3"
CX004	4	6.0000	.2500	.1345	1'-0"	1'-8"	15'-6 5/8"
Approx.Weight							
297#							
382#							
382#							
297#							

Bolt Connection & Plate Schedule									
Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo	
A	8	A325	3/4"	2 1/2"	3/8"	3	1	0097284	
B	6	A325	3/4"	2 1/2"	3/8"	1	2	0097284	

Frame Clearances
Horiz. Clearance between members 1 (CX004) and 4 (CX004): 35'-3"
Vert. Clearance at member 1 (CX004): 15'-6 5/8"
Vert. Clearance at member 4 (CX004): 15'-6 5/8"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 10 21'-4" Ridge Ht.
- 9 2'-3 5/16"
- 8 1'-3 5/16"
- 7 1'-0"
- 6 2'-0 5/8"
- 5 4'-3 15/16"
- 4 3'-7 7/16"
- 3 1'-4 9/16"
- 2 1'-7 7/16"
- 1 8 1/2"

Dimension Key

Shape Name = Bath Properties Wall 4, Frame 5

- Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
- Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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VP Buildings			
3200 Players Club Circle - Memphis TN 38125			
Rev	Date	By	Description
NTS			

5/13/2025

13:44:28

FRAME CROSS SECTION AT FRAME LINE(S) 5

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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VPC Version: 25.1.1

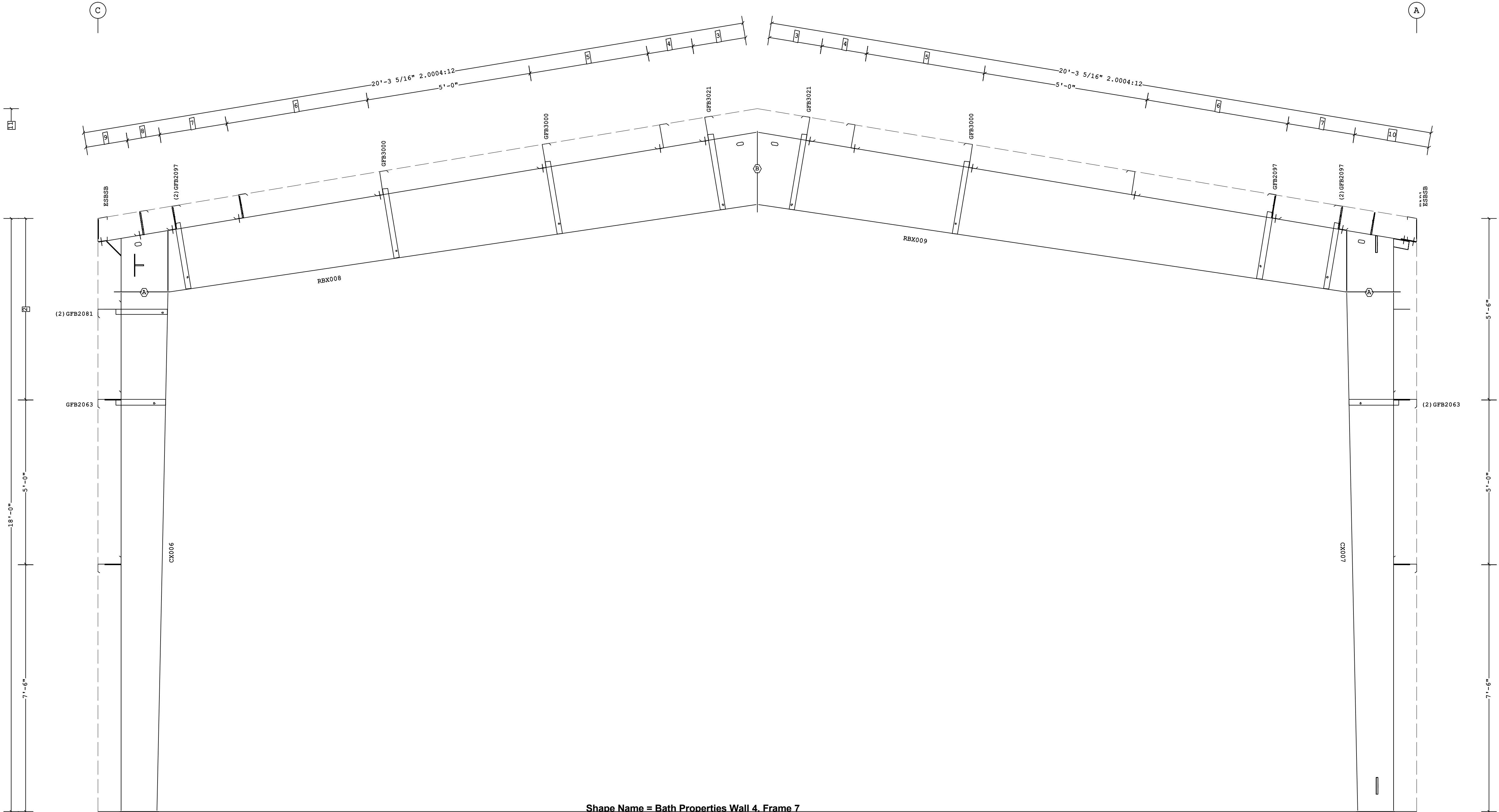
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Job #	
Date	5/7/2025
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Frame Member Schedule							
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth
CX006	1	6.0000	.2500	.1345	1'-1"	1'-5"	15'-9 1/8"
RBX008	2	5.0000	.1875	.1345	1'-10"	2'-2"	20'-3"
RBX009	3	5.0000	.1875	.1345	2'-2"	1'-10"	20'-3"
CX007	4	6.0000	.2500	.1345	1'-1"	1'-5"	15'-9 1/8"
Approx.Weight							
293#							
394#							
392#							
298#							

Bolt Connection & Plate Schedule									
Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo	
A	10	A325	3/4"	2 1/2"	3/8"	3	2	0097284	
B	8	A325	3/4"	2 1/2"	3/8"	1	3	0097284	

Frame Clearances
Horiz. Clearance between members 1 (CX006) and 4 (CX007): 35'-9"
Vert. Clearance at member 1 (CX006): 15'-9 1/8"
Vert. Clearance at member 4 (CX007): 15'-9 1/8"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 11 21'-4" Ridge Ht.
- 10 2'-3 5/16"
- 9 1'-3 5/16"
- 8 1'-0"
- 7 2'-0 5/8"
- 6 4'-3 15/16"
- 5 3'-7 7/16"
- 4 1'-4 9/16"
- 3 1'-7 7/16"
- 2 2 @ 2'-9"
- 1 8 1/2"

Dimension Key

Shape Name = Bath Properties Wall 4, Frame 7

- 1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
- 2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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VP Buildings			
3200 Players Club Circle - Memphis TN 38125			
Rev	Date	By	Description
NTS			
5/13/2025 13:44:31			

FRAME CROSS SECTION AT FRAME LINE(S) 7

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



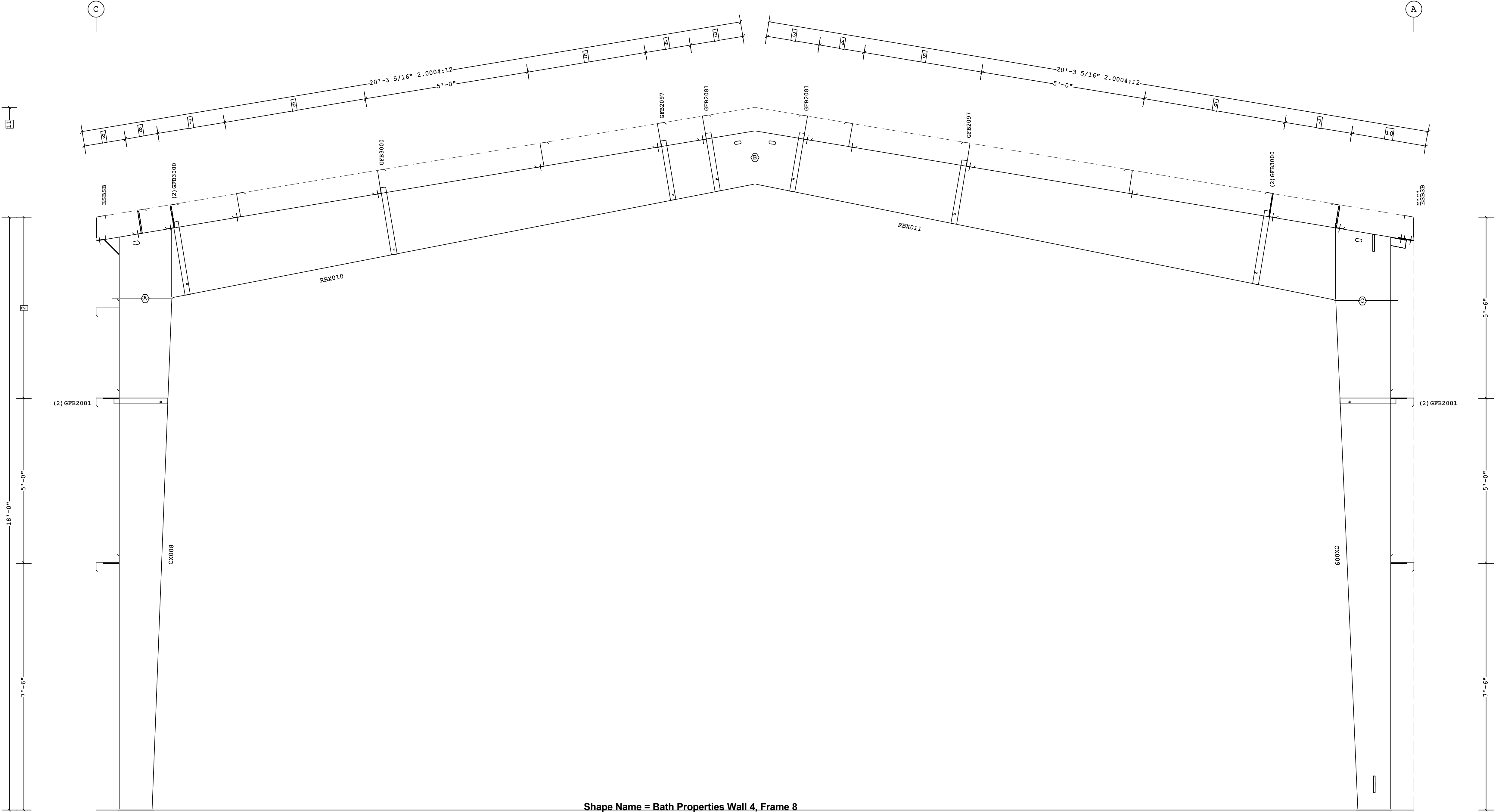
Job #	
Date	5/7/2025
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Frame Member Schedule							
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth
CX008	1	6.0000	.2500	.1345	1'-0"	1'-7"	15'-6 7/16"
RBX010	2	5.0000	.1875	.1345	2'-1"	1'-7"	20'-3"
RBX011	3	5.0000	.1875	.1345	1'-7"	2'-2"	20'-3"
CX009	4	6.0000	.2500	.1345	1'-0"	1'-8"	15'-5 9/16"
Approx.Weight							
293#							
370#							
379#							
300#							

Bolt Connection & Plate Schedule									
Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo	
A	10	A325	3/4"	2 1/2"	3/8"	4	1	0097284	
B	8	A325	3/4"	2 1/2"	3/8"	1	3	0097284	
C	8	A325	3/4"	2 1/2"	3/8"	3	1	0097284	

Frame Clearances
Horiz. Clearance between members 1 (CX008) and 4 (CX009): 35'-4"
Vert. Clearance at member 1 (CX008): 15'-6 7/16"
Vert. Clearance at member 4 (CX009): 15'-5 9/16"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 11 21'-4" Ridge Ht.
- 10 2'-3 5/16"
- 9 1'-3 5/16"
- 8 1'-0"
- 7 2'-0 5/8"
- 6 4'-3 15/16"
- 5 3'-7 7/16"
- 4 1'-4 9/16"
- 3 1'-7 7/16"
- 2 2 @ 2'-9"
- 1 8 1/2"

Dimension Key

Shape Name = Bath Properties Wall 4, Frame 8

- Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
- Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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VP Buildings			
3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
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5/13/2025 13:44:33			

FRAME CROSS SECTION AT FRAME LINE(S) 8

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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VPC Version: 25.1.1

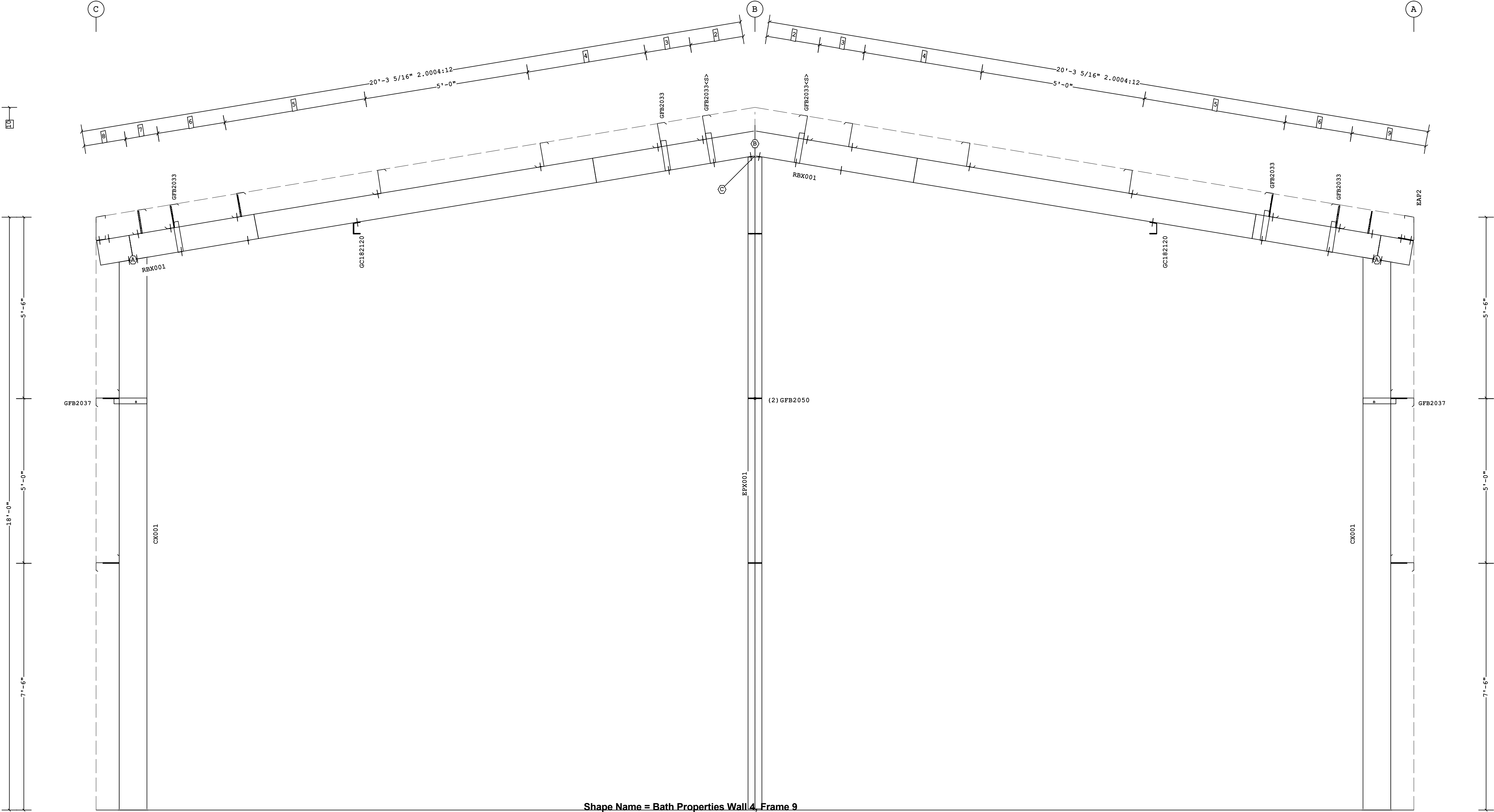
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Date	5/7/2025
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Frame Member Schedule										
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth	Approx.Weight	Detail	
CX001	1	5.0000	.1345	.1345	10"	10"	16'-9 5/16"	169#		
RBX001	2-4	5.0000	.1875	.1345	9"	9"	20'-3"	228#		
RBX001	5-7	5.0000	.1875	.1345	9"	9"	20'-3"	228#		
CX001	8	5.0000	.1345	.1345	10"	10"	16'-9 5/16"	169#		
EPX001	9	5.0000	.1345	.1345	1'-0"	1'-0"	19'-10 1/4"	215#	BR25CA	

Bolt Connection & Plate Schedule										
Id	Qty	Grade	Bolt	Bolt	Plate	Rows	Rows	Washer	PartNo	
			Dia.	Length	Thick.	Out	In			
A	4	A325	1/2"	1 1/2"	3/8"	1	1		49080	
B	6	A325	1/2"	1 1/2"	3/8"	2	1		49080	
C	4	A325	1/2"	1 1/2"	3/8"	1	1	Yes	49080	
<S> - (2) Washers (095872) req'd at Flange Brace to Secondary.										

Frame Clearances
Horiz. Clearance between members 1(CX001) and 8(CX001): 36'-11"
Vert. Clearance at member 1(CX001): 16'-9 5/16"
Vert. Clearance at member 8(CX001): 16'-9 5/16"
Vert. Clearance at member 9(EPX001): 19'-10 1/4"
Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)



- 10 21'-4" Ridge Ht.
9 2'-3 5/16"
8 1'-3 5/16"
7 1'-0"
6 2'-0 5/8"
5 4'-3 15/16"
4 3'-7 7/16"
3 1'-4 9/16"
2 1'-7 7/16"
1 8 1/2"
☐ Dimension Key

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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FRAME CROSS SECTION AT FRAME LINE(S) 9
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VP Buildings
3200 Players Club Circle Memphis TN 38125

Rev	Date	By	Description
NTS			

FRAME CROSS SECTION AT FRAME LINE(S) 9

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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VPC Version: 25.1.1

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Date	5/7/2025
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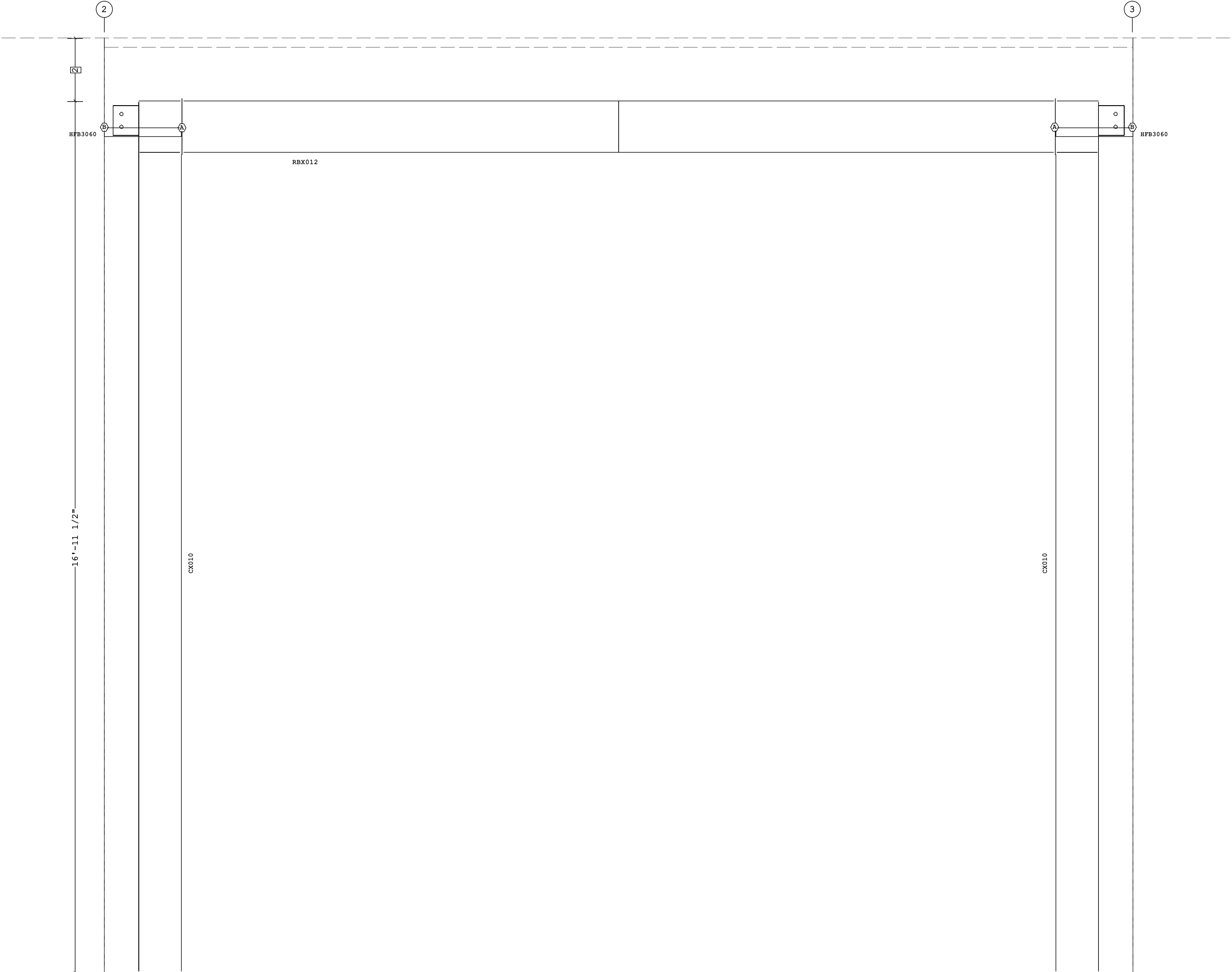
5/13/2025

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Filename: Bath Propertie

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Frame Member Schedule								Bolt Connection & Plate Schedule										Frame Clearances	
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth	Approx.Weight	Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo		
CX010	1	6.0000	.2500	.1345	10"	10"	16'-11 1/2"	269#										Horiz. Clearance between members 1 (CX010) and 4 (CX010): 17'-0"	
RBX012	2-3	5.0000	.1345	.1345	1'-0"	1'-0"	16'-11 3/4"	185#	A	4	A325	3/4"	2 1/2"	3/8"	1	1	0097284	Vert. Clearance at member 1 (CX010): 15'-11"	
CX010	4	6.0000	.2500	.1345	10"	10"	16'-11 1/2"	269#	B	2	A325	3/4"	2 1/2"	-	-	-	0097284	Vert. Clearance at member 4 (CX010): 15'-11"	
																		Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)	



2 1'-2 11/16"
1 8"
☐ Dimension Key

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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D

VP Buildings
3200 Players Club Circle Memphis TN 38125

Rev	Date	By	Description

NTS

5/13/2025

13:44:36

PORTAL FRAME ELEVATION ALONG C (a)

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	

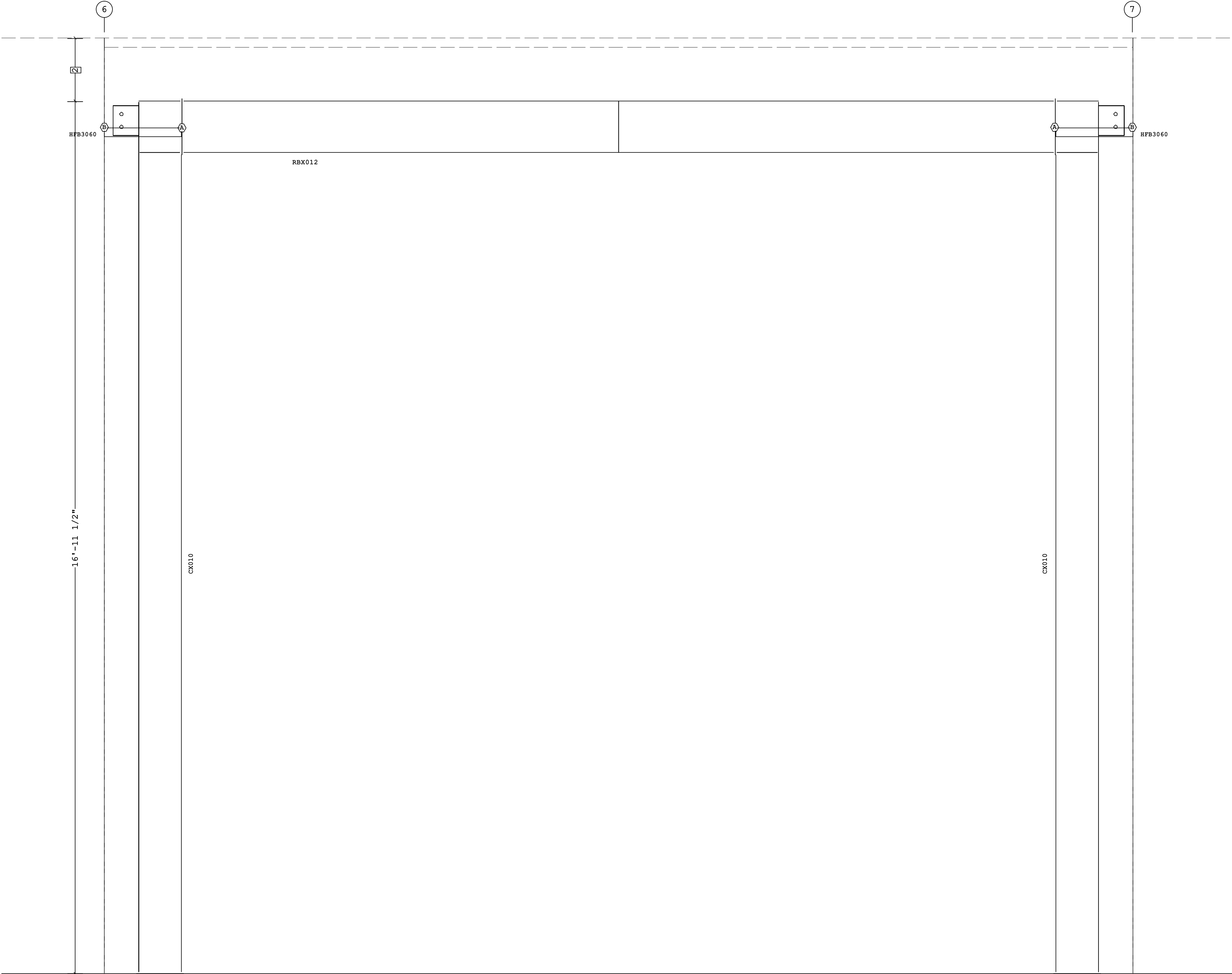


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Frame Member Schedule								Bolt Connection & Plate Schedule										Frame Clearances	
Part	Mem	Width	Thick	WebThk.	Depth1	Depth2	Approx.Lgth	Approx.Weight	Id	Qty	Grade	Bolt Dia.	Bolt Length	Plate Thick.	Rows Out	Rows In	PartNo		
CX010	1	6.0000	.2500	.1345	10"	10"	16'-11 1/2"	269#										Horiz. Clearance between members 1 (CX010) and 4 (CX010): 17'-0"	
RBX012	2-3	5.0000	.1345	.1345	1'-0"	1'-0"	16'-11 3/4"	185#	A	4	A325	3/4"	2 1/2"	3/8"	1	1	0097284	Vert. Clearance at member 1 (CX010): 15'-11"	
CX010	4	6.0000	.2500	.1345	10"	10"	16'-11 1/2"	269#	B	2	A325	3/4"	2 1/2"	-	-	-	0097284	Vert. Clearance at member 4 (CX010): 15'-11"	
																		Finished Floor Elevation = 100'-0" (Unless Noted Otherwise)	



2 1'-2 11/16"
1 8"
☐ Dimension Key

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.
2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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D

VP Buildings			
3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			

PORTAL FRAME ELEVATION ALONG C (b)

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



VP BUILDINGS
VARCO PRUDEN

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VPC Version: 25.1.1

Job #	
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5/13/2025

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Filename: Bath Propertie

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COLUMN OR BEAM WEB

ROD

HEX NUT

WASHER "A"

WASHER "B"

HILLSIDE WASHER

WEB REINFORCEMENT PLATE (IF PRESENT) MAY BE SHOP WELDED ON EITHER SIDE OF THE WEB.

DESCRIPTION/PART NO				
ROD DIA.	NUT	HARD STEEL ROUND WASHER A	HARD STEEL WASHER B	HILLSIDE WASHER
3/8"	95321	3/8" FLAT WASHER (96408)	1/2" BEVEL SQUARE WASHER (46040)	
1/2"	95230	1/2" FLAT WASHER (95872)	3/4" FLAT ROUND WASHER (95946)	543334
5/8"	95233	5/8" FLAT WASHER (95945)		
3/4"	95235	3/4" FLAT WASHER (95946)	1" FLAT ROUND WASHER (95948)	543335
7/8"	95237	7/8" FLAT WASHER (95947)		
1"	95238	1" FLAT WASHER (95948)	1 1/8" FLAT ROUND WASHER (95949)	543336
1 1/8"	95239	1 1/8" FLAT WASHER (95949)		

REV. DATE:08/02/17

REV. NO.04

BR01G2

ROD BRACE
WEB SLOT ASSEMBLY

ROD DIA.	CLEVIS MARK NO.	CLEVIS PIN DIA.
3/8"	097572	
1/2"	097573	
5/8"	095277	
3/4"	095278	
7/8"	095279	
1"	095280	
1 1/8"	095281	
1 1/4"	095282	
1 3/8"	097593	
1 1/2"	097594	

CLEVIS

COTTER PIN

BRACE ROD

CLEVIS PIN (WITH SHOULDER) MAY BE FURNISHED WITHOUT A SHOULDER, THEN TWO COTTER PIN WILL BE PROVIDED.

REV. DATE:06/23/11

REV. NO.03

BR02K1

ROD BRACING
CLEVIS ASSEMBLY

BRACE LOCATION

FRAME

BRACE LOCATION

ROOF SECONDARY MEMBER

FLANGE BRACE (GFB-) OR (HFB-)

CLIP REQ'D W/ THICK FLANGES

FLANGE BRACE WILL TYPICALLY CONNECT TO THE WEB OF THE FRAME MEMBER.

FIELD NOTE

THE BUILDER WILL HAVE TO FIELD REAM-OUT THE 9/16" DIAM. HOLE IN THE SECONDARY MEMBERS TO 13/16" DIAM. HOLES. (FOR THE 12MDB- FLANGE BRACE WITH A 3/4" X 2 1/2" A325 BOLT WHEN REQUIRED.)

FLANGE BRACE REQUIREMENTS:

RULE#1- ALL FLANGE BRACES ON CROSS SECTIONS MUST BE INSTALLED.

RULE#2- SINGLE FLANGE BRACES ARE REQUIRED WHEN PART MARK ON CROSS SECTION IS NOT ACCOMPANIED BY (2).

RULE#3- FLANGE BRACES ARE REQUIRED BOTH SIDES OF THE FRAME WEB WHEN PART MARK IS ACCOMPANIED BY (2).

RULE#4- WHENEVER POSSIBLE, PLACE SINGLE BRACES TOWARD THE CENTER OF THE BUILDING.

RULE#5- WHENEVER POSSIBLE, PLACE ALL SINGLE BRACES ON THE SAME SIDE OF THE FRAME WEB.

** 10" & 11 1/2" PURLINS REQUIRE 3 BOLTS AT EACH END OF PURLIN LAP.

REV. DATE:05/08/18

REV. NO.02

BR06AE

TYPICAL FLANGE BRACE CONNECTIONS
CONT. PURLIN LAP SHOWN, CONT. GIRT & SIMPLE PURLIN

(2 OR 4) A325 BOLTS

PORTAL FRAME

PRIMARY FRAME

1/2" x 1 1/2" A-325 BOLT (49080) TYP.

FLANGE BRACE REQUIREMENTS:

PORTAL COLUMN DEPTH	FLANGE BRACE
≤ 24"	HFB3060
> 24" & ≤ 36"	HFB4106
> 36"	HFB6032

NOTES:

① FIELD DRILL 9/16" HOLES IN PRIMARY AND PORTAL FRAME WEBS FOR FLANGE BRACE CONNECTIONS.

② DRILL 9/16" HOLE IN PORTAL FRAME COLUMN WEB APPROX. 2" FROM BOLTING PLATE ⊕ BEAM ⊕.

REV. DATE:03/17/16

REV. NO.03

BR12J1

PORTAL FRAME DETAIL
CONNECTION TO FRAME

PURLIN

ENDPOST

RAKE BEAM

NOTE: ALL BOLTS TO BE 1/2" X 1 1/2" A325 BOLTS (49080) (U.N.O.)

1/2" x 1 1/2" A325 BOLTS (49080) WITH HARDENED WASHER (095872) EACH SIDE (TYP AT PURLIN END OF BRACE)

FLANGE BRACE (GFB- OR HFB-)

REV. DATE:11/16/19

REV. NO.03

BR25CA

ENDPOST TOP BETWEEN PURLINS
NO CAP CHANNEL - BRACED TO PURLINS ONLY

F = FEET G = GAGE

I = INCHES O = OPERATION

E = EIGHTHS C = FIN/COLOR

PANEL/COVERING

W 1 3 1 1 7 2 6 1 K T D

* F F I I E G G O C C C

LENGTH CODE

INSULATION

I B 1 3 0 1 0 3 6 0 3 0 W V

** F F F I I I I I I E C C

LENGTH WIDTH THK CODE

SECONDARY (STANDARD)

O B Z 1 9 1 1 4 1 7 - - - - -

* * * F F I I E G C * + + + + *

DEPTH LENGTH GAGE ADJUST.CODES

SHAPE

SECONDARY (SPECIAL)

O 0 1 0 8 Z 1 9 1 1 4 1 7 - - -

* * * * * F F I I E G G * * *

COUNTER DEPTH & LENGTH GAGE ADJUST CODES

SHAPE

ROD BRACING

O 3 R S 2 5 1 0

I E * * F F I I

DIA LENGTH

RS = THREADS BOTH ENDS

RT = THREADS ONE END - CLEVIS ONE END

RU = CLEVIS BOTH ENDS

RP = THREAD BOTH ENDS - NO HILLSIDES

CX*** = COLUMN (PLATE)

CGX*** = COLUMN (GAGE)

WCX*** = COLUMN (HOTROLL)

RBX*** = RAFTER (PLATE)

BGX*** = RAFTER (GAGE)

WRX*** = RAFTER (HOTROLL)

TRX*** = TRUSS RAFTER

ICX*** = INTERIOR COLUMN

PCX*** = PIPE COLUMN

TOX*** = TUBE COLUMN

EPX*** = ENDPOST (PLATE)

EGX*** = ENDPOST (GAGE)

CBX*** = CANOPY (PLATE)

CBX*** = PIGGYBACK CANOPY

DCC*** = 8 1/2" GAGE POST

DCE*** = 10" GAGE POST

REV. DATE:06/29/12

REV. NO.01

EN50B1

MARK NUMBER KEY
COMMON GENERATED MARK NUMBERS

BASIC ERECTION GUIDE REQUIRED FOR THIS PROJECT:

REFER TO:

VARCO PRUDEN BUILDINGS

BASIC ERECTION GUIDE

The Field Guide for correctly storing and erecting Varco Pruden Metal Building Systems

BACK COVER: 4001 BASIC ERECTION GUIDE

REV. DATE:01/30/14

REV. NO.00

ENV002

BASIC ERECTION GUIDE - STRUCTURAL

RAKE BEAM

1/2" x 1 1/2" A325 BOLTS (49080) FILL ALL HOLES

ENDPOST

TO B.L.

OUTSET GIRTS

1'-3" AT 10" & 11 1/2" E.W. GIRTS

1'-0" AT 7" & 8 1/2" E.W. GIRTS

6" AT ALL INSET GIRTS

REV. DATE:07/01/09

REV. NO.02

PF10C2

RAKE BEAM CONNECTION
TO ENDPOST

1. Use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections, secondary clip connections, and flange brace connections, unless noted otherwise.

2. Slot reinforcement plates need not be located on the same side of the web as the hillside washer.

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D

VP Buildings

3200 Players Club Circle Memphis TN 38125

Rev	Date	By	Description
NTS			
5/13/2025 SEDSheet 13:44:37			

PRIMARY BRACING SED'S

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders POW	
Filename:	Bath Propertie

A BlueScope Steel Company

VPC Version: 25.1.1

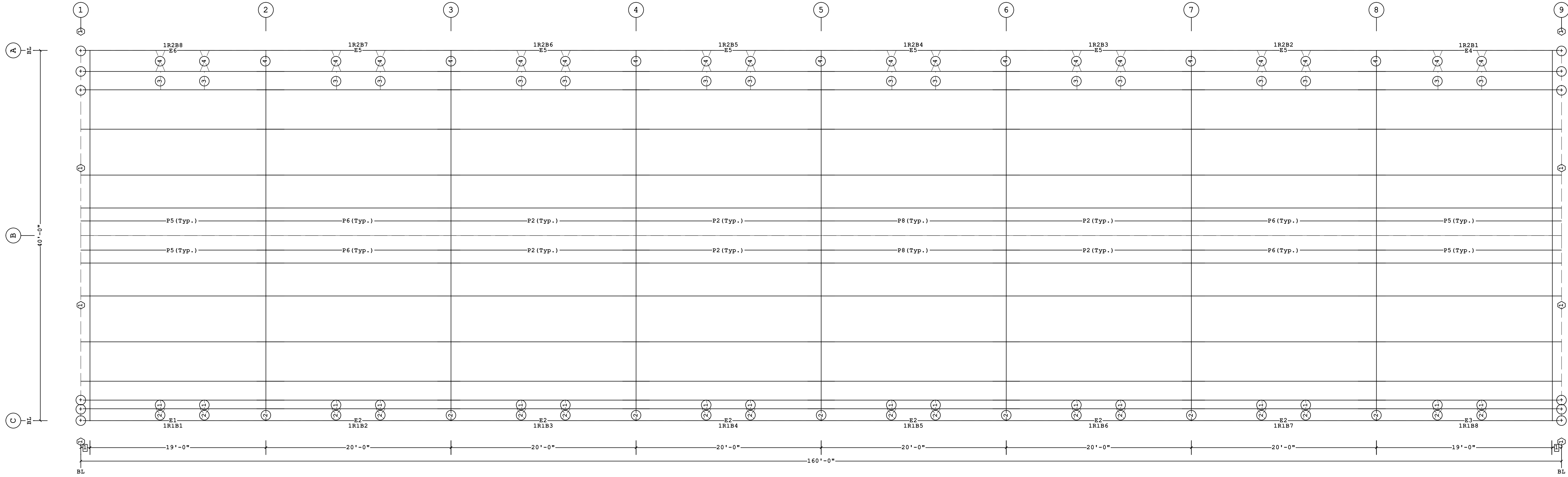
a division of BlueScope Buildings North America, Inc.

Job #	
Date	5/7/2025
Drawn/Check	
Page	

Secondary Part Schedule				
Mark	Part	Thick.	Depth	Lap
E1	00108ES1911417B02	0.0600	8 1/2"	
E2	08E1911417DDB02	0.0600	8 1/2"	
E3	00208ES1911417B02	0.0600	8 1/2"	
E4	00308ES1911411B02	0.1130	8 1/2"	
E5	08E1911411DDB02	0.1130	8 1/2"	
E6	00408ES1911411B02	0.1130	8 1/2"	
P2	08Z221141522B0	0.0730	8 1/2"	1'-4 1/2"
P5	08Z2111412A3B0	0.0980	8 1/2"	1'-10 1/2"
P6	08Z211141511B0	0.0730	8 1/2"	10 1/2"
P8	08Z221141622B0	0.0680	8 1/2"	1'-4 1/2"

Secondary Bracing Schedule			
Id	Qty	Mark No	Spacing
1	16	CPBB010108 (Typ.	1'-0"
2	39	PBA0107	1'-3 5/16"
3	16	CPBB020204	2'-0 5/8"
4	39	PBA0207	2'-3 5/16"
See SED:			
BR09JH, BR09JG, BR09JR, BR09RY, BR09RZ			
BR09K5, BR09K2			

Part Mark Key
1 RKC15
(+) SSR Fixed Clip Location



1 1'-0"
Dimension Key

Shape Name = Bath Properties

- Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.
- Flange Braces are an integral part of the stability of the structural system and must be properly installed prior to erection of wall and roof sheets.
- Removal or alteration of any component is prohibited.

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D

VP Buildings
3200 Players Club Circle Memphis TN 38125

ROOF SECONDARY PLAN

Rev	Date	By	Description

NTS

5/13/2025

13:44:40

Builder Baxter Builders LLC

Customer

Location La Pine, Oregon

Project Bath Properties

Builders POW

Filename:

Bath Propertie



A BlueScope Steel Company
VPC Version: 25.1.1

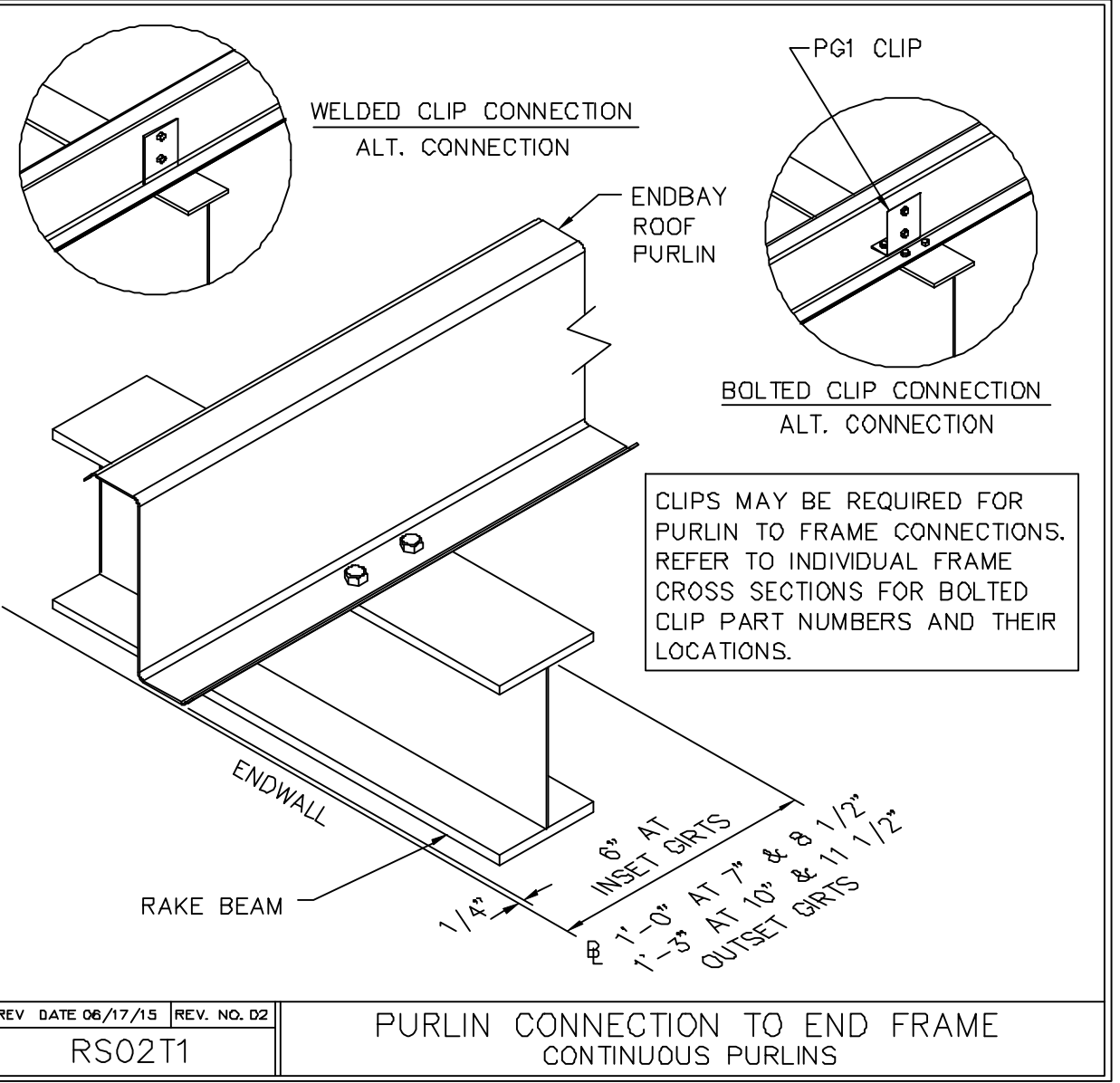
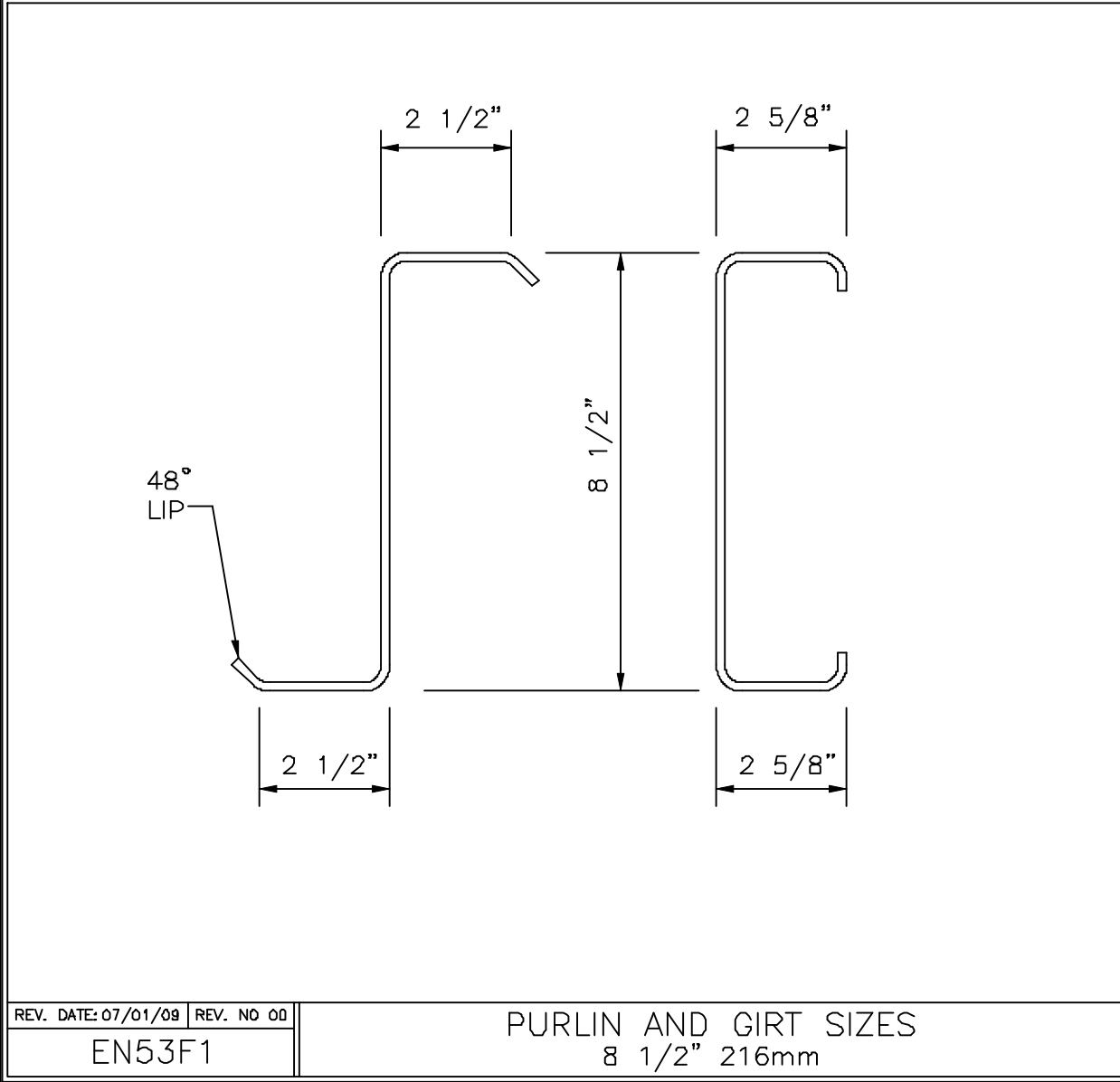
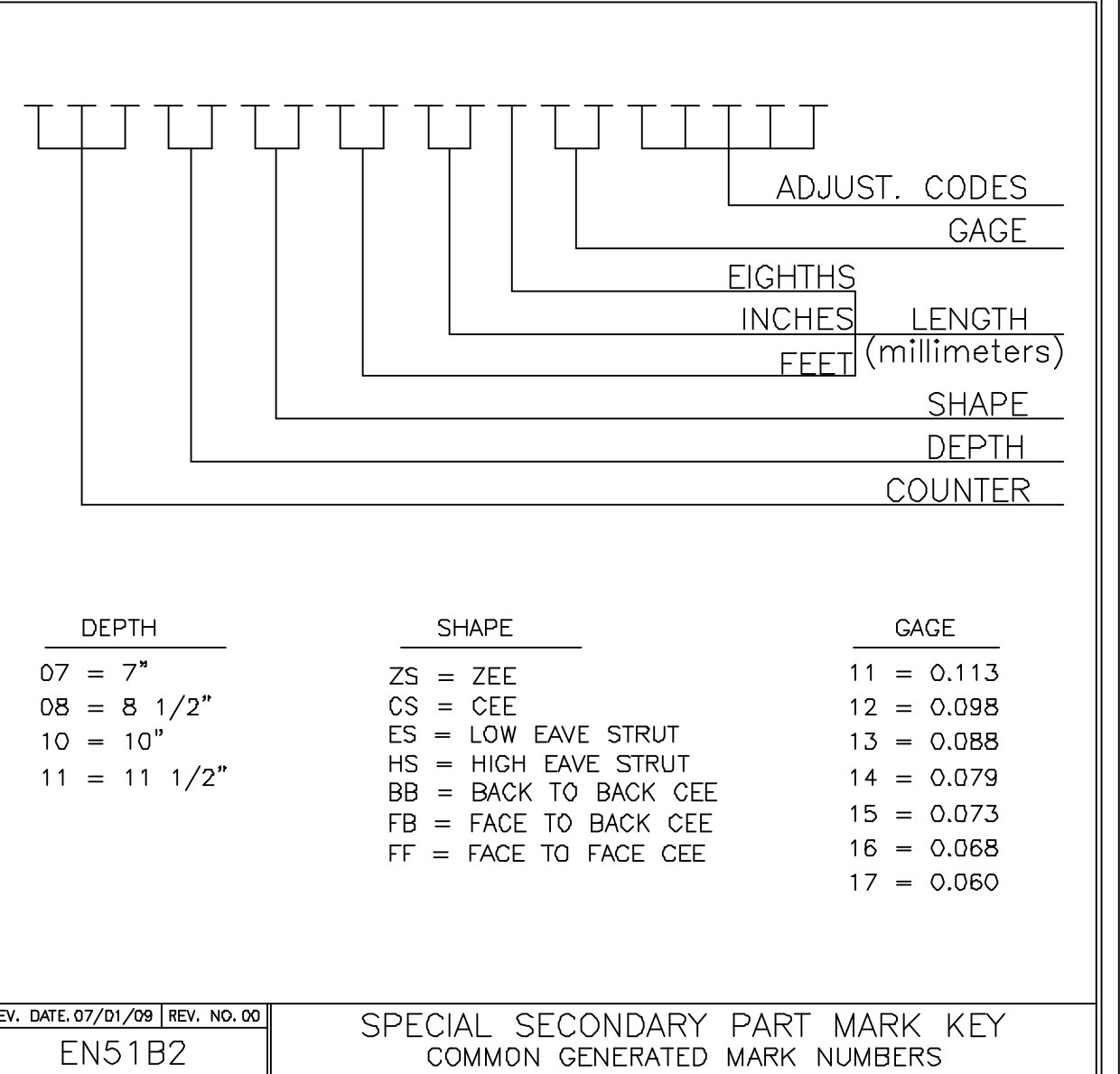
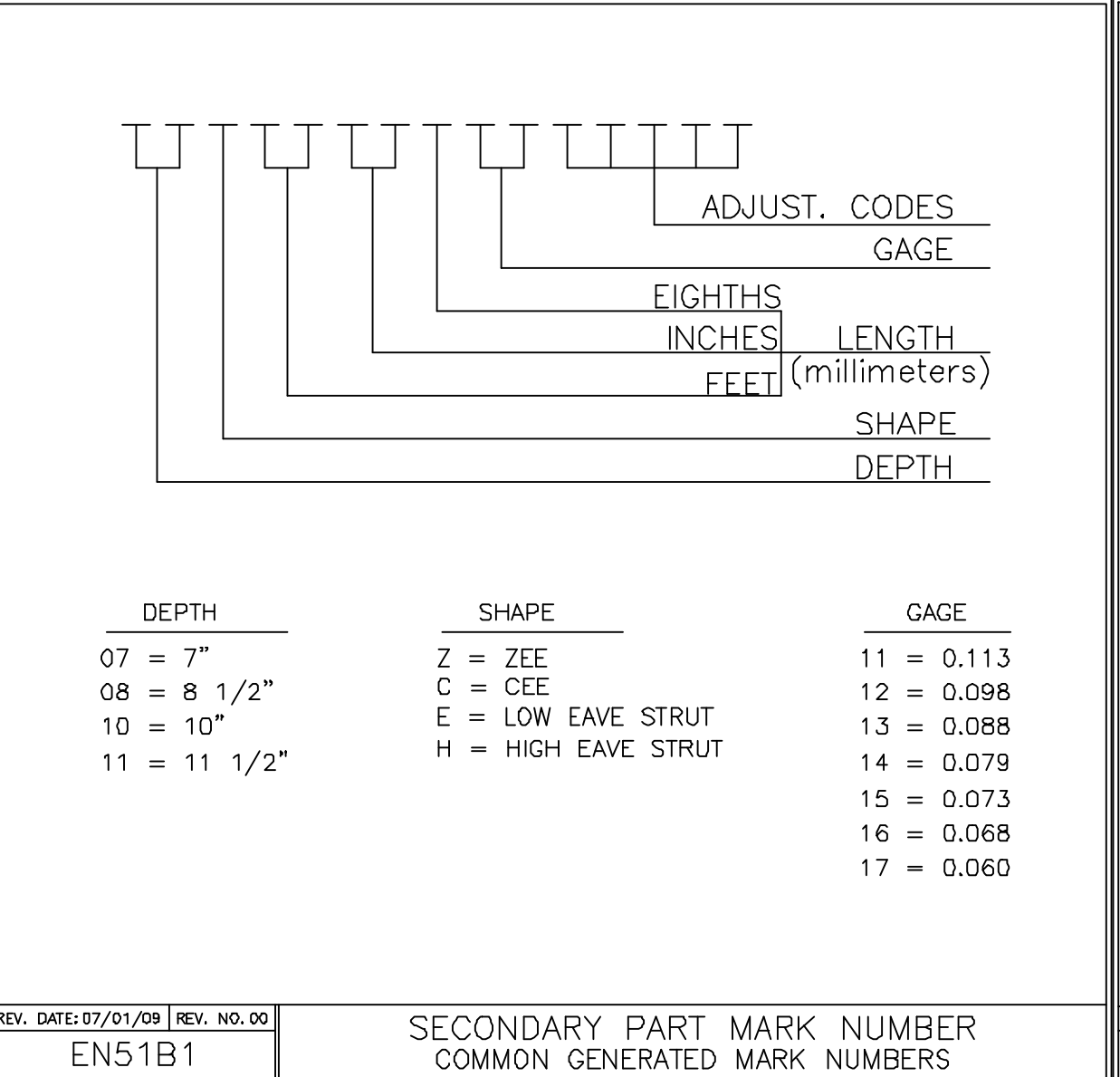
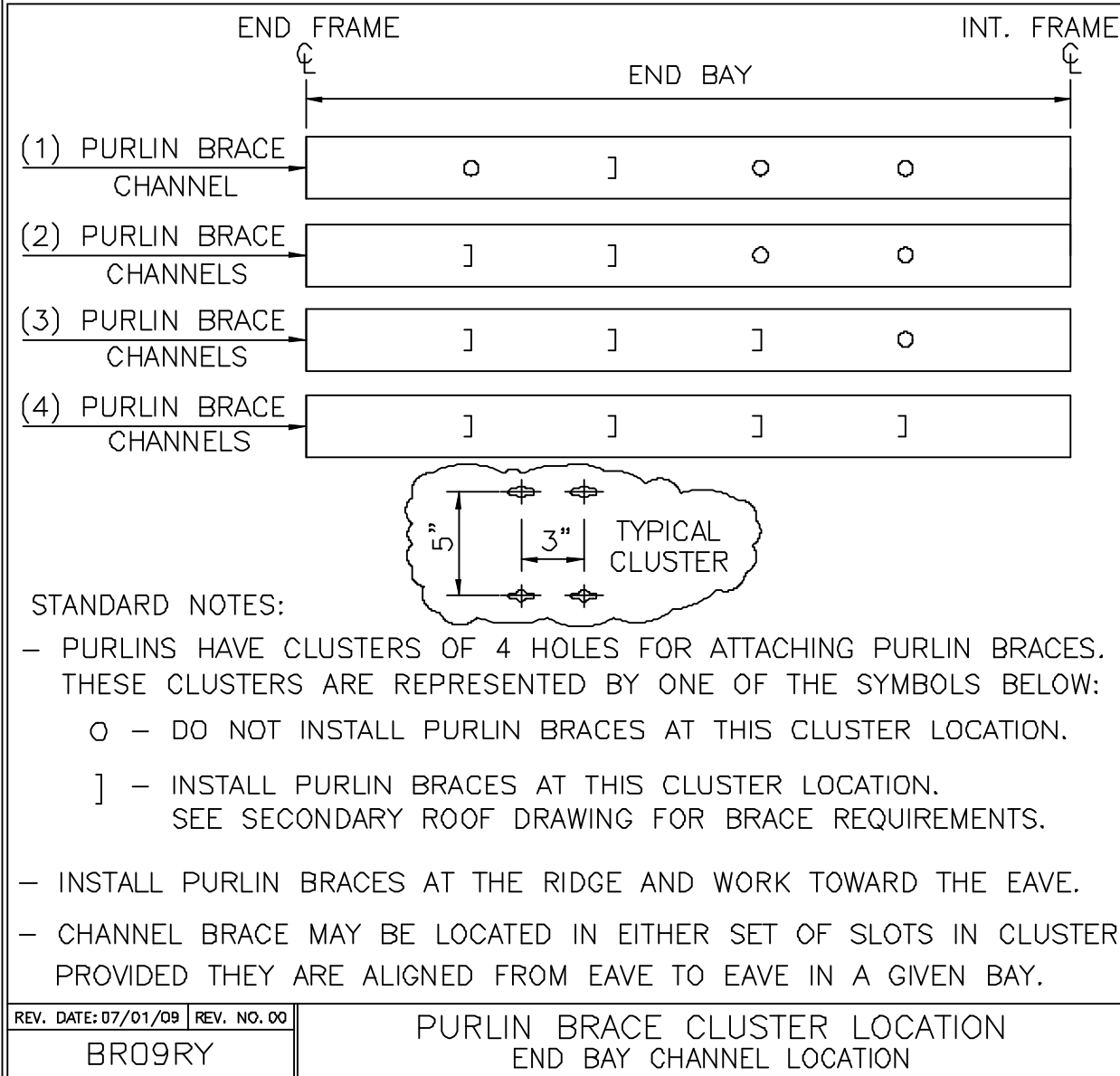
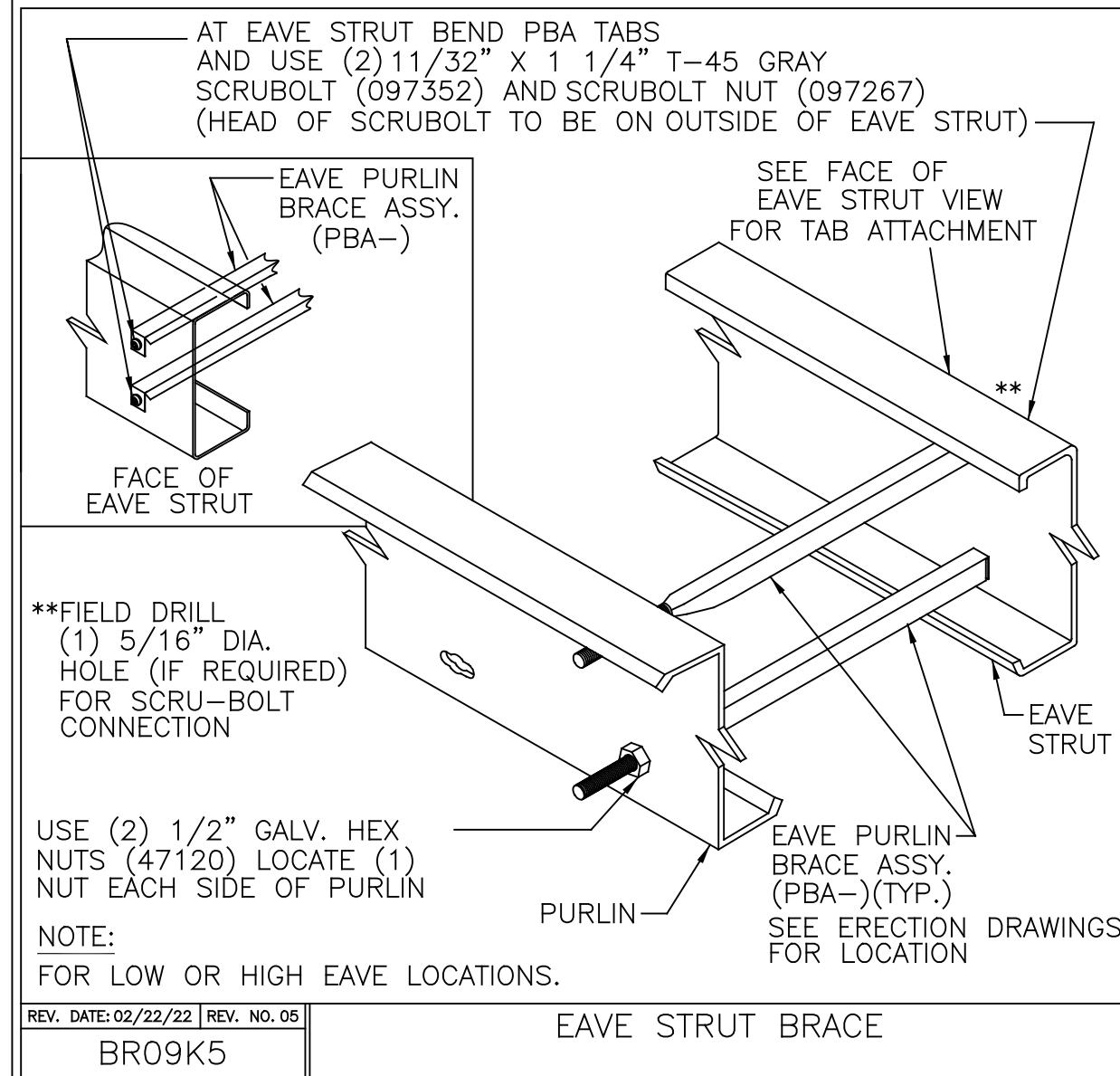
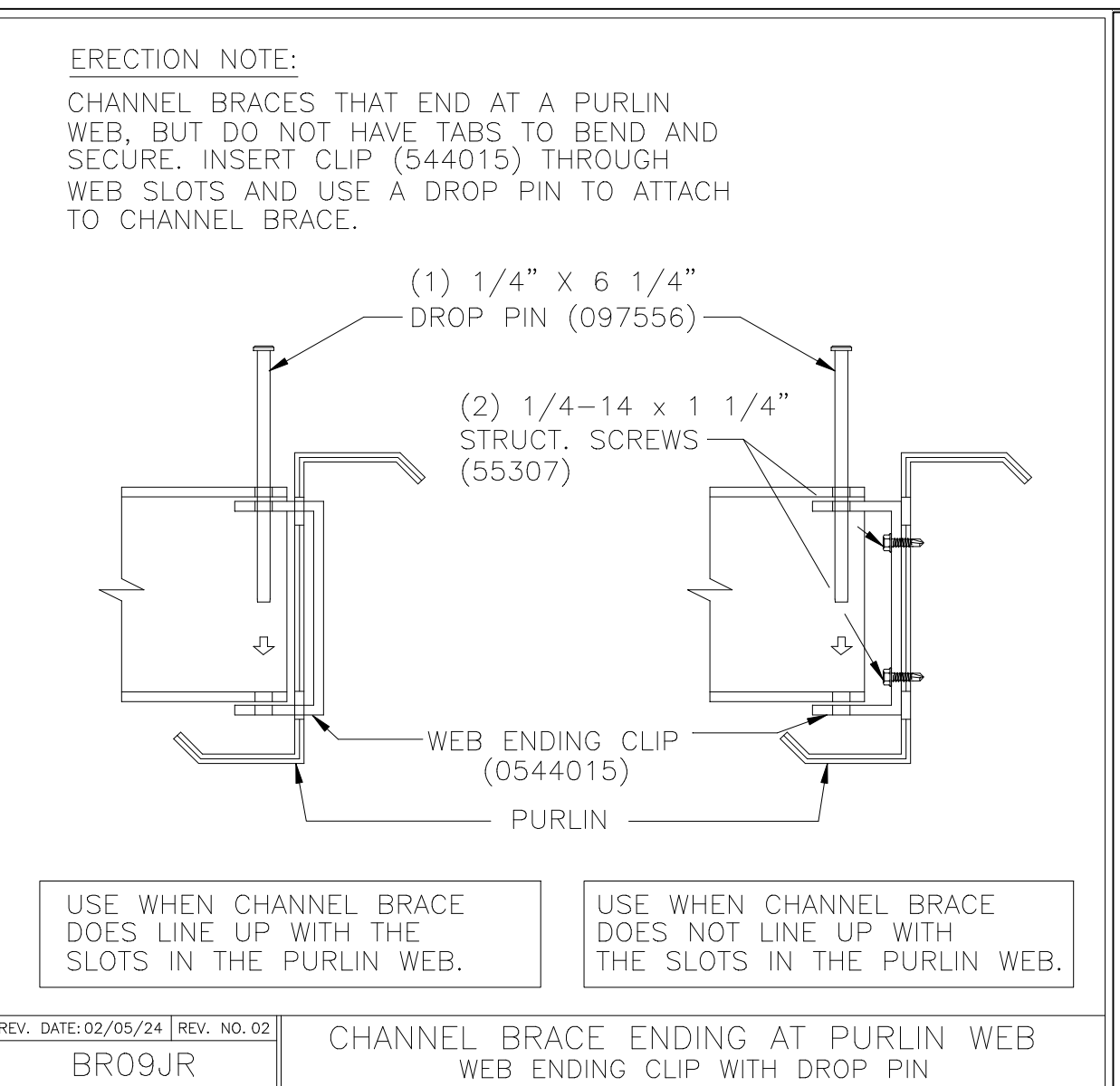
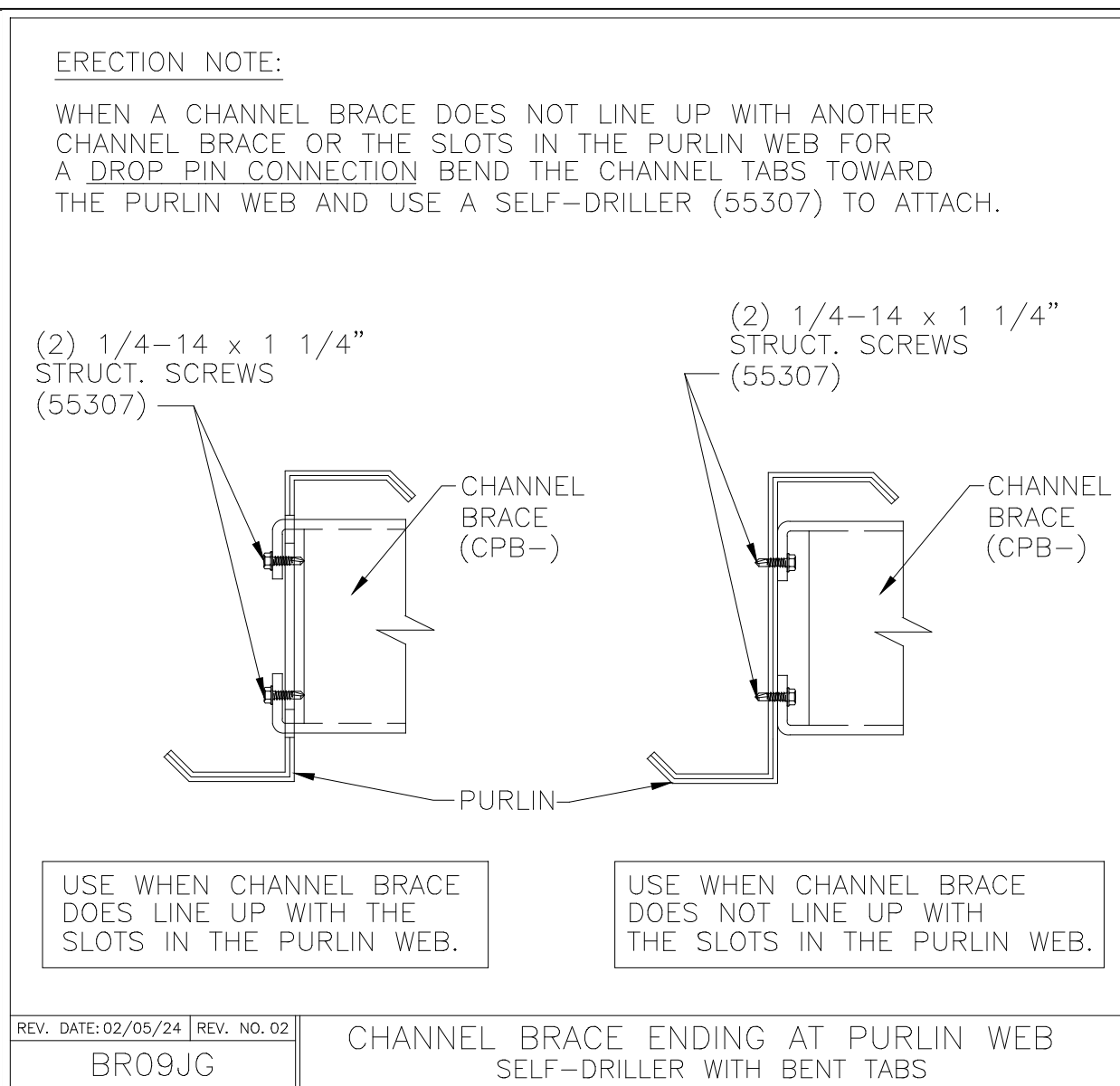
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Job #

Date 5/7/2025

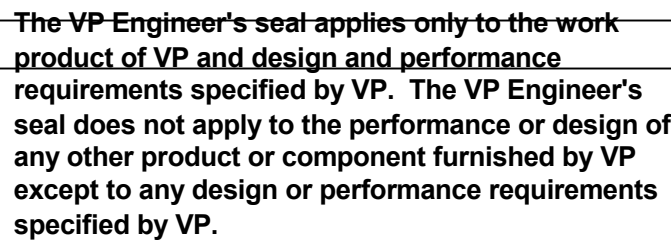
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Page



<div>1. Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.</div> <div>2. Flange Braces are an integral part of the stability of the structural system and must be properly installed prior to erection of wall and roof sheets.</div> <div>3. Removal or alteration of any component is prohibited.</div>	<div>The VP Engineer's seal applies only to the work product of VP and design and performance requirements specified by VP. The VP Engineer's seal does not apply to the performance or design of any other product or component furnished by VP except to any design or performance requirements specified by VP.</div>	<div>This drawing, including the information hereon, remains the property of VP Buildings. It is provided solely for erecting the building described in the applicable purchase order and may be reproduced only for that purpose. It shall not be modified, reproduced or used for any other purpose without prior written approval of VP Buildings.</div> <div>The general contractor and/or erector is solely responsible for accurate good quality workmanship in erecting this building in accordance with this drawing, details referenced in this drawing, all applicable VP Buildings erection guides, and industry standards pertaining to proper erection, including the correct use of temporary bracing.</div>	<div>D</div> <div>VP Buildings</div> <div>3200 Players Club Circle Memphis TN 38125</div>	ROOF SECONDARY SED'S (a)							
				Rev	Date	By	Description	Builder	Baxter Builders LLC	<div><div><div><div></div></div><div>VP BUILDINGS</div><div>VARGO PRUDEN</div></div><div>A BlueScope Steel Company</div><div>VPC Version: 25.1.1</div></div> <div>a division of BlueScope Buildings North America, Inc.</div>	<div>Job #</div> <div>Date</div> <div>5/7/2025</div> <div>Drawn/Check</div> <div>Page</div>
								Customer			
								Location	La Pine, Oregon		
								Project	Bath Properties		
				NTS				Builders PO#		Filename:	Bath Propertie
5/13/2025 SEDSheet				13:44:40							

1. Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.
2. Flange Braces are an integral part of the stability of the structural system and must be properly installed prior to erection of wall and roof sheets.
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20'-0"
SECONDARY ELEVATION AT 1

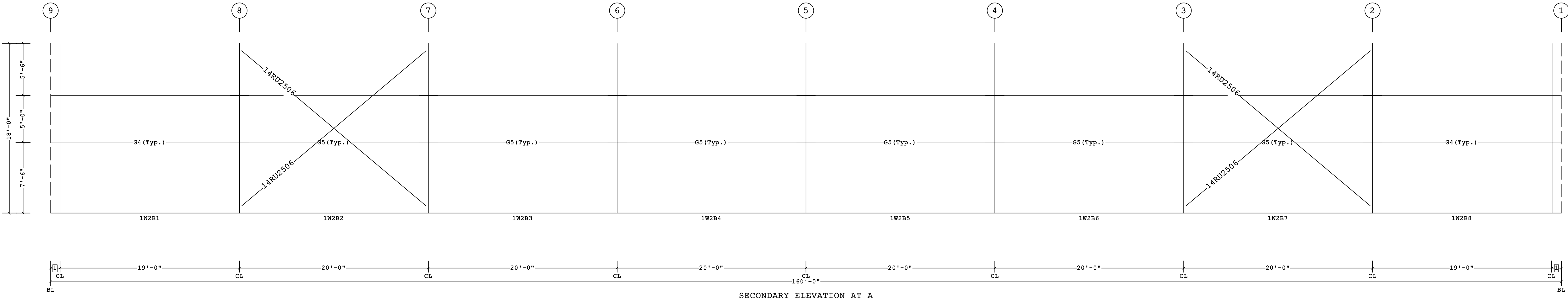
VP BUILDINGS
VARCO PRUDEN
A BlueScope Steel Company
VPC Version: **25.1.1**

Job #	
Date	5/7/2025
Drawn/Check	
Page	

Secondary Part Schedule					
Mark	Part	Thick.	Depth	Lap	Detail
G4	08Z2011417A100	0.0600	8 1/2"	10 1/2"	WS12A2,WSR065,WS01G3
G5	08Z21114171100	0.0600	8 1/2"	10 1/2"	WSR065,WS01G3

Bracing Part Schedule		
Part	Qty	Length
14RU2506	4	25'-6"

Detail
BR02K1



1 1'-0"
Dimension Key

- Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.
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VP Buildings 3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			

SECONDARY ELEVATION AT A

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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VPC Version: 25.1.1

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Date	5/7/2025
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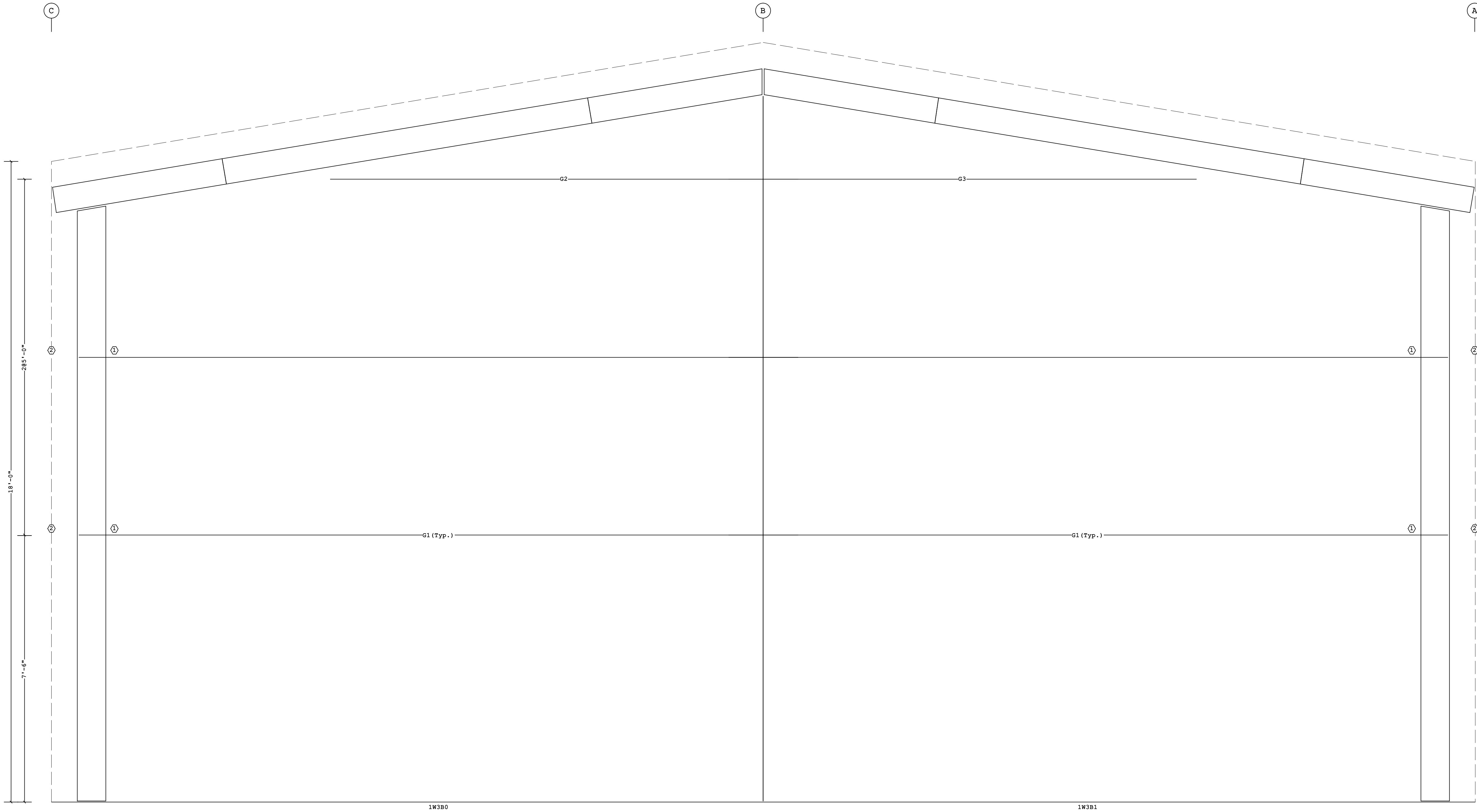
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5/13/2025

13:44:45

Filename: Bath Propertie

Secondary Part Schedule					
Mark	Part	Thick.	Depth	Lap	Detail
G1	08Z2002417Q100	0.0600	8 1/2"	10 1/2"	WS12A2,WSR065,WS01G3
G2	00108ZS130171700	0.0600	8 1/2"	10 1/2"	WS12A2,WS01G3,WSR065,WS04C2
G3	00208ZS130171700	0.0600	8 1/2"	10 1/2"	WS12A2,WS04C2,WSR065,WS01G3



- 2 GFA106
1 VCC07003090
○ Part Mark Key

1. Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.
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VP Buildings
3200 Players Club Circle - Memphis TN 38125

Rev	Date	By	Description

NTS

5/13/2025

13:44:46

SECONDARY ELEVATION AT 9

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



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VPC Version: 25.1.1

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Job #

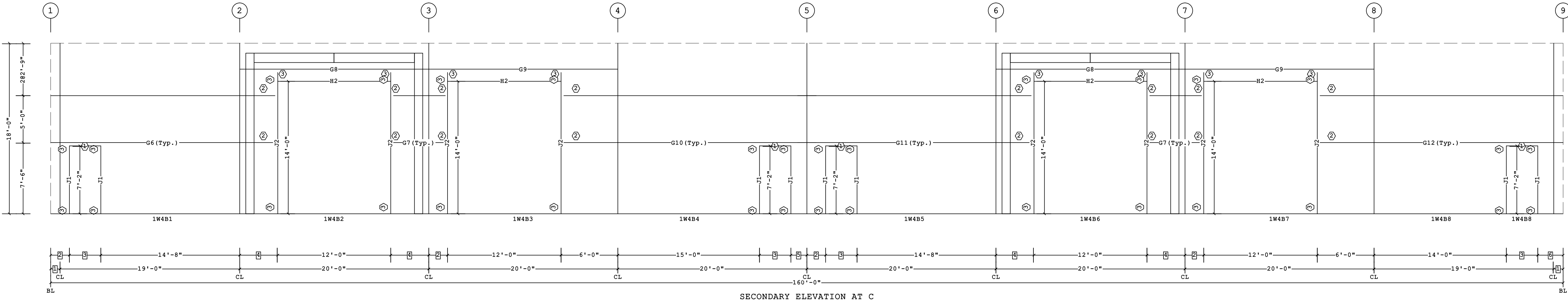
Date 5/7/2025

Drawn/Check

Page

Filename: Bath Propertie

Secondary Part Schedule				
Mark	Part	Thick.	Depth	Lap
G10	00508ZS260841700	0.0600	8 1/2"	10 1/2"
G11	00608ZS240841700	0.0600	8 1/2"	10 1/2"
G12	00708ZS250841700	0.0600	8 1/2"	
G6	00308ZS230841700	0.0600	8 1/2"	
G7	00408ZS050541700	0.0600	8 1/2"	
G8	08Z1911416DD00	0.0680	8 1/2"	
G9	08Z1911417DD00	0.0600	8 1/2"	
H1	DHC0304017	0.0000	8 1/2"	
H2	00208JS1200017	0.0600	8 1/2"	
J1	00108JS0702217	0.0600	8 1/2"	
J2	00308JS1411217	0.0600	8 1/2"	
Detail				
WSR065, WS01G3, WS20F2				
WS20F2, WSR065, WS01G3				
WS12A2, WS20F2				
WS12A2, WS20F2				
WSR065, WS01G2				
WSR065, WS01G2				
WS21D7				
WS20F9				
WS20B2, WS20B8				
WS20F9, WS20F2, WS20B2, WS20B8				



- 6

2'-8"
- 5

1'-8"
- 4

4'-0"
- 3

3'-4"
- 2

2'-0"
- 1

1'-0"
- 3

PG1
- 2

JTG1
- 1

H1
- Dimension Key

Part Mark Key

Shape Name = Bath Properties, Wall = 4

1. Unless noted, use 1/2 x 1 1/2 A325T Bolt (49080) and Nut (47120) w/o washers. Snug tighten bolts for all secondary connections.
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D				VP Buildings		SECONDARY ELEVATION AT C		
3200 Players Club Circle Memphis TN 38125								
Rev	Date	By	Description	Builder		Baxter Builders LLC		
				Customer				
				Location		La Pine, Oregon		
				Project		Bath Properties		
				Builders PO#				
NTS				Filename:		Bath Propertie		
5/13/2025				13:44:47				

VP BUILDINGS

VARCO PRUDEN

A BlueScope Steel Company

VPC Version: 25.1.1

a division of BlueScope Buildings North America, Inc.

Job #

Date

5/7/2025

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Page

ERECTION DRAWING PART MARK

C
P
R

1 W 1 B 3

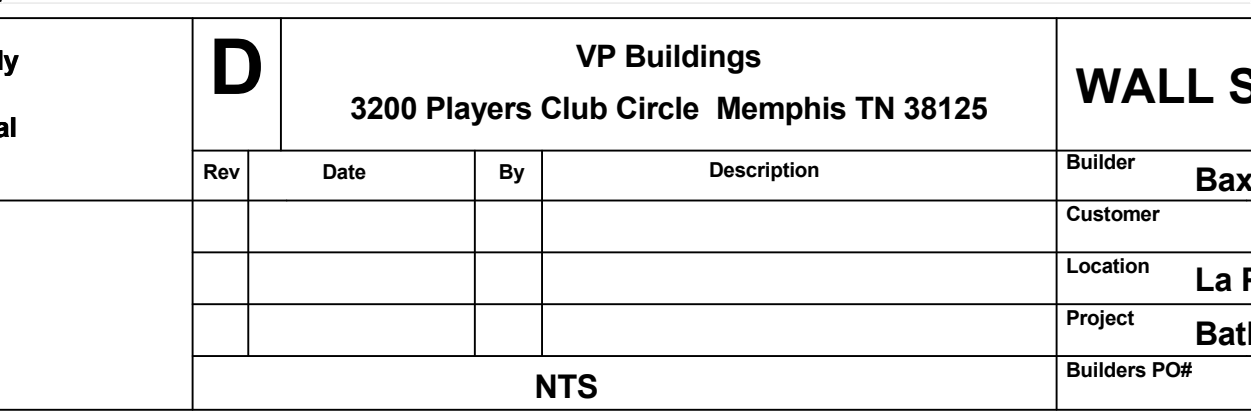
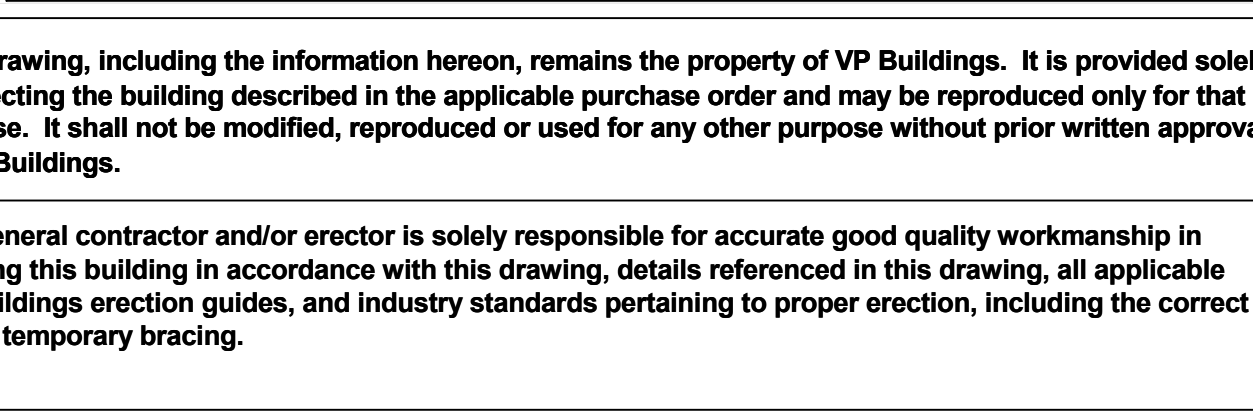
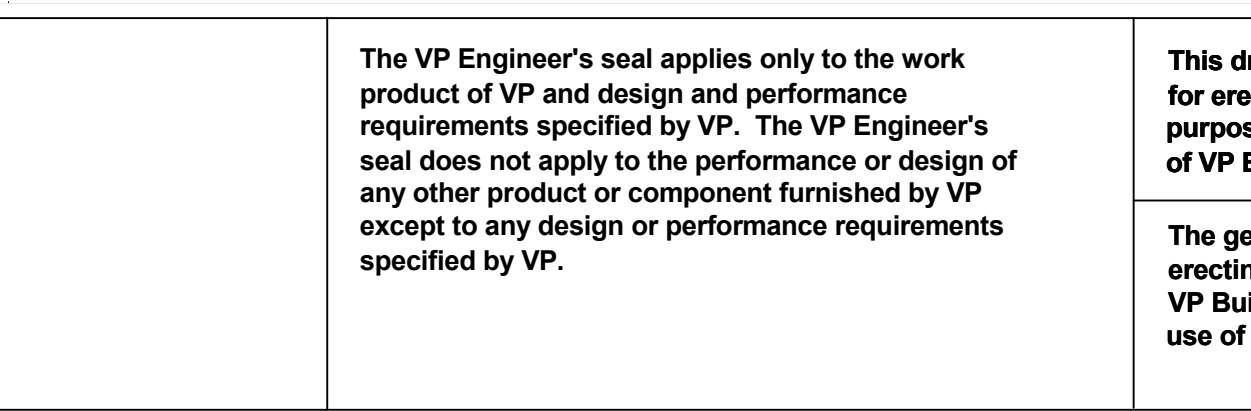
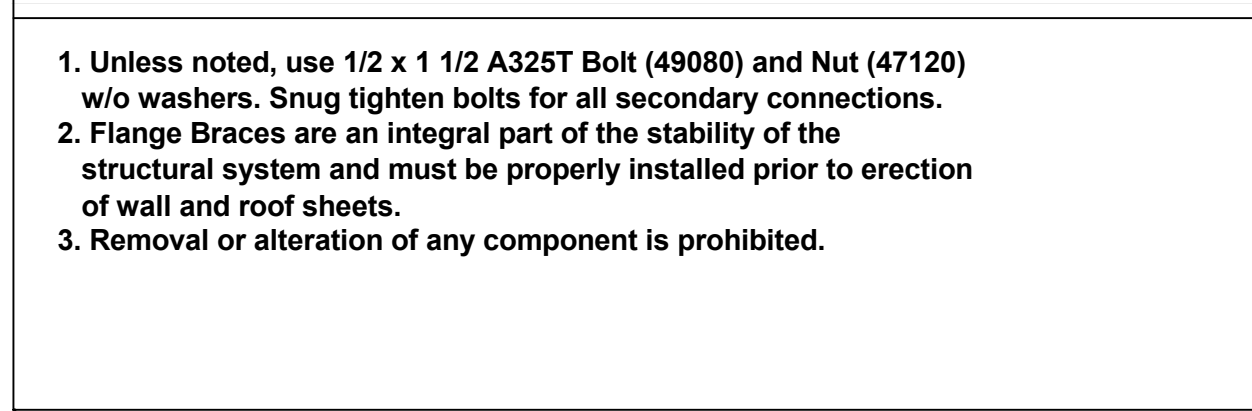
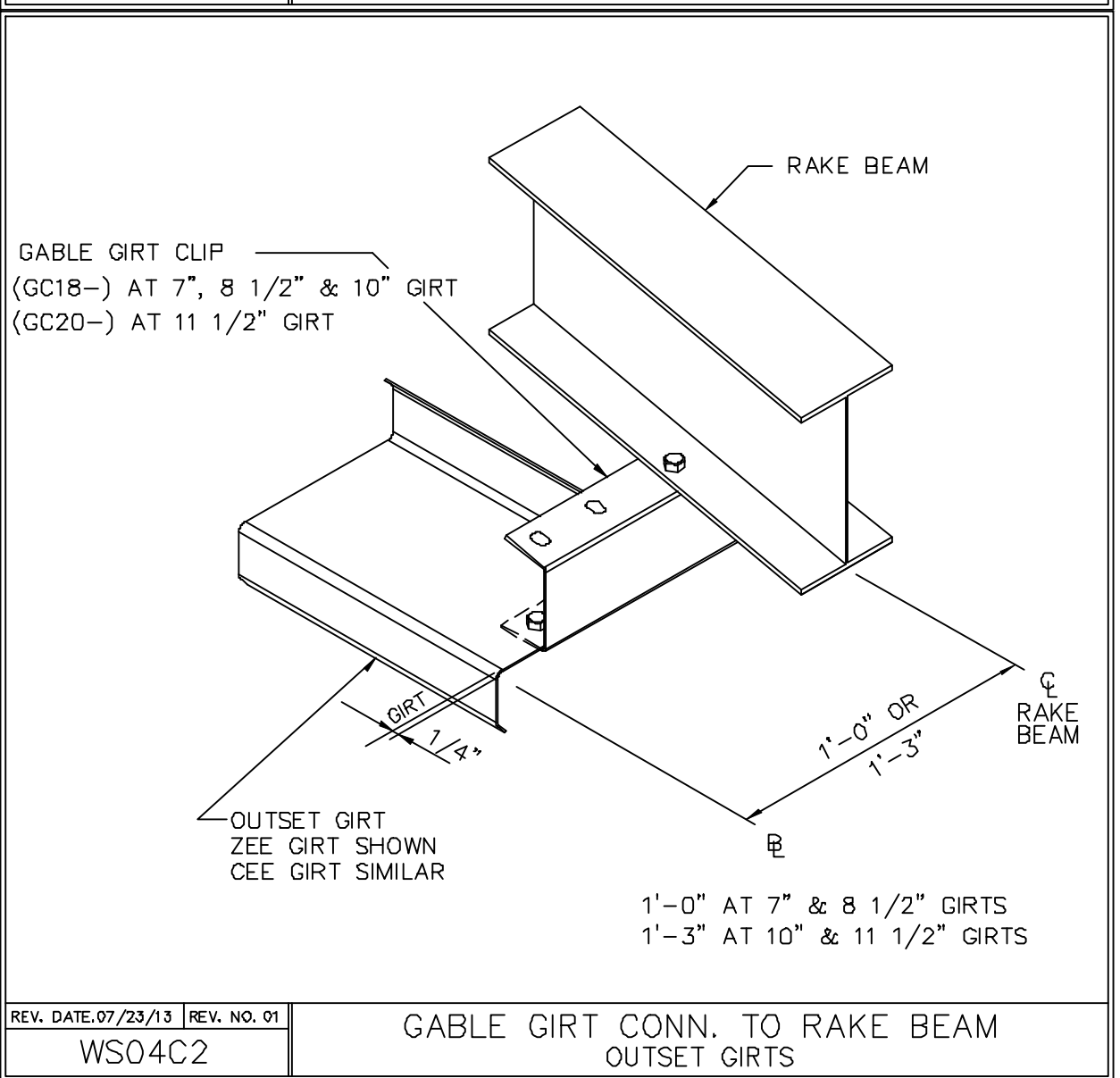
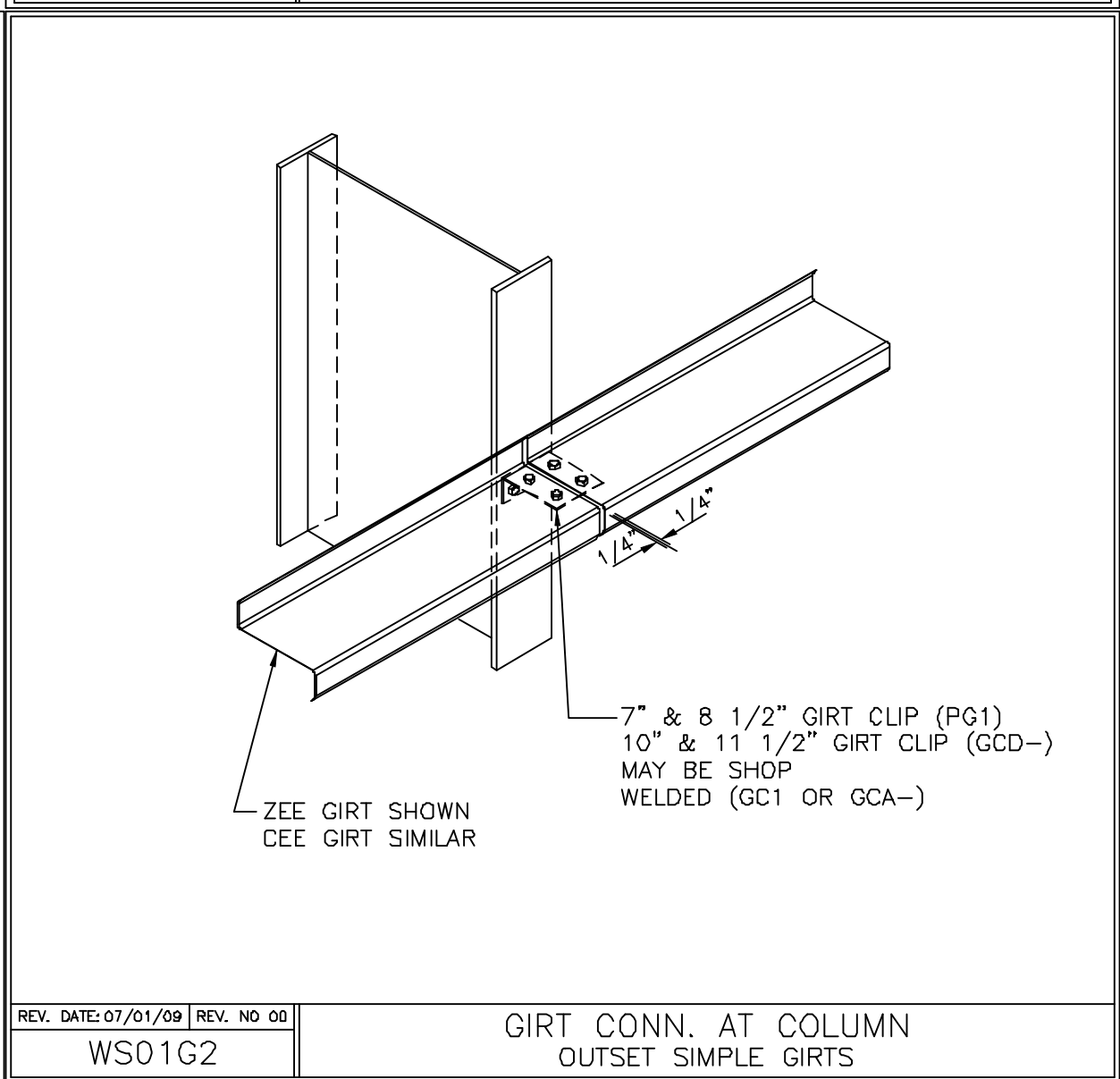
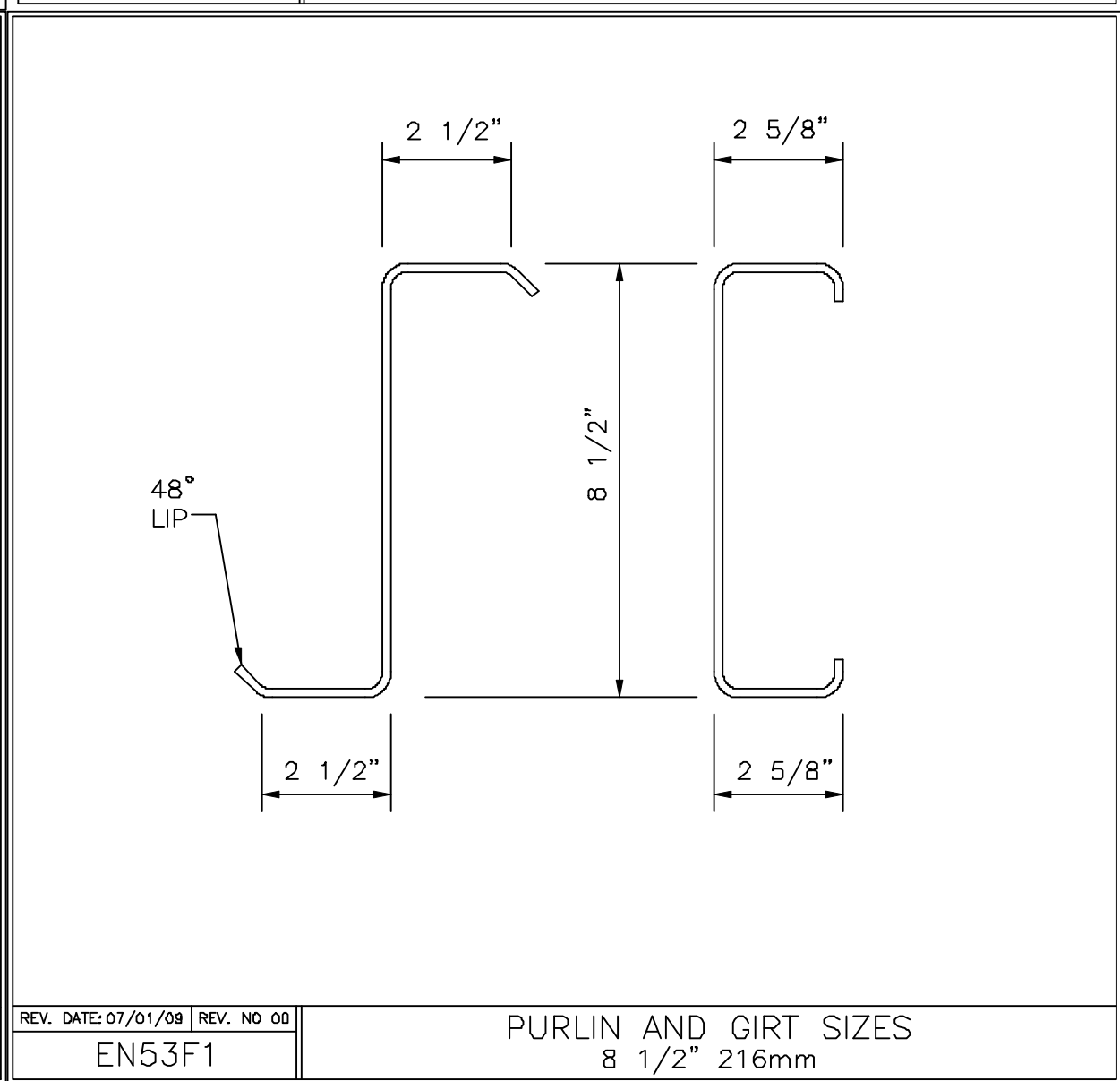
BAY NUMBER

CANOPY (C)/PARTITION(P)/

ROOF(R)/WALL(W) NUMBER

BUILDING SHAPE

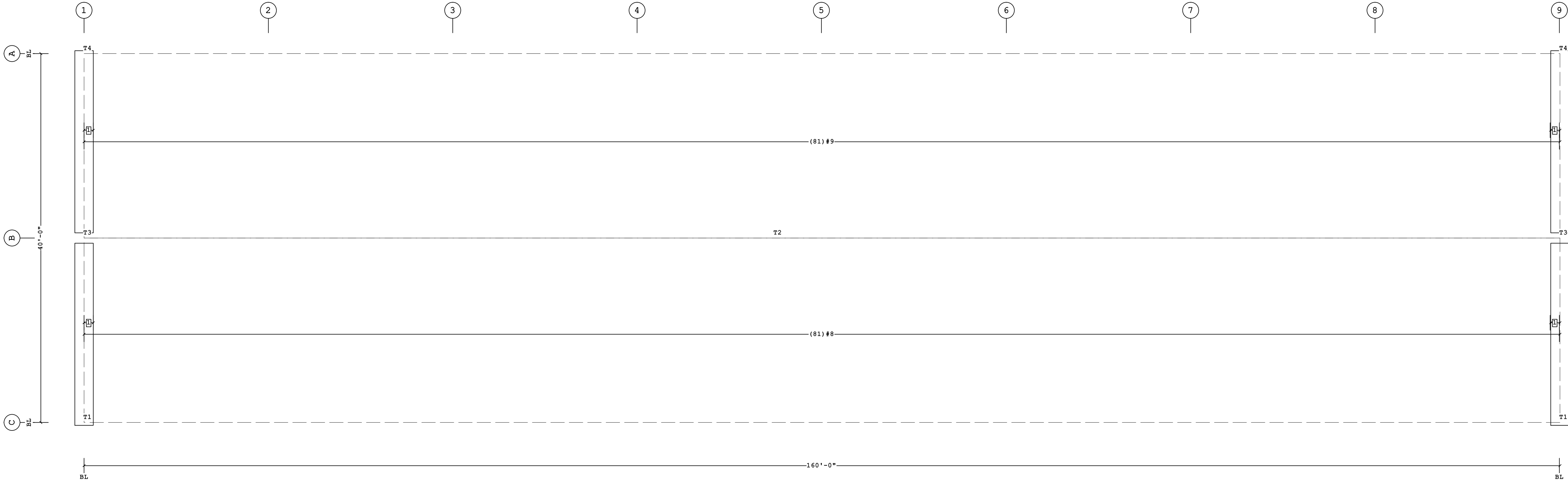
THE BAY/BUNDLE CODE IDENTIFIES THE BUILDING CANOPY/PARTITION/ ROOF/WALL PLANE AND BAY



Covering Schedule						
Id	Qty	Length	Type	Gage	OP	Fin. Color
#8	81	19'-11 3/4"	SSR	24	2	K TD
#9	81	19'-11 3/4"	SSR	24	2	K TD
Oper. Code:2=SQ,SQ						
Finish:K=KXL (Kynar)						
Color:TD=Standard Color						

Trim Schedule	
Id	Parts
T1	BS1,FPRF1,GGC2,MCC1
T2	80539,SRCF06,(16)SRCF10,SRCM06,(16)SRCM10
T3	SPC20,SRR2
T4	BS1,FPRF1,GGC2,MCC1

Color	Details
Cool Dark Bronze	RC38N1
Match Roof Color	RC34A7,RCV418,RCV419,RCV420,RCV421,RCV531
Cool Dark Bronze	RCV501,RCV502
Cool Dark Bronze	RC38N1



ROOF COVERING PLAN

Dimension Key
1'-0" Starter Panel (Cut Dim. = 1'-1")

- Pre-drilling 1/8 diameter holes for structural fasteners may be required for heavy gage nested zee's and/or fasteners to structural beams
- Steel panels are an integral part of the structural system. removal or alteration without prior authorization is prohibited.
- Due to manufacturing limitations short panels may require field cutting, see the covering schedule for cut lengths.
- See job details for covering and trim fastener specification.

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VP Buildings 3200 Players Club Circle Memphis TN 38125			
Rev	Date	By	Description
NTS			

ROOF COVERING PLAN

Builder	Baxter Builders LLC
Customer	
Location	La Pine, Oregon
Project	Bath Properties
Builders PO#	



VP BUILDINGS
VARCO PRUDEN

A BlueScope Steel Company
VPC Version: 25.1.1

Job #	
Date	5/7/2025
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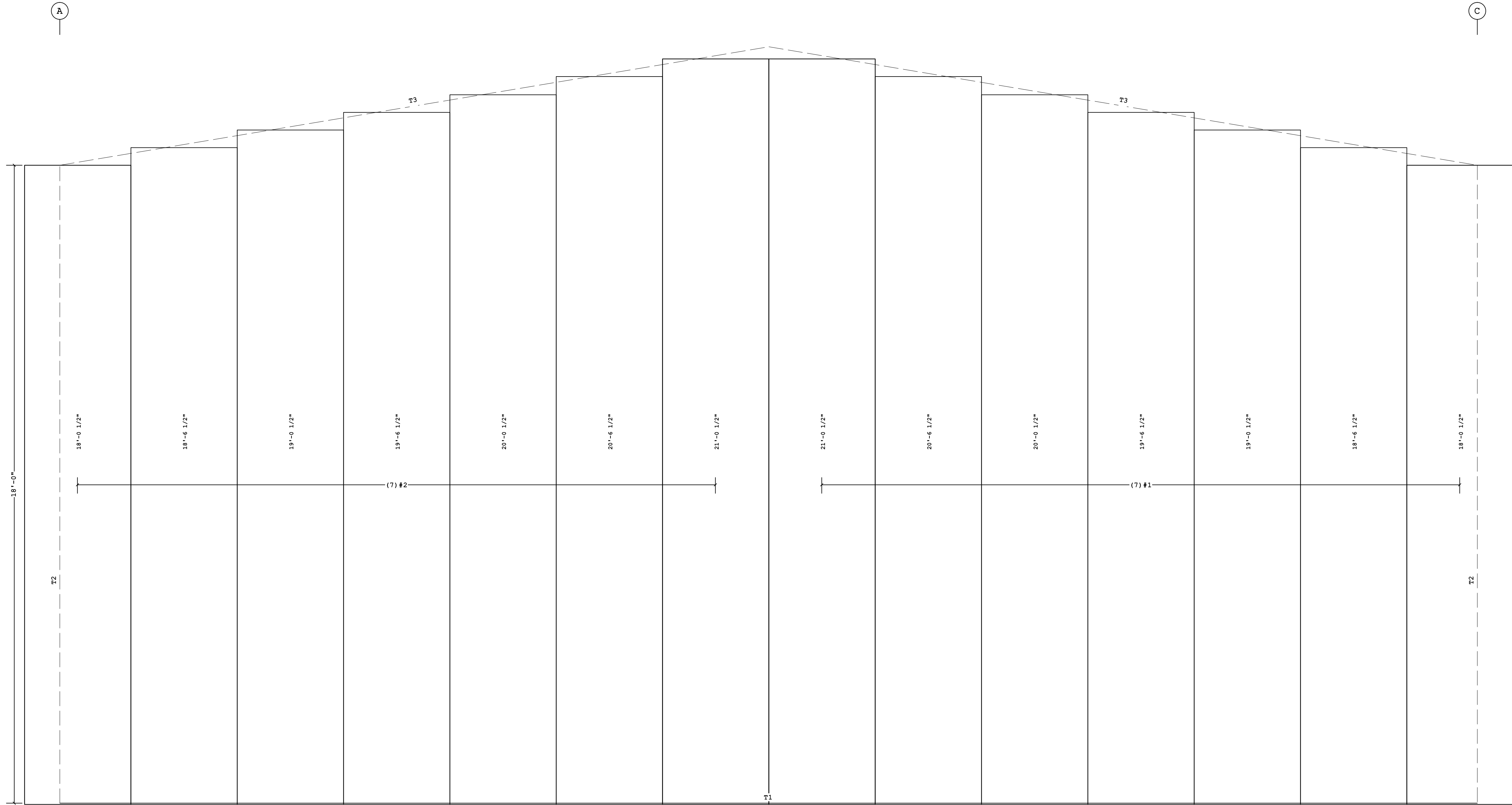
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5/13/2025

13:44:51

Filename: Bath Propertie

Covering Schedule										Trim Schedule			Color		Details	
Id	Qty	Type	Start	Length	Gage	OP	Fin.	Color	Increment	Direction	Id	Parts	Cool Dark Bronze		EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065	
#1	7	PR	21'-0	1 1/2"	26	1	K	TD	-6"	Left to Right	T1	(1.6)BA12S, (4)BT10	Match Wall Color		WC20A1	
#2	7	PR	21'-0	1 1/2"	26	1	K	TD	-6"	Right to Left	T2	CT20	Cool Dark Bronze		RC10A2, RC30A1, RS10L5	
Oper. Code:1=SQ,SQ										(3)RFR10-130,RKF16,RKF10, (3)RSB10, (3)RSC10						
Finish:K=KXL (Kynar)																
Color:TD=Standard Color																



Fastener Schedule	
Part	Description
0097597STD	(T-2) #17/#12-14 x 1 7/8" Stand-Off, 5/16" Hex Hd, SS Cap w/Washer
Roof Struct 1 1/2(T-2)	#12-14 x 1 1/2", 5/16" Hex Hd, SS Cap w/Washer
Roof Stitch 7/8 S(T-1)	1/4-14 x 7/8", 5/16" Hex Hd, SS Cap w/Washer

COVERING ELEVATION AT 1

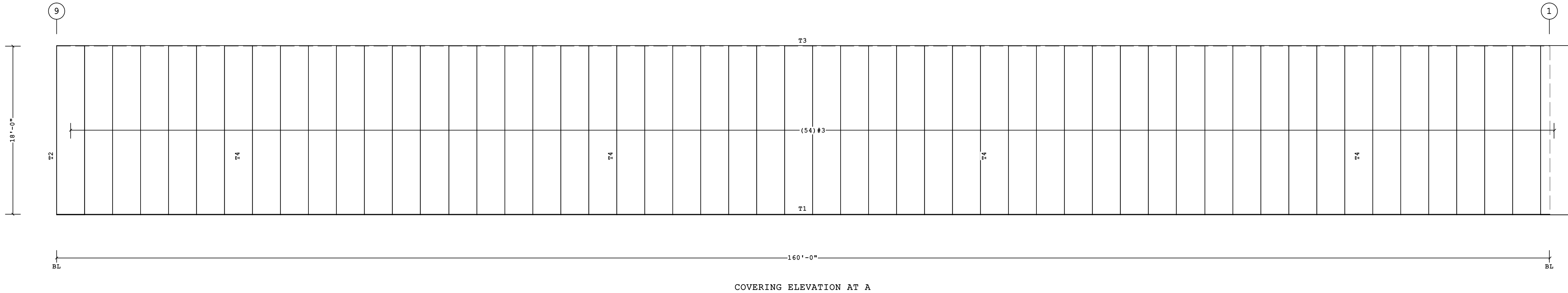
Shape Name = Bath Properties, Wall = 1

<div>1. Pre-drilling 1/8 diameter holes for structural fasteners may be required for heavy gage nested zee's and/or fasteners to structural beams</div> <div>2. Steel panels are an integral part of the structural system. removal or alteration without prior authorization is prohibited.</div> <div>3. Due to manufacturing limitations short panels may require field cutting, see the covering schedule for cut lengths.</div> <div>4. See job details for covering and trim fastener specification.</div>	<div>The VP Engineer's seal applies only to the work product of VP and design and performance requirements specified by VP. The VP Engineer's seal does not apply to the performance or design of any other product or component furnished by VP except to any design or performance requirements specified by VP.</div>	<div>This drawing, including the information hereon, remains the property of VP Buildings. It is provided solely for erecting the building described in the applicable purchase order and may be reproduced only for that purpose. It shall not be modified, reproduced or used for any other purpose without prior written approval of VP Buildings.</div> <div>The general contractor and/or erector is solely responsible for accurate good quality workmanship in erecting this building in accordance with this drawing, details referenced in this drawing, all applicable VP Buildings erection guides, and industry standards pertaining to proper erection, including the correct use of temporary bracing.</div>	<div>D</div> <div>VP Buildings</div> <div>3200 Players Club Circle Memphis TN 38125</div>	COVERING ELEVATION AT 1					
				Rev	Date	By	Description	Builder	Baxter Builders LLC
								Customer	
								Location	La Pine, Oregon
								Project	Bath Properties
								Builders PO#	
				NTS					
<div>5/13/2025</div> <div>13:44:53</div> <div>Filename: Bath Propertie</div> <div>a division of BlueScope Buildings North America, Inc.</div>									

Covering Schedule
Id Qty Type Start Length Gage OP Fin. Color Direction
#3 54 PR 18'-0 3/4" 26 1 K TD Left to Right
Oper. Code:1=SQ,SQ
Finish:K=KXL (Kynar)
Color:TD=Standard Color

Trim Schedule
Id Parts
T1 (6.4)BA12S,(16)BT10
T2 CT20
T3 (8)EG202,(16)GRA10,(16)PCA10A,(82)STR4
T4 5CE75,(2)CP510,DN1,(4)DST1

Color
Cool Dark Bronze
Match Wall Color
Cool Dark Bronze
Match Wall Color
Details
EN52A1,ENV003,RC00A1,WC01AB,WC04G1,WSR065
WC20A1
RC03A2,RC32A1,RC38E2,RC61A6,RCV324,RCV536,
WC04G1,WC11F1
RC38F1



Fastener Schedule
Part Description
0097597STD (T-2) #17/#12-14 x 1 7/8" Stand-Off, 5/16" Hex Hd, SS Cap w/Washer
Roof Struct 1 1/2(T-2) #12-14 x 1 1/2", 5/16" Hex Hd, SS Cap w/Washer
Roof Stitch 7/8 S(T-1) 1/4-14 x 7/8", 5/16" Hex Hd, SS Cap w/Washer

Shape Name = Bath Properties, Wall = 2

1. Pre-drilling 1/8 diameter holes for structural fasteners may be required for heavy gage nested zee's and/or fasteners to structural beams
2. Steel panels are an integral part of the structural system. removal or alteration without prior authorization is prohibited.
3. Due to manufacturing limitations short panels may require field cutting, see the covering schedule for cut lengths.
4. See job details for covering and trim fastener specification.

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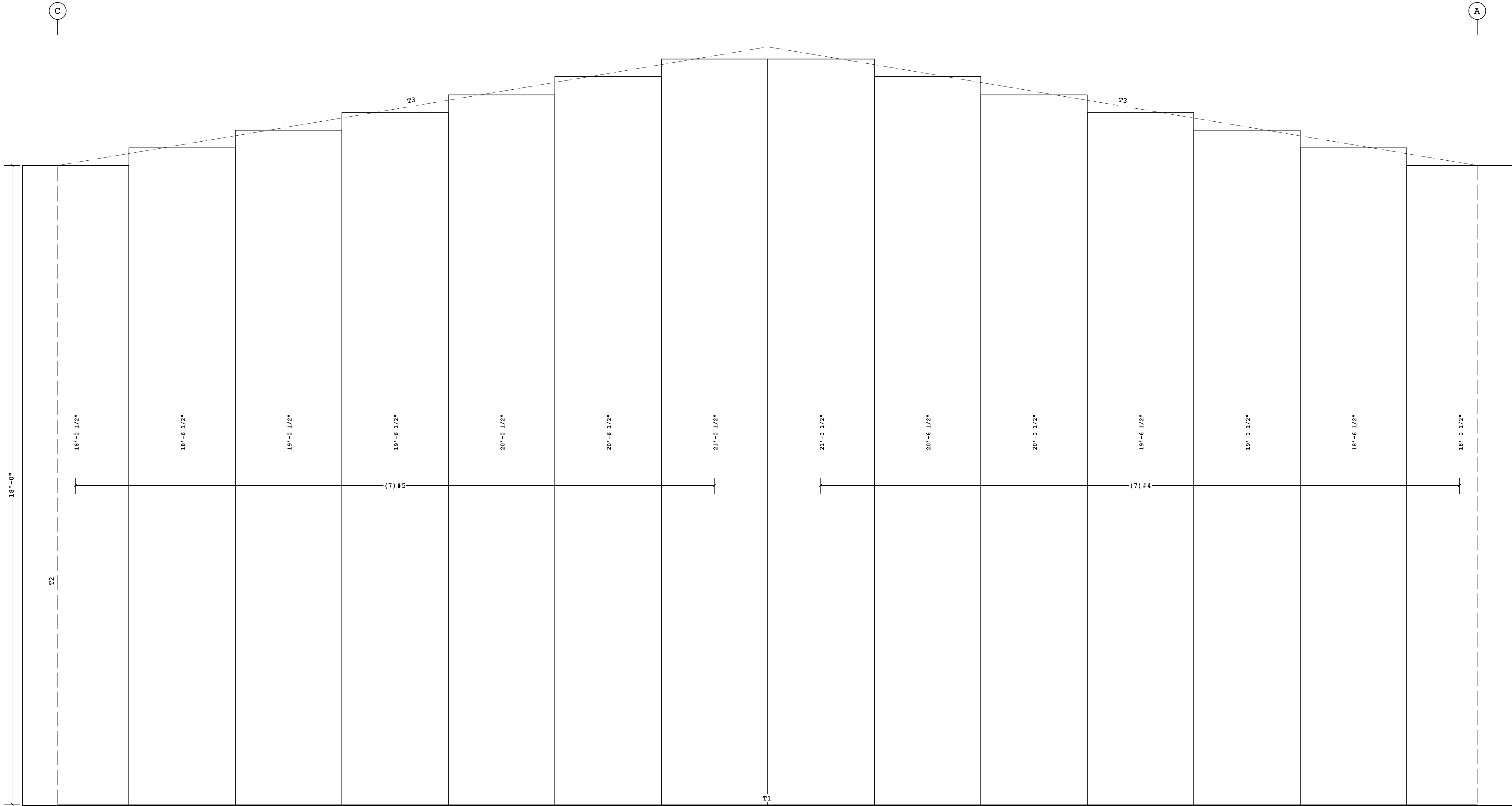
D	VP Buildings 3200 Players Club Circle Memphis TN 38125			COVERING ELEVATION AT A		
	Rev	Date	By	Description	Builder	Baxter Builders LLC
					Customer	
					Location	La Pine, Oregon
					Project	Bath Properties
NTS				Builders PO#		
5/13/2025				13:44:54		
File name:				Bath Propertie		

A BlueScope Steel Company
VPC Version: 25.1.1

a division of BlueScope Buildings North America, Inc.

Job #	
Date	5/7/2025
Drawn/Check	
Page	

Covering Schedule										Trim Schedule									
Id	Qty	Type	Start	Length	Gage	OP	Fin.	Color	Increment	Direction	Id	Parts	Color	Details					
#4	7	PR	21'-0	1/2"	26	1	K	TD	-6"	Left to Right	T1	(1.6)BA12S, (4)BT10	Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065					
#5	7	PR	21'-0	1/2"	26	1	K	TD	-6"	Right to Left	T2	CT20	Match Wall Color	WC20A1					
Oper. Code:1=SQ,SQ										(3)RFR10-130, RKF16, RKF10, (3)RSB10, (3)RSC10									
Finish:K=KXL (Kynar)																			
Color:TD=Standard Color																			



Fastener Schedule	
Part	Description
0097597STD	(T-2) #17/#12-14 x 1 7/8" Stand-Off, 5/16" Hex Hd, SS Cap w/Washer
Roof Struct 1 1/2(T-2)	#12-14 x 1 1/2", 5/16" Hex Hd, SS Cap w/Washer
Roof Stitch 7/8 S(T-1)	1/4-14 x 7/8", 5/16" Hex Hd, SS Cap w/Washer

COVERING ELEVATION AT 9

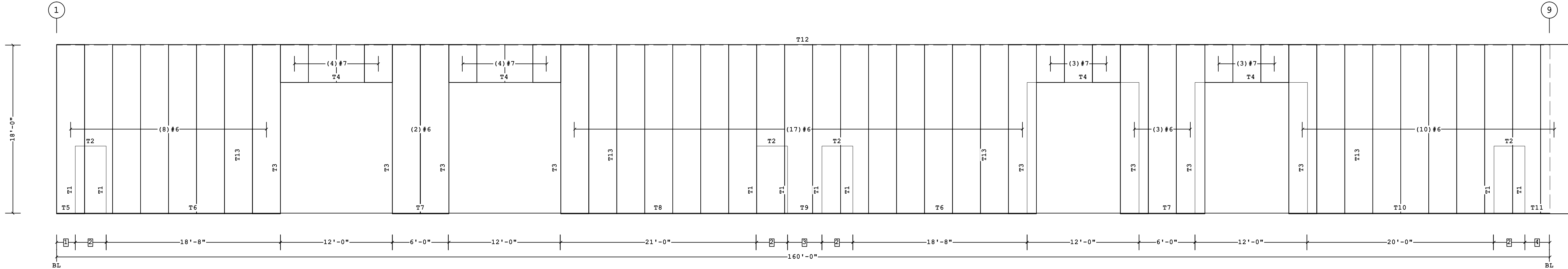
Shape Name = Bath Properties, Wall = 3

<div>1. Pre-drilling 1/8 diameter holes for structural fasteners may be required for heavy gage nested zee's and/or fasteners to structural beams</div> <div>2. Steel panels are an integral part of the structural system. removal or alteration without prior authorization is prohibited.</div> <div>3. Due to manufacturing limitations short panels may require field cutting, see the covering schedule for cut lengths.</div> <div>4. See job details for covering and trim fastener specification.</div>	<div>The VP Engineer's seal applies only to the work product of VP and design and performance requirements specified by VP. The VP Engineer's seal does not apply to the performance or design of any other product or component furnished by VP except to any design or performance requirements specified by VP.</div>	<div>This drawing, including the information hereon, remains the property of VP Buildings. It is provided solely for erecting the building described in the applicable purchase order and may be reproduced only for that purpose. It shall not be modified, reproduced or used for any other purpose without prior written approval of VP Buildings.</div> <div>The general contractor and/or erector is solely responsible for accurate good quality workmanship in erecting this building in accordance with this drawing, details referenced in this drawing, all applicable VP Buildings erection guides, and industry standards pertaining to proper erection, including the correct use of temporary bracing.</div>	D	VP Buildings			COVERING ELEVATION AT 9			
				3200 Players Club Circle Memphis TN 38125			Builder Baxter Builders LLC			
				Rev	Date	By	Description	Customer		
								Location La Pine, Oregon		
								Project Bath Properties		
								Builders PO#		
				NTS			A BlueScope Steel Company			
				5/13/2025			VPC Version: 25.1.1			
13:44:55			Filename: Bath Propertie							
			a division of BlueScope Buildings North America, Inc.							

Covering Schedule								
Id	Qty	Type	Start Length	Gage	OP	Fin.	Color	Direction
#6	40	PR	18'-0 3/4"	26	1	K	TD	Left to Right
#7	14	PR	4'-0 1/4"	26	1	K	TD	Left to Right
Oper. Code:l=SQ,SQ								
Finish:K=KXL (Kynar)								
Color:TD=Standard Color								

Trim Schedule	
Id	Parts
T1	DFT10, JT07
T2	DFT05, HTS05
T3	DFT14, JT14
T4	DFT12, HTS12
T5	(0.1) BA125, BT10
T6	(0.7) BA125, (2) BT10
T7	(0.2) BA125, BT10
T8	(0.8) BA125, (3) BT10
T9	(0.1) BA125, BT10
T10	(0.8) BA125, (2) BT10
T11	(0.1) BA125, BT10
T12	(8) EG202, (16) GRA10, (16) PCA10A, (82) STR4
T13	5CE75, (2) CP510, DN1, (4) DST1

Color	Details
Cool Dark Bronze	WC24A1
Cool Dark Bronze	WC24A2
Cool Dark Bronze	WC24A1
Cool Dark Bronze	WC24A2
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	EN52A1, ENV003, RC00A1, WC01AB, WC04G1, WSR065
Cool Dark Bronze	RC03A2, RC32A1, RC38E2, RC61A6, RCV324, RCV536, WC04G1, WC11F1
Match Wall Color	RC38P1



4	2'-8"
3	3'-8"
2	3'-4"
1	2'-0"
<input type="checkbox"/> Dimension Key	

Fastener Schedule	
Part	Description
0097597STD	(T-2) #17/#12-14 x 1 7/8" Stand-Off, 5/16" Hex Hd, SS Cap w/Washer
Roof Struct 1 1/2(T-2)	#12-14 x 1 1/2", 5/16" Hex Hd, SS Cap w/Washer
0097585-105	(T-2) #12-14 x 1 1/2", 5/16" Hex Hd, SS Cap w/Washer
Roof Stitch 7/8 S(T-1)	1/4-14 x 7/8", 5/16" Hex Hd, SS Cap w/Washer

Shape Name = Bath Properties, Wall = 4

1. Pre-drilling 1/8 diameter holes for structural fasteners may be required for heavy gage nested zee's and/or fasteners to structural beams
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4. See job details for covering and trim fastener specification.

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D	VP Buildings			COVERING ELEVATION AT C			
	3200 Players Club Circle Memphis TN 38125						
Rev	Date	By	Description	Builder	<div><div><div><div></div><div>VP BUILDINGS</div><div>VARCO PRUDEN</div></div><div>A BlueScope Steel Company</div><div>VP Version: 25.1.1</div></div><div>a division of BlueScope Buildings North America, Inc.</div></div>		Job #
				Customer			Date
				Location			5/7/2025
				Project			Drawn/Check
				Builders PO#			Page
NTS				Filename:	Bath Propertie		
5/13/2025		13:44:56					

REV. DATE: 01/29/21	REV. NO. 04	SSR SEAMING GUIDELINES RIB AND SEAM PROFILE
EN01B2		

REV. DATE:11/17/17	REV. NO. 01	PR ROOF & PR/RPR WALL PANELS
EN52A1		

REV. DATE: 07/01/09	REV. NO 00	STANDING SEAM ROOF PANELS (SSR)
EN52D1		

REV. DATE:01/30/14	REV. NO. 00	ROOF INSTALLATION GUIDE – SSR
ENV001		

REV DATE:01/30/14	REV. NO. 00	BASIC PANEL AND ACCESSORIES
ENV003		CONTAINS: PR ROOF - WALLS: PR, RPR, VR

REV. DATE: 11/18/20	REV. NO. 00	SSR PANEL CLIP (STANDARD CLIP) PROFILE AND MARK NUMBERS
ENV012		

REV. DATE: 07/01/09	REV. NO 00	NOTES
RC00A1		

REV. DATE: 09/19/16	REV. NO. 02	SSR PANEL SIDELAP ADDITIONAL WEATHERSEAL AT ICE DAMMING COND.
RC03A2		

REV. DATE: 06/26/19	REV. NO. 06	SSR STARTING RAKE PR, VR, OR RPR WALL W/ THERMAL BLOCK
RC10A2		

REV. DATE: 06/26/19	REV. NO. 07	SSR RAKE TRIM PR, VR OR RPR WALL
RC30A1		

REV. DATE: 06/26/19	REV. NO. 05	SSR ROOF W/ EAVE GUTTER PR, VR OR RPR WALL
RC32A1		

REV. DATE: 06/26/19	REV. NO. 01	SSR RIDGE
RC34A7		EXPANSION JOINT (@ 160'-0" O.C. MAX.)

REV. DATE: 03/23/21	REV. NO. 06	GUTTER STRAP ATTACHMENT SSR ROOF (ICE DAMMING)
RC38E2		

REV. DATE: 06/26/19	REV. NO. 04	EAVE GUTTER CORNER ASSEMBLY
RC38N1		ALL SYSTEMS

a division of BlueScope Buildings North America, Inc.

5/13/2025 SEDSheet

13:45:01

Planting schedule

Project: 16630 Box Way

Lot: 14

Date: August 27, 2025

Subject: Planting Schedule

The following planting schedule is recommended for the 16630 Box Way landscape areas:

Street Frontage

- Minimum 1.5" caliper *Acer rubrum* 'Franksred' (Frank's Red Maple) 3 trees total to be spaced at 30' on center.
 - Trees should be staked with two 2" lodgepole pine stakes, placed outside the rootball.
- 1-gallon *Festuca idahoensis* (Idaho Fescue), triangularly spaced 42" on center.

Landscape Swales

- 1.5" caliper *Prunus virginiana* 'Canada Red' (Canada Red Chokecherry)
- 1-gallon *Festuca idahoensis* (Idaho Fescue), triangularly spaced 42" on center.

Planting Instructions

For all plant pits, dig holes 2x the diameter of the rootball and scarify the sides of the pit.

- Install plants in the center of the hole.

- Backfill holes with a 50/50 mix of compost and native soil.
- Berm edges of plant pits to help with initial water retention.
- Soak plants thoroughly at initial planting.
- Use time-release fertilizer and mycorrhizal inoculum, both of which are highly recommended for plant establishment and long-term health.

Commercial Building Application

247-____-_____

Office Use

Project Information

Project Description:			
Business Commercial Name (If applicable):			
Property Address:			
#		Street	
		City	
Details of Project		Are any of the following being constructed or remodeled?	
Project Cost/Bid:	<input type="checkbox"/> Deferred/Phased Submittal	<input type="checkbox"/> Restaurant	<input type="checkbox"/> Pool(s)
Structure Sq. footage:	New Structure Height:	<input type="checkbox"/> Spa(s)	<input type="checkbox"/> None of these

Mechanical Information

Bid/Project Value for All Mechanical Work: \$: _____			
Heat Source			
<input type="checkbox"/> Gas	<input type="checkbox"/> Electric	<input type="checkbox"/> Propane (if heat source is propane, fill in the Propane Contact information located on the next page)	
<input type="checkbox"/> Gas and Electric	<input type="checkbox"/> Other(Specify):		
Enter the Quantity of all that apply.			
Exhaust Fans		Cadet Heater, Baseboard Electric Wall Heater	
Radiant Floor Heating	Open Loop	Boiler	Over 200,000 BTU
	Closed Loop		Under 200,000 BTU
Heat Pump	Over 100,000 BTU	Furnace/ Forced Air Unit	Over 100,000 BTU
	Under 100,000 BTU		Under 100,000 BTU

Electrical Information

Is Temporary Power (TP) needed?		
<input type="checkbox"/> Now	<input type="checkbox"/> No	<input type="checkbox"/> At issuance
If at issuance or now, fill in Temporary power contractor information on contact page located on the next page.		
No. of Services & Subpanels:	No. of Circuits/ services:	No. of Amps/ services:
Limited Energy Low Voltage		
Mark all that apply:		
Boiler Controls		Intercom / Paging
Clock System		Nurse Calls
Medical		Landscape Irrigation
Data Tele-Communication		Outdoor Landscape Lighting
HVAC		Protective Signaling
Instrumentations		Other (specify):

Plumbing Information

Installing/Altering/Relocating Water line?	Enter # of Feet
Installing/Altering/Relocating Septic or Sewer line?	Enter # of Feet
Enter quantity of all that apply	
Bath Sinks	Water Heater
Water	Washing Machines
Tubs / Showers	Utility Sink
Kitchen Sinks	Hose Bibs
Dish Washers	Backflow Devices
Disposals	Other Specify



Contact Information

Building Owner Information

Name:

Email:

Phone/Cell:

Mailing Address:

#

Street

City

State

Zip code

Applicant Information (if applicant is same as building owner, skip this section)

Name:

Email:

Phone/Cell:

Tenant Information

Name:

Email:

Phone/Cell:

Contractor Information:

Name:

CCB #:

Phone/Cell:

Email:

Plumbing Contractor

Name :

License #:

Electrical Contractor

Name :

License #:

Mechanical Contractor

Name :

License #:

Propane Contractor (if heat source is propane)

Name:

License #:

Temporary Power Contractor (if temporary power needed)

Name:

License #:

Design Engineer

Name:

Email:

Phone/Cell:

Authorized Representative and Acknowledgement Form

This authorization expires 12 months from date of owner signature.

Project Description:	Property Address:
	# Street City
Applicant's Name <i>(If different from owner)</i>	Applicant's Phone Number
Applicant Mailing Address	
Primary Contact (PC) Name:	PC Phone #
PC Mailing Address:	
PC Email:	

Authorized Representative

I, _____ (building owner), have authorized _____ (authorized representative), to act as my agent in performing the activities necessary to obtain services provided by Deschutes County Community Development Department. I agree that any costs not satisfied by the Authorized Representative are my responsibility.

Owner SignatureDate

Authorized Representative SignatureDate

Acknowledgements

Zoning Setback

The inspector's review of zoning setbacks is based on information provided by the applicant. Only a State of Oregon Licensed land surveyor can certify the correct location of this building on this lot. Deschutes County Community Development Department strongly recommends that the applicant retain a licensed land surveyor to verify the zoning setbacks applicable to this lot. Accuracy of building setbacks is the sole responsibility of the applicant and the applicant accepts all risks associated with any inaccurate information contained in the County's setback review.

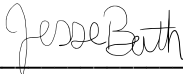
By signing, I acknowledge that all information contained in this form is true to the best of my knowledge.

Authorized Representative

Owner

Representative's Signature

Date

Jesse Bath

Owner's Signature

Date

OR

Print Name

Print Name



LA PINE

O R E G O N

CITY OF LA PINE

16345 Sixth Street — PO Box 2460

La Pine, Oregon 97739

TEL (541) 536-1432 — FAX (541) 536-1462

www.lapineoregon.gov

HYDRANT NUMBER: 180

MAKE / SIZE: Waterous

LEAD SIZE: 4 1/2

LOCATION AUX. VALVE: _____

HYDRANT LOCATION: 16630 Box Way

NAME OF BUSINESS WORK BEING DONE FOR: _____

Date	Static Pressure	Residual Pressure	Pitot	Flow Rate	Flow @ 20 PSI
7-28-25	59 psi	54 psi	7	1407 gpm	4,249 gpm

[Signature]
Signature

Branden B
Printed Name

7-28-25
Date

7-28-25
Date

$$\frac{59}{59} - \frac{20}{54} = \frac{39}{5}$$

$$\frac{7.23}{2.39} = 3.02$$

$$1407 \times 3.02 = 4,249 \text{ gpm}$$



ENGINEERING, INC.

Traffic Generation Letter

August 25, 2025

Subject: Lot 14, Newberry Business Park – 16330 Box Way.
City, State: La Pine, Oregon

L.B. Engineering, Inc. is submitting for a 6,400 s.f. footprint building, that will be used for General Light Industrial uses.

L.B. Engineering, Inc. has completed the traffic generation letter for the proposed building with the findings presented below.

SITE INFORMATION

The building has a footprint of 6,400 s.f. that will be used for General Light Industrial uses

The site is located at 16330 Box Way, La Pine, OR and is Tax Lot 221014AB00132

The site zoning is I - Industrial

The site will be accessed off of Box Way

TRIP GENERATION

We will be using ITE Trip Generation Manual, 11th Edition, 140 Manufacturing

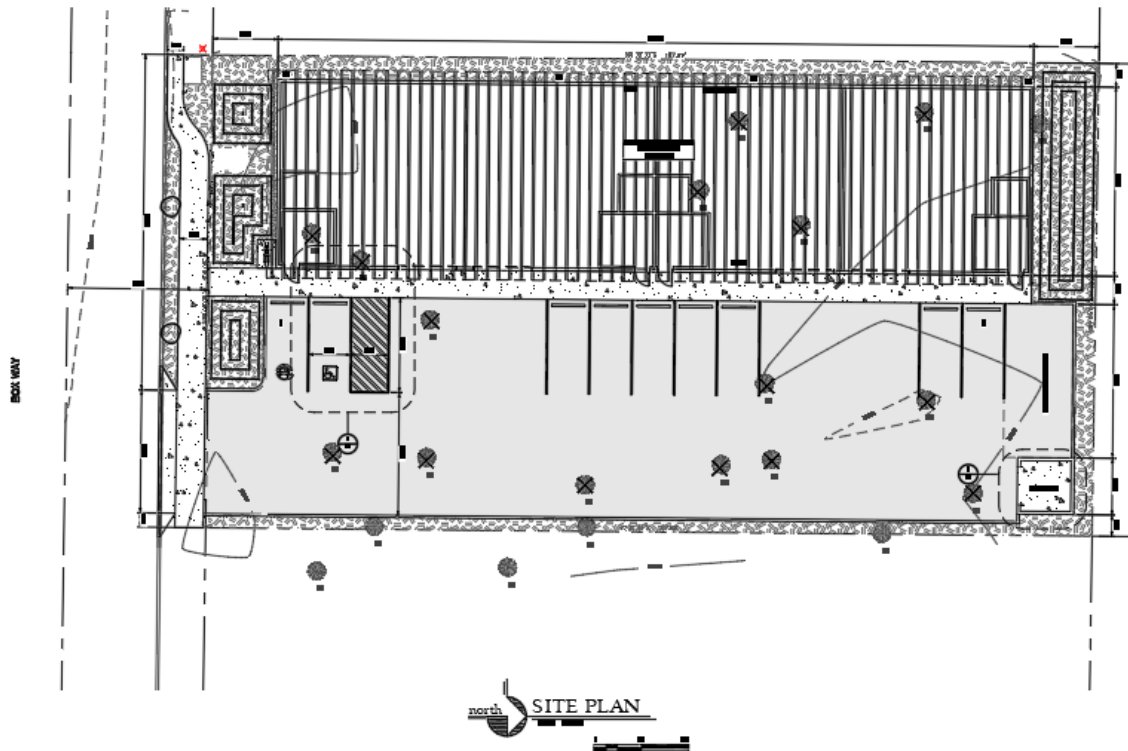
PROPOSED USE

ADT TABLE 1- TRIP GENERATION FOR PROPOSED BUILDING ITE CODE 110 – General Light Industrial per 1000 s.f. (6,400 s.f.)						
ADT TIME STUDY	TRIP RATE	KFS	IN/OUT (PERCENT)	IN	OUT	TOTAL
WEEKDAY A.M. PEAK HOUR	0.74	6.4	88/12	4	1	5
WEEKDAY P.M. PEAK HOUR	0.65	6.4	14/86	1	3	4
TOTAL PEAK TRIPS GENERATED						9
DAILY TRIPS GENERATED	4.87	6.4	50/50	16	15	31

AERIAL



SITE PLAN



REQUIREMENTS FOR A TRAFFIC IMPACT ANALYSIS:

1. A change in zoning or a plan amendment designation;

RESPONSE: *This Land use submittal is for site plan approval with no change in zoning or plan amendment designation.*

2. Operational or safety concerns documented in writing by a road authority;

RESPONSE: *Box Way is low use street, we do not anticipate and operational or safety concerns.*

3. An increase in site traffic volume generation by [300] average daily trips (ADT) or more;

RESPONSE: *This site proposal does not increase ADT by 300 daily trips or more.*

4. An increase in peak hour volume of a particular movement to and from a street or highway by [20] percent or more;

RESPONSE: *The highest classification street is Reed Rd to the North which is a collector street. With only 4 additional PM peak hour trips this will not increase any particular movement by 20 percent or more.*

5. An increase in the use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by ten vehicles or more per day;

RESPONSE: *The tenants are unknown at this time but with the relatively small size of the proposed building it is not anticipated that there would be an increase of vehicles exceeding 20,000 pounds by 10 or more.*

6. Existing or proposed approaches or access connections that do not meet minimum spacing or sight distance requirements or are located where vehicles entering or leaving the property are restricted, or such vehicles are likely to queue or hesitate at an approach or access connection, creating a safety hazard;

RESPONSE: *The proposed approaches meet minimum spacing and sight distance requirements for a local street; there are no anticipated safety concerns.*

7. A change in internal traffic patterns that may cause safety concerns; or

RESPONSE: *The site is vacant so all internal traffic patterns are proposed, there are no anticipated safety concerns.*

8. A TIA required by ODOT pursuant to OAR 734-051.

RESPONSE: *The site does not front an ODOT road classification*

CONCLUSION:

Based on the above responses a Traffic Impact Analysis should not be required to meet the City of La Pine development code.

Respectfully Submitted,

Lennie Brant, SE
President
L. B. Engineering, Inc.