

Community Development Department PO Box 2460 16345 Sixth Street La Pine, Oregon 97739

Phone: (541) 536-1432, Fax: (541) 536-1462

Email: info@ci.la-pine.or.us

Site Plan Application

File Number # SPR4-25-0001

Fee: Less than 1,000 sq ft
Fee: 1,001 to 5,000 sq ft
Fee: 5,001 to 10,000 sq ft
Fee: More than 10,000 sq ft
Fee: \$ 1,100.00
Fee: \$ 2,000.00
Fee: \$ 3,500.00
Fee: \$ 4,000.00

| PROPERTY OWNER AND | APPLICANT INFORMAT | TION |
|---|--|--------------------------|
| Central Oregon Intergovernmental Co Applicant Name CO Francis Senger - Mission Building | ^{puncil} Phone <u>(</u> 541) 870-7014 | ¹ Fax |
| Address 479 NE Alden Avenue | | |
| Email Francis@missionbuilding.com | | |
| Property Owner City of La Pine | Phone | Fax |
| Address PO Box 2460 | _ CityLa Pine State | OR Zip Code <u>97739</u> |
| Email | | |
| PROPERTY | DESCRIPTION | |
| Property Location (address, intersection of cro 16628 Assembly Way | oss street, general area) | |
| Tax lot number: T-15 R-13 Section | Tax Lot(s) | 221014AB00108 |
| Zoning I Total Land Area Present Land Use Vacant | | |
| Describe Project (i.e. type of use, hours of ope | eration, other project chara | acteristics): |

Type II Site Plan review for a single story, 6,087 square foot industrial building, associated with the La Pine Business Small Business Incubator Program. Consistent with the RFP for the project, the design includes 4-suites with individual restrooms, adjacent parking lot, landscaping, drainage swales, street trees, decorative lighting, a 6' sidewalk, sidewalk aesthetics, and driveway from Assembly Way.

PROJECT DESCRIPTION

Please give a brief description of the project:

Type II Site Plan review for a single story, 6,087 square foot industrial building, associated with the La Pine Business Small Business Incubator Program. Consistent with the RFP for the project, the design includes 4-suites with individual restrooms, adjacent parking lot, landscaping, drainage swales, street trees, decorative lighting, a 6' sidewalk, sidewalk aesthetics, and driveway from Assembly Way.



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PROFESSIONAL SERVICES Collaborative Design 406-248-3443 Architect/Designer/Engineer _ Architects Phone Billings 2280 GRANT ROAD, SUITE C City Address State MT Zip Code Email _____ travis@cd-mt.com FOR OFFICE USE ONLY Approval Process Engineering Date Received: Actual Planning Construction Building Rec'd By:

CHECKLIST

REQUIRED ITEMS TO BE SUBMITTED FOR SITE AND DESIGN REVIEW.

Note: additional information may be required depending on the actual project.

| may be required acpellating of the actual project. |
|---|
| Complete Application. The application must be signed by the property owner and the applicant. |
| Burden of proof statement, three (3) copies addressing approval criteria. |
| Title Report or Subdivision Guarantee verifying ownership, including legal description of land. |
| Fee Schedule (please see attached). |
| Site and Landscape plan; Building Elevations; seven (7) full sized copies of each which must be folded individually, or in sets to 8 $\frac{1}{2}$ " X 11" in size and 1 reduced (8 $\frac{1}{2}$ by 11 or 11 by 17) copy. |
| Floor plans, three copies for each building which must be folded individually, or in sets to 8 ½" X 11" in size, plus 1 reduced (8 ½ by 11 or 11 by 17). Include the class of construction. |
| Vicinity map. |
| Trip Generation statement prepared by a professional transportation planner or equivalent. 5 copies, Note: if more than 200 ADT result (or at the discretion of the City Engineer), a Traffic Impact Study may be required. |
| Preliminary Grading and Storm Drainage Plan. 3 copies (11" x 17") |
| Fire Flow Analysis |
| CD or electronic version of submittal items (Word, Jpeg or PDF) |



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SITE PLAN

| | Project name, scale (not to exceed 1" = 50'), north arrow. |
|-------------|--|
| | Date the site plan is prepared. |
| | Street names and locations of all existing and proposed streets, curbs, and |
| | sidewalks within or adjacent to the proposed development. Show distance to centerline of street. |
| | Zoning of each adjacent property. |
| | Square footages by use – existing and proposed (storage, office, meeting, etc.) |
| | Percentage of lot coverage and square footage by; |
| П | a) structures |
| | b) recreation areas |
| | c) landscaping |
| | d) non-permeable surfaces (including parking areas, access aisles) |
| | Total number of parking spaces (existing and proposed). |
| | Total landscaped area square footage (existing and proposed). |
| | All vehicle and pedestrian access points and paths. |
| | Location of all proposed and existing buildings, fences and structures within the |
| | project area. Indicate which ones are to remain and which are to be removed. |
| | Location and size of all public utilities in and adjacent to the site, including: |
| | a) Water lines and meter sizes. |
| | b) Sewers, manholes and cleanouts.c) Storm drains and catch basins. |
| | The proposed location of: |
| | a) Connection to the City water system. |
| | b) Connection to the City sewer system. |
| | c) The proposed method of drainage of the site. |
| | d) Postal box locations, if more than 7 units are proposed. |
| | Location of existing canals and laterals. |
| | Retention of on-site drainage. |
| | Existing easements on the property. |
| | Location and size of any public areas within the development. |
| | All fire hydrants, existing and proposed, within 500 feet of the site. |
| | A topographic map of the site if the slope of the site exceeds 5%. |
| | Locations of all existing natural features including trees, natural drainage ways, |
| | rock outcroppings, et cetera. |
| BUILDING EL | <u>EVATIONS</u> |
| | Drawings or sketches of all four views of each new structure. |
| | Building materials, colors (fascia, doors, trim, etc.), pitch of roof, shape and other |
| | design features of the building(s). |
| | All exterior mechanical devices. |
| LANDSCAPE | PLAN (may be included on the site plan for smaller projects) |
| | i i mi ii i i ii i ii i ii i ii ii ii ii |

Tree and plant species.



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|---|---|
| | Tree and plant sizes (new only). |
| | All trees having a six-inch trunk diameter 3' above grade or greater shall be |
| | shown on the landscape plan. Location/placement of existing and proposed vegetation to be retained, planted or removed. |
| | Approximate location of irrigation lines, and type of irrigation system to be used. |
| FLOOR PLAN | <u>V</u> |
| | All significant rooms within each structure; label or number rooms, including square footage for each room. |
| | Electrical / mechanical equipment areas. |
| <u>LIGHTING PL</u> | <u>.AN</u> |
| | All exterior light locations. |
| | Brochure, illustration, cut sheet or photo for each light fixture type to be used. |
| understands misstatement application, a | this application, the undersigned certifies that he / she has read and the submittal requirements stated above. Note: if the applicant makes a t of fact on the application regarding ownership, authority to submit the acreage, or any other fact material relied upon in making a decision, the City tice to the applicant and subject to an applicant's right to a hearing declare on yold. |
| Owner: | Signature Date: 07/18/2025 |
| Applicant:F | Francis Senger DN. 2-US, E-francis@misslonbuilding.com, Date: |
| | Signature |

LA PINE BUSINESS INCUBATOR

LAND-USE SUBMITTAL

PREPARED FOR

MISSION

MISSION BUILDING & RENOVATION LLC 479 NE ALDEN AVE BEND OR 97701 T. 541.550.2747 FRANCIS SENGER

ARCHITECT



COLLABORATIVE DESIGN ARCHITECTS 2280 GRANT ROAD - SUITE C, BILLINGS, MT 59102 T. 406.248.3443 TRAVIS SMITH, AIA

CIVIL ENGINEERING

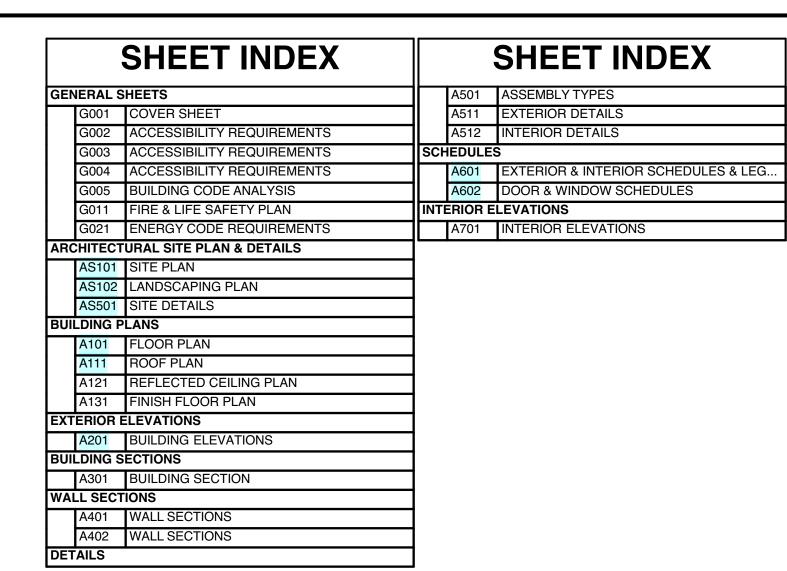


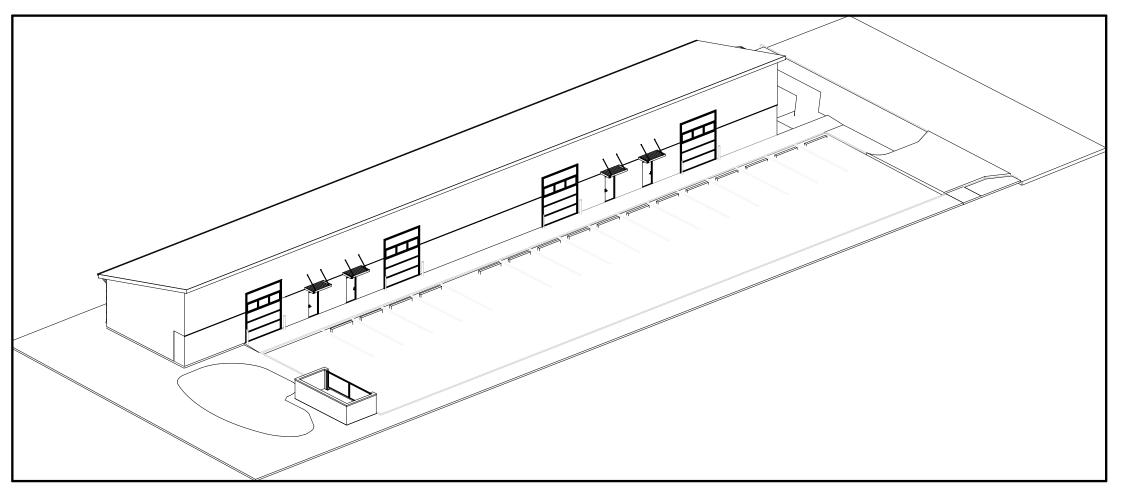
HWA CIVIL ENGINEERING 62930 O. B. RILEY ROAD, SUITE 100 T. 541.389.9351 **GRANT HARDGRAVE, PE**

STRUCTURAL ENGINEERING



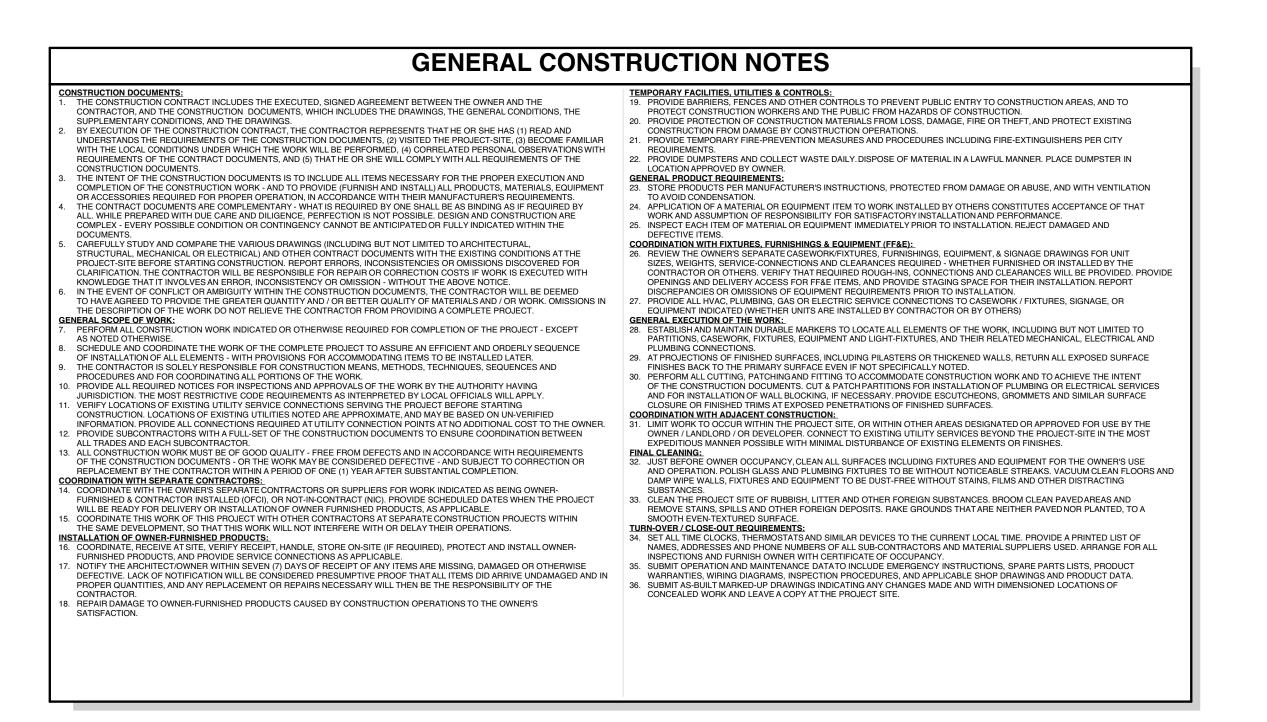
WALKER STRUCTURAL ENGINEERS 2863 NW CROSSING DR. #201, BEND, OR 97703 CRAIG DAVIS, PE





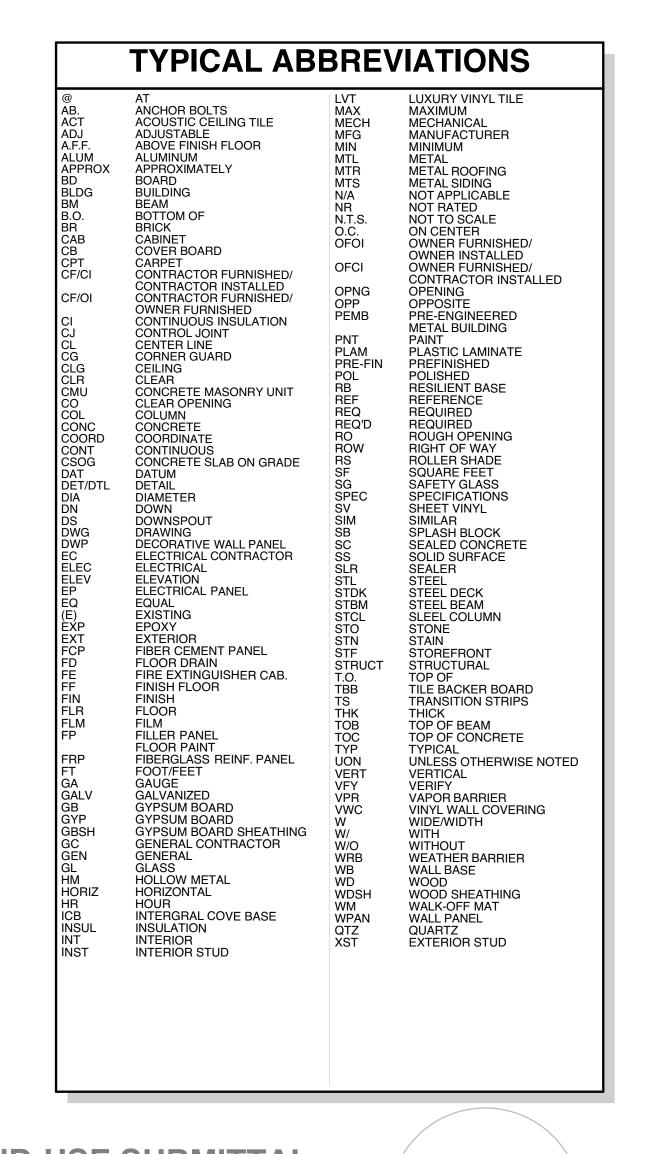
1 DESIGN AXONOMETRIC

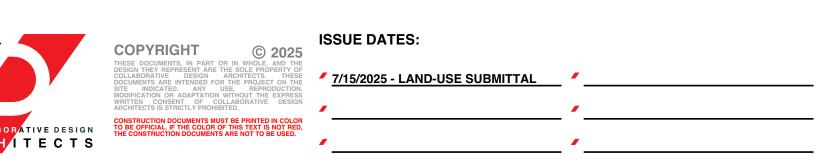
PROJECT LOCATION 1st St Reed Rd Assembly Way Hinkle Wy Dillon Way William Foss Rd Wil



| GRAP | HIC SYMBOLS LEGEN | D |
|-------------------------------------|-------------------|--------------------------------|
| DRAWING # SECTION MARKE | ER | GRID |
| SHEET# | LEVEL NAME X'-X" | LEVEL LINES |
| DRAWING # EXTERIOR ELEVA | ATION MARKER XX | TION |
| SHEET # | | WALL / FLOOR / ROOF TYPE TAG |
| DRAWING # INTERIOR ELEVA DRAWING # | | ENLARGED PLAN OR DETAIL MARKER |
| | SHEET# | |

PREPARED FOR: FRANCIS SENGER







PROJECT ARCHITECT : TRAVIS SMITH , AIA

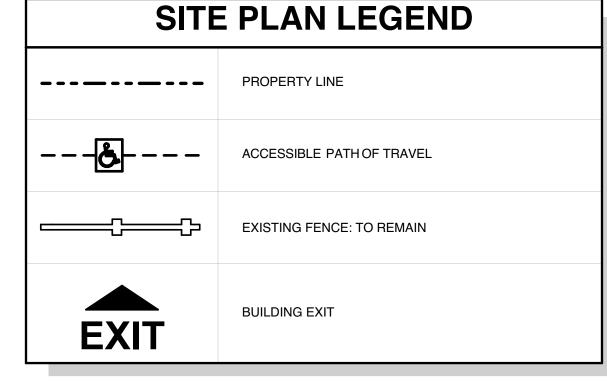
PROJECT NUMBER: 2519



STAMP

10,205 SQ FT x 15% = 1,530 SQ FT

EXISTING SANITARY AND STORM SEWER, WATER, NATURAL GAS, ELECTRICAL, FIBER OPTIC, TELEPHONE, OVERHEAD POWER LINES, AND OTHER UTILITY SYSTEMS BOTH ON SITE AND OFF SITE. THE CONTRACTOR SHALL COMPARE UTILITY INFORMATION WITH THE CONTRACT DOCUMENTS. IF A CONSTRUCTION CONFLICT IS DISCOVERED BETWEEN THE UTILITY INFORMATION OBTAINED AND THE CONTRACT DOCUMENTS NOTIFY THE ARCHITECT IMMEDIATELY. THE CONTRCATOR SHALL MAINTAIN FIRE TRUCK ACCESS TO THE SITE THROUGHOUT THE CONSTRUCTION PROCESS UNLESS AN ALTERNATE PLAN IS APPROVED BY THE FIRE DEPARTMENT.



BUILDING ENTRANCES

SITE PLAN KEYNOTES

- 1. BICYCLE PARKING RACK (4 SPACES) STANDARD INVERTED U-SHAPE BIKE RACKS
- 2. SNOW STORAGE AREA 3. CONCRETE WHEEL STOP
- 4. BOLLARD PER 5/AS501
- 5. KNOX BOX

PROJECT DATA

24,899 SQ FT SITE AREA:

BUILDING USE: SHOP, WAREHOUSE **BUILDING CODE:** 2022 OSSC OCC. GROUP:

CONSTRUCTION TYPE:

F-1 MODERATE HAZARD FACTORY INDUSTRIAL

FIRE SPRINKLERS: FULLY SPRINKLERED

SITE COVERAGE ANALYSIS

TOTAL LOT AREA: 24,899 SQ FT

STRUCTURES: 6,115 SQ FT (24.5%) LANDSCAPING: 7,686 SQ FT (30.9%)

SNOW STORAGE

REQUIRED 15% OF SITE AREA TO BE CLEARED

SITE PLAN



ASSEMBLY WAY

REED ROAD

INCUBATOR 6,090 sq ft

> LAND-USE SUBMITTAL LA PINE BUSINESS INCUBATOR 16628 ASSEMBLY WAY, LA PINE, OREGON 97739

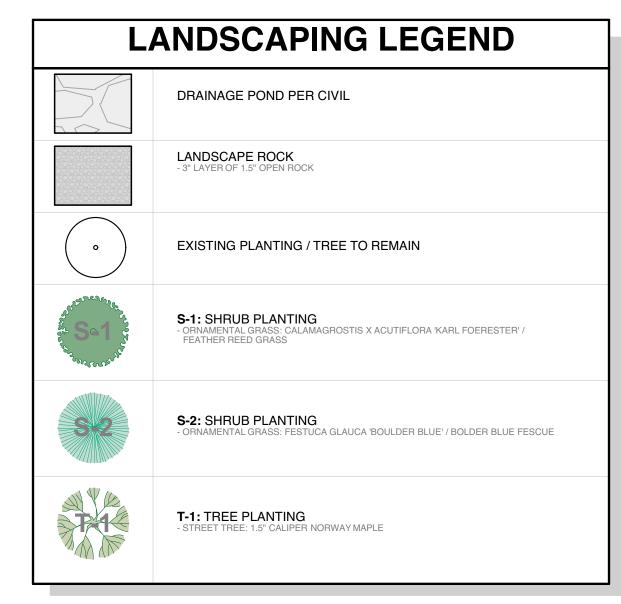
PROJECT NUMBER: 2519 PROJECT ARCHITECT : TRAVIS SMITH , AIA

STAMP **AS101** SITE PLAN

B. INSTALLED PLANT MATERIALS SHALL BY PROPERLY GUYED & STAKED AT TIME OF PLANTING: 1.) SHRUBS: SHALL BE SUPPLIED IN ONE GALLON (MIN) OR SIX INCH BURLAP BALLS WITH A MINIMUM SPREAD OF 12". 2.) TREES: SHALL BE A MINIMUM OF 6'-0" IN HEIGHT & FULLY BRANCHED. C. ALL PLANTINGS INSTALLED IN THE DESIGNATED CLEAR VISION AREAS ARE NOT TO EXCEED 3'-6" IN HEIGHT AT FULL MATURITY.

LANDSCAPING GENERAL NOTES

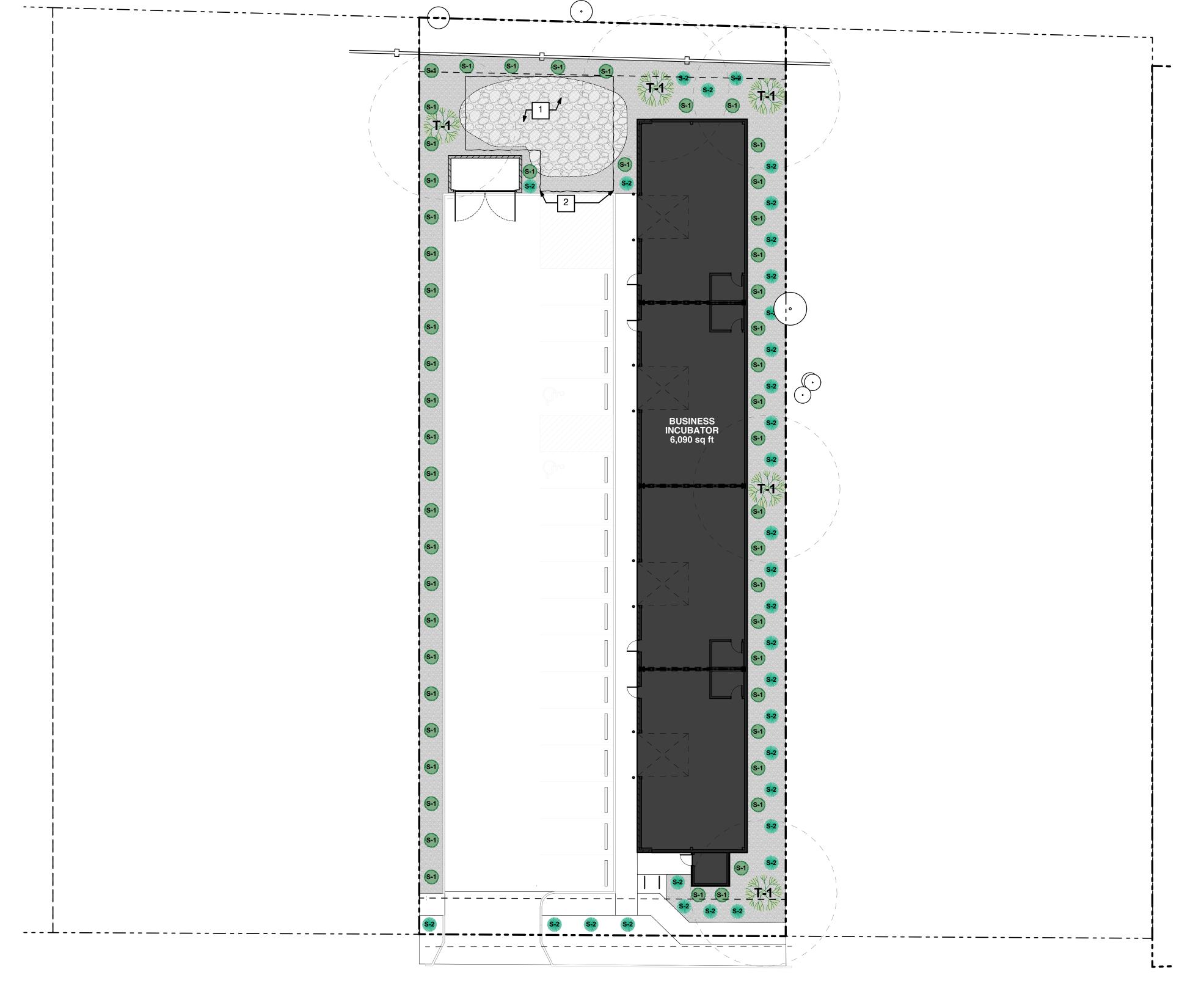
A. ALL IN GROUND IRRIGATION TO BE HIGH EFFICIENCY DRIP EMITTERS TO PLANTING



LANDSCAPING KEYNOTES

1. DRAINAGE POND PER CIVIL

2. SNOW STORAGE AREA, NO PROPOSED LANDSCAPING WITHIN BOUNDARY.



LANDSCAPING PLAN

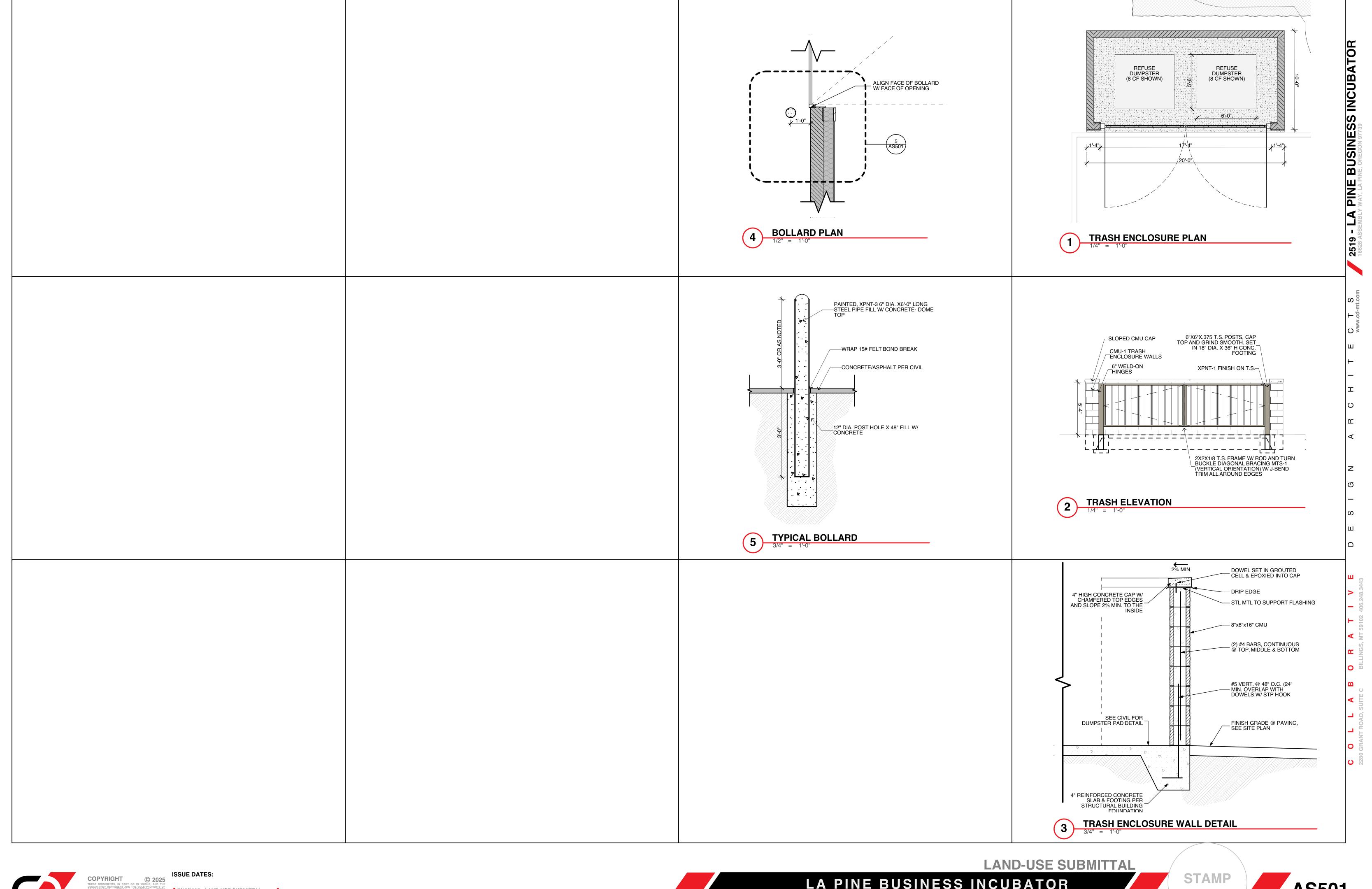






LA PINE BUSINESS INCUBATOR 16628 ASSEMBLY WAY, LA PINE, OREGON 97739







ISSUE DATES:

IN PART OR IN WHOLE, AND THE ENT ARE THE SOLE PROPERTY OF ESIGN ARCHITECTS. THESE NOED FOR THE PROJECT ON THE ANY U.S.E., REPRODUCTION THE ANY U.S.E., REPRODUCTION THE ANY U.S.E. REPRODUCTION TO COLLABORATIVE DESIGN OF COLLABORATIVE DESIGN OF COLLABORATIVE DESIGN OF COLOR OF THIS TEXT IS NOT RED, DOUMENTS ARE NOT TO BE USED.



LA PINE BUSINESS INCUBATOR
16628 ASSEMBLY WAY, LA PINE, OREGON 97739

PROJECT NUMBER: 2519

▼ PROJECT ARCHITECT : TRAVIS SMITH , AIA



FLOOR PLAN GENERAL NOTES

- A. FRAME OF INTERIOR DOORS NOT DIMENSIONED TO BE 4" FROM CORNER OF WALL. FRAME OF EXTERIOR DOORS NOT DIMENSIONED TO BE 6" FROM CORNER
 - WHERE WALL THICKNESS CHANGES IN THE SAME WALL PLANE, KEEP FRAMING ALIGNED AND ADD FINISH MATERIAL NECESSARY TO KEEP FLUSH UNLESS
 - OTHERWISE NOTED. CONCRETE FOUNDATION WALLS TO HAVE EXTERIOR INSULATION PER WALL
 - . WALLS THAT DEMISE TENANT SPACES SHALL CONTINUE TO UNDERSIDE OF ROOF STRUCTURE, TYPICAL.

FLOOR PLAN LEGEND

COORDINATE WALL ASSEMBLY TO CONCEAL PEMB MEMBER IN ITS ENTIRETY RESTROOM RESTROOM UTILITY SINK UTILITY SINK
PER PLUMBING PER PLUMBING

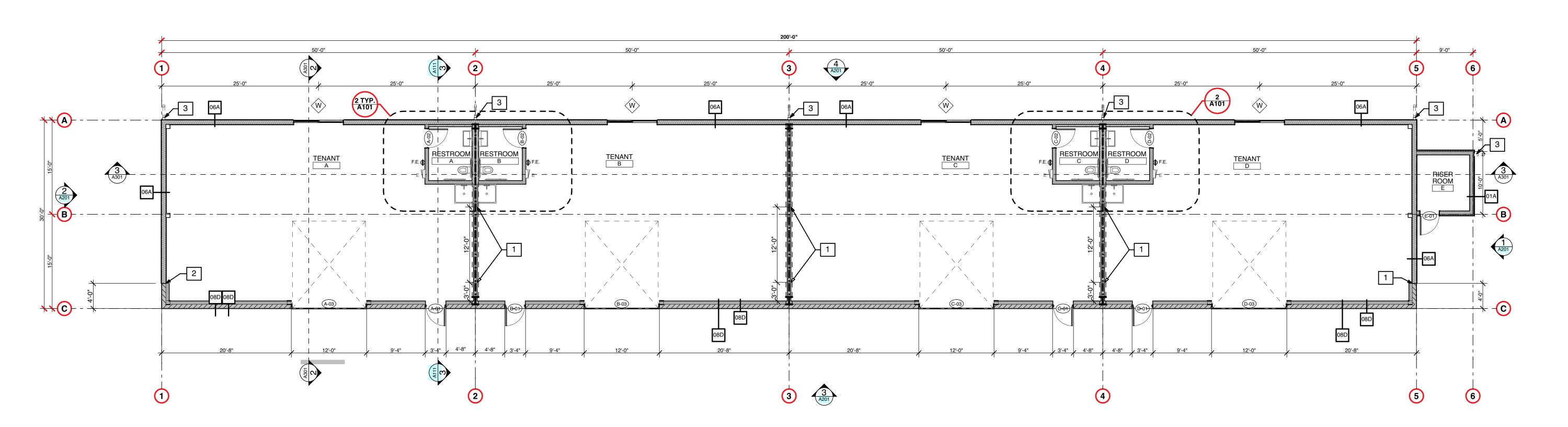
5LB A:B:C FIRE EXTINGUISHER ON WALL BRACKET ELECTRICAL PANEL PER ELECTRICAL

> PEMB MAIN FRAME PER SUPPLIER. COORDINATE CENTER WALL PARTITION ON MEMBER UNLESS OTHERWISE NOTED PER ACOUSTIC SEALANT -J-BEAD, BOTH SIDES -INTERIOR WALL PER **DEFLECTION CLIP**

PARTITION BRACING AT STRUCTURE

TYP. RESTROOM ENLARGED FLOOR PLAN

3/8" = 1'-0"



FLOOR PLAN KEYNOTES

1. COORDINATED WALL FRAMING FOR FUTURE 12'-0" H CLEAR OPENING IN

2. ALIGN WALL ASSEMBLIES AS NOTED FOR FLUSH INTERIOR FINISH FACE

PARTITION SIZED. SEE PLANS FOR EXTENTS.

3. DOWNSPOUT LOCATION

FLOOR PLAN1/8" = 1'-0"







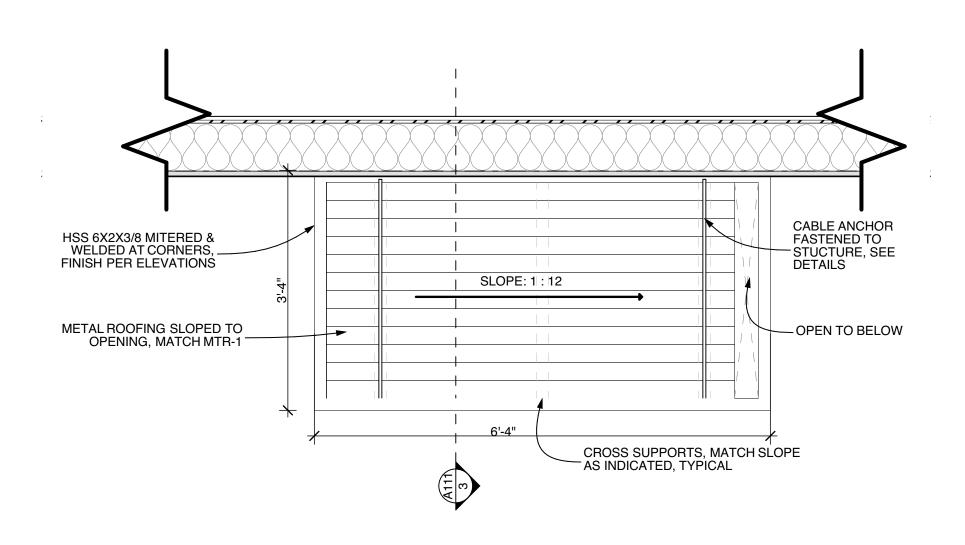
ROOF PLAN LEGEND

1. ROOF EAVE PER PEMB MANUFACTURER, EXTENTS PER PLANS

4. DOWNSPOUT LOCATION

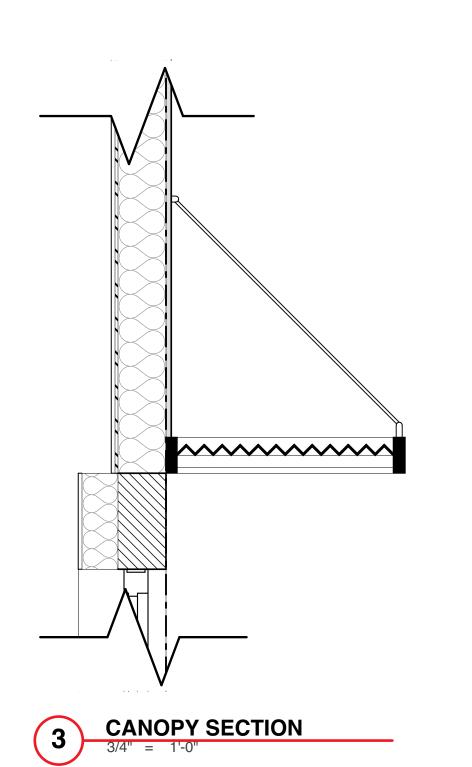
ROOF PLAN KEYNOTES

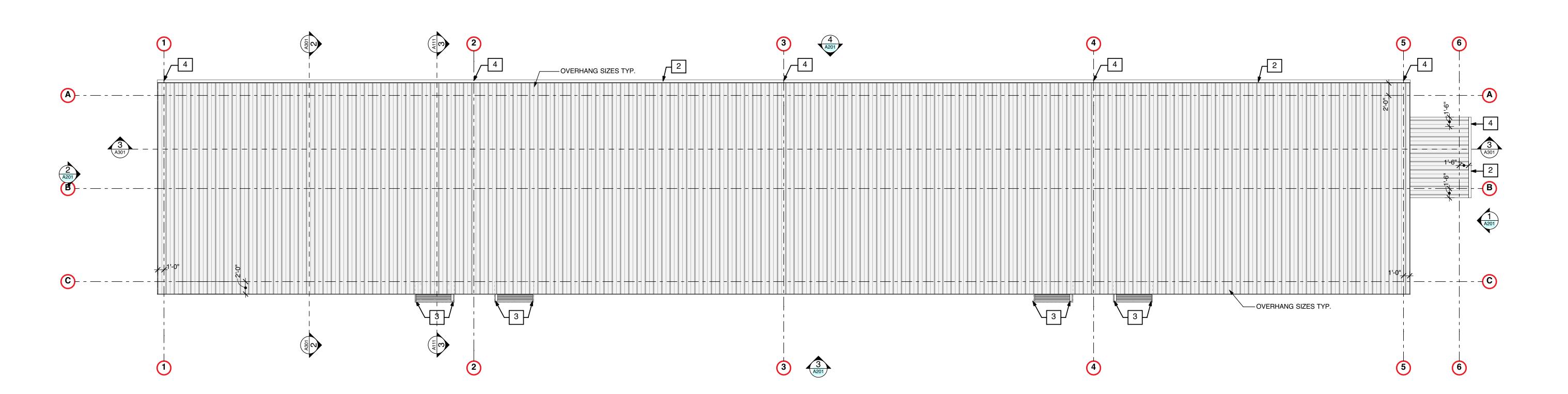
- 3. CANOPY PER DETAILS, CENTER ALIGN TO MAN DOOR LOCATIONS PER PLANS, DASHED LINE INDICATES EXTENTS. SEE DETAILS FOR DIMENSIONS.
- STANDING SEAM METAL ROOFING COLOR: MTR-1
- **ROOF PLAN GENERAL NOTES** A. GENERAL CONTRACTOR TO PROVIDE MECHANICALLY FASTENED SNOW STOPS
 - PER METAL ROOFING MANUFACTURER B. DOWNSPOUT OUTLETS SHALL BE PLACED AWAY FROM THE FOOTING AT A DISTANCE SPECIFIED IN THE GEOTECHNICAL REPORT. TYPICAL. C. PROVIDE HEAT TAPE FOR ALL DOWNSPOUTS AND GUTTERS



CANOPY PLAN DETAIL

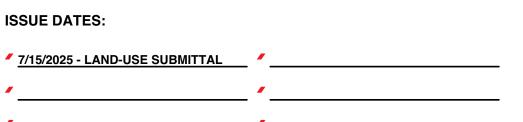
3/4" = 1'-0"





ROOF PLAN1/8" = 1'-0"









PREPARED FOR: FRANCIS SENGER

B. ALL FINISHES SHALL BE APPLIED ACCORDING TO MANUFACTURER

TRADES TO AVOID DAMAGE TO OTHER BUILDING SYSTEMS.

A. ALL COLOR, MATERIAL AND FINISH APPLICATIONS TO BE APPROVED BY THE OWNER AND THE ARCHITECT PRIOR TO APPLICATION. THE CONTRACTOR SHALL PROVIDE SAMPLES OF THE COLORS AND FINISHES PRIOR TO FABRICATION/

SPECIFICATIONS. CONTRACTORS SHALL COORDINATE APPLICATIONS WITH OTHER

(PREFINISHED). COLOR TO MATCH ADJACENT MATERIAL. PROVIDE SAMPLES FOR

E. CAULKING SHALL BE URETHANE TYPE SIKAFLEX OR APPROVED EQUAL OF COLOR

C. ALL WINDOWS AND STOREFRONT SHALL BE FROM THE SAME MANUFACTURER.

D. FLASHINGS SHALL BE 24 GAUGE GALVANIZED METAL (PREFINISHED) WITH ALL

COUNTER FLASHING TO BE 26 GAUGE GALVANIZED METAL AS REQUIRED

TO MATCH ADJACENT SURFACE AND INSTALLED PER MANUFACTURER'S

INSTRUCTIONS. PROVIDE PROPER BACKER RODS WHERE REQUIRED.

NOTE: NOT ALL KEYNOTES MAY BE USED ON THIS SHEET

PRE-FINISHED CONTINUOUS METAL GUTTER WITH EXTRA SNOW LOAD BRACING

EXTERIOR ELEVATION KEYNOTES

- PRE-FINISHED DOWNSPOUT AND CONDUCTOR HEAD
 METAL SIDING CORNER TRIM PER PEMB MANUFACTURER
- METAL SIDING EDGE TRIM PER PEMB MANUFACTURER METAL ROOF FASCIA PER PEMB MANUFACTURER
- CONTINUOUS METAL SNOW FENCE ATTACHED TO STANDING SEAM ROOF RIBS BUILDING ADDRESS SIGNAGE. 1/2" ALUMINUM LETTERS ON 1/2" STANDOFFS. TEXT
- AND FONT PER OWNER . EXTERIOR LIGHT PER ELECTRICAL. CENTERED 6" OVER DOOR OPENING
- 10. EXTERIOR BOLLARD
- 11. FIRE DEPARTMENT CONNECTION PER PLUMBING

- 12. GAS SERVICE PER ELECTRICAL
 13. ELECTRICAL SERVICE PER ELECTRICAL
- 14. AWNING PER PEMB MANUFACTURER
- 15. WALL MOUNTED ACCESIBLE PARKING SIGN 16. 3200 SERIES KNOX BOX LOCATION. MOUNTING HEIGHT PER MANUFACTURER
- 17. EXTERIOR LIGHT PER ELECTRICAL
 18. CMU CONTROL JOINTS

21'-6" B.O. UPPER EAVE

F. A LIGHTER HATCH IS USED TO INDICATE DEPTH IN DRAWING FOR CLARITY.

ARCHITECT/OWNER APPROVAL.

APPLICATION.

G. PROVIDE AND COORDINATE WITH ARCHITECT LOCATIONS OF VERTICAL AND/OR HORIZONTAL EXPANSION JOINTS IN EXTERIOR CLADDING AS REQUIRED BY MANUFACTURER.

SOUTH ELEVATION

B.O. UPPER EAVE

4 | MTS-1 _

5 | MTS-1

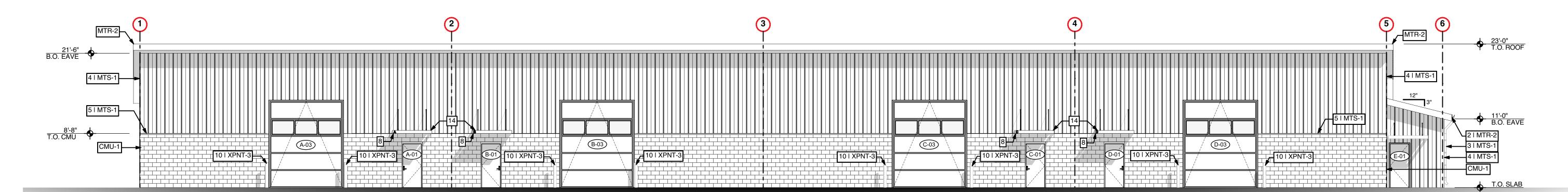
NORTH ELEVATION

B.O. LOWER EAVE

2 I MTR-2

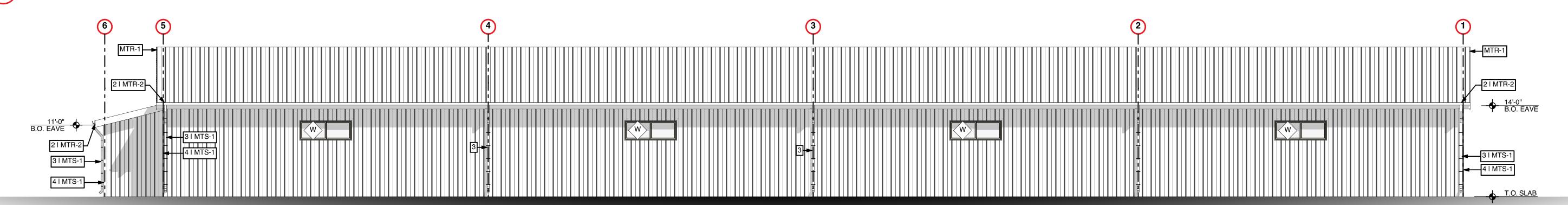
3 I MTS-1

4 | MTS-1



WEST ELEVATION

1/8" = 1'-0"



EAST ELEVATION

1/8" = 1'-0"











| INTERIOR | MATERIAL SCH | EDULE | | | | |
|-------------|----------------------|---------------------------|---------------------------|--|--|---------------|
| MATERIAL ID | MATERIAL IMAGE | DESCRIPTION | MANUFACTURER | PRODUCT INFORMATION | NOTES | SPECIFICATION |
| FLOORING - | BASE - TRANSITIONS | | | | | |
| WB-1 | | RUBBER WALL BASE | PATCRAFT | COLLECTION: FINISHWORX COLOR: EARL GREY #5 DESCRIPTION: 4.25" WALL BASE | PER MANUFACTURER'S SPECIFICATIONS | - |
| SC-1 | | SEALED CONCRETE | BY CONTRACTOR | COLOR: GRAY | PER MANUFACTURER SPECIFICATIONS | - |
| PAINT - COA | TINGS - WALL FINISHE | S | | | | |
| PNT-1 | | PAINT COLOR | SHERWIN WILLIAMS | COLOR: NATURAL WHITE NUMBER: SW-9542 | WALLS & CEILINGS: SEMI-GLOSS SHEEN METAL FINSHES: HIGH- GLOSS SHEEN WET AREAS: EPOXY PAINT | - |
| TILE - WALL | COVERINGS - TRANSIT | TIONS | | | | |
| FRP-1 | | FIBER REINFORCED PANEL | CRANE COMPOSITES | COLOR: GRAY | 5' HEIGHT TYPICAL | - |
| CG-1 | , | CORNER GUARD | THE CORNER GUARD STORE | COLLECTION: 90 DEGREE, 16 g.a. COLOR: STAIN #4, SS DESCRIPTION: 2'x2" WINGS, 48" H | PER MANUFACTURER SPECIFICATIONS | - |
| TRM-1 | | SHADOW MOLD | BY CONTRACTOR | COLOR: STAIN #4, SS | PER MANUFACTURER SPECIFICATIONS | - |

| EXTERIOR | MATERIAL SCH | IEDULE | | | | |
|----------------|---|--------------------------|---------------------------------|--|--|---------------|
| MATERIAL ID | MATERIAL IMAGE | DESCRIPTION | MANUFACTURER | PRODUCT INFORMATION | NOTES | SPECIFICATION |
| METALS | | | | | | |
| MTS-1 | | METAL SIDING | AMERICAN BUILDING COMPONENTS | COLLECTION: PBR PANELS COLOR: POLAR WHITE DESCRIPTION: 26 Ga., 1 1/4" RIBS | EXPOSED FASTENER | - |
| MTR-1 | | METAL ROOFING | AMERICAN BUILDING COMPONENTS | COLLECTION: PBR PANELS COLOR: POLAR WHITE DESCRIPTION: 26 Ga., 1 1/4" RIBS | EXPOSED FASTENER | - |
| MTR-2 | St. | METAL TRIMS | AMERICAN BUILDING COMPONENTS | COLLECTION: SCULPTURED COLOR: TRUE BLACK DESCRIPTION: COMPONENTS | SCULPTURED EAVE, GUTTER, RAKE & CORNER TRIMS PER SIDING SUPPLIER | - |
| MASONRY | | | | | | |
| CMU-1 | | CONCRETE MASONRY UNIT | RCP BLOCK & BRICK | <u>SERIES:</u> PER MFG <u>COLOR:</u> GRAY | SEE STRUCTURAL DRAWINGS | - |
| EXTERIOR PA | AINT - STAIN - POWDER | R COAT - MISC | | • | | |
| XPNT-1 | | EXTERIOR PAINT COLOR | SHERWIN WILLIAMS | COLOR: TRICORN BLACK | METAL FINSHES: HIGH- GLOSS SHEEN | - |
| XPNT-2 | | EXTERIOR PAINT COLOR | SHERWIN WILLIAMS | COLOR: SAFETY YELLOW | METAL FINSHES: HIGH- GLOSS SHEEN | - |
| XPNT-3 | | EXTERIOR PAINT COLOR | SHERWIN WILLIAMS | COLOR: CHARCOAL GRAY | METAL FINSHES: HIGH- GLOSS SHEEN | - |
| XPNT-4 | | EXTERIOR PAINT COLOR | SHERWIN WILLIAMS | COLOR: MATCH - | METAL FINSHES: HIGH- GLOSS SHEEN | - |
| XPNT-5 | | EXTERIOR PAINT COLOR | SHERWIN WILLIAMS | COLOR: MATCH - | METAL FINSHES: HIGH- GLOSS SHEEN | - |
| WINDOWS - G | GLAZING | | <u>l</u> | <u> </u> | | |
| ALUM WINDOW | | | | SEE WINDOW SCHEDULE | | |
| GLAZING | | | | SEE WINDOW SCHEDULE | | |

LAND-USE SUBMITTAL

PROJECT NUMBER: 2519



LA PINE BUSINESS INCUBATOR 16628 ASSEMBLY WAY, LA PINE, OREGON 97739 PROJECT ARCHITECT : TRAVIS SMITH , AIA PREPARED FOR: FRANCIS SENGER



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PINE

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BE BEVELED WITH A SLOPE NO GREATER THAN 1:2

A DOOR SHALL BE AS FOLLOWS:

FINISH TO MATCH LEVERS.

PLANS PRIOR TO ORDERING DOORS.

D-1. BASIS OF DESIGN: CORNELL ESC20

D-6. DOOR ON MAGNETIC HOLD OPEN

D-3. OVERHEAD DOOR WITH POWER OPERATOR.

D-7. DOOR EQUIPPED WITH ACCESS CONTROL

D-8. DOOR EQUIPPED WITH POSITION SENSOR

D-4. EGRESS HARDWARE ON EXTERIOR OF DOOR.

D-5. DOOR ON MAGNETIC HOLD OPEN LINKED TO FIRE ALARM

D-2. DOOR TO SWING 180 DEG.

A. EXTERIOR HINGED DOORS: (RESERVED)

B. INTERIOR HINGED DOORS: 5 LBF (22.2N) C. SLIDING OR FOLDING DOORS: 5 LBF (22.2N)

C. DOOR HARDWARE. HANDLES, PULLS, LATCHES, LOCKS, AND OTHER OPERATING DEVICES ON ACCESSIBLE DOORS SHALL HAVE A SHAPE THAT IS EASY TO GRASP

WITH ONE HAND AND DOES NOT REQUIRE TIGHT GRASPING, TIGHT PINCHING,

DEGREES, THE DOOR WILL TAKE AT LEAST 3 SECONDS TO MOVE TO A POINT 3 IN

(75 MM) FROM THE LATCH, MEASURED TO THE LEADING EDGE OF THE DOOR.

E. DOOR OPENING FORCE. THE MAXIMUM FORCE FOR PUSHING OR PULLING OPEN

THESE FORCES DO NOT APPLY TO THE FORCE REQUIRED TO RETRACT LATCH

BOLTS OR DISENGAGE OTHER DEVICES THAT HOLDS THE DOOR IN A CLOSED

G. ALL SWINGING DOORS TO BE SET WITH (2) PAIRS OF BALL-BEARING HINGES IN A

DOOR SCHEDULE NOTES

H. GENERAL CONTRACTOR TO VERIFY DOOR SWINGS & STOP LOCATIONS WITH

F. VESTIBULE DOOR TO INCLUDE FULL THRESHOLD, BUMPERS, WEATHER STRIPPING, OR OTHER ESSENTIAL WEATHER PROOFING ACCESSORIES.

OR TWISTING OF THE WRIST TO OPERATE. LEVER-OPERATED MECHANISMS, PUSH-TYPE MECHANISMS, AND U-SHAPED HANDLES ARE ACCEPTABLE DESIGNS.

D. DOOR CLOSERS. IF A DOOR HAS A CLOSER, THEN THE SWEEP PERIOD OF THE

CLOSER SHALL BE ADJUSTED SO THAT FROM AN OPEN POSITION OF 70

| WINDOW TYPE | WIDTH | HEIGHT | SILL HEIGHT | MATERIAL | FINISH | FIRE RATING | SHGC | U-VALUE | NOTES |
|----------------|-------|--------|-------------|----------|--------|-------------|------|---------|-------|
| W | 8'-0" | 3'-0" | 8'-8" | Others | TBD | | | | |

| DOOR TYPES | | | |
|--------------------|---|---|---|
| TYPE | Α | В | G |
| FRONT ELEVATION | | | |

FRAME

MATL

HM-IS

STEEL

HM-IS

STEEL

HM-IS

STEEL

HM-IS

STEEL

HM-IS

FINISH

PNT-1

PNT-1

PNT-1

PNT-1

PNT-1

PNT-1

PNT-1

PNT-1

PNT-1

HARDWARE

SET NO

RATING

U-VALUE SHGC

NOTES

DOOR

PNT-1

PNT-1

WHITE

PNT-2

PNT-1

PNT-2

PNT-1

PNT-1

PNT-1

STEEL | WHITE | SEE ELEV

WHITE

GLAZING

SEE ELEV

SEE ELEV

WHITE | SEE ELEV

HM-IS

WD

STEEL

HM-IS

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STEEL

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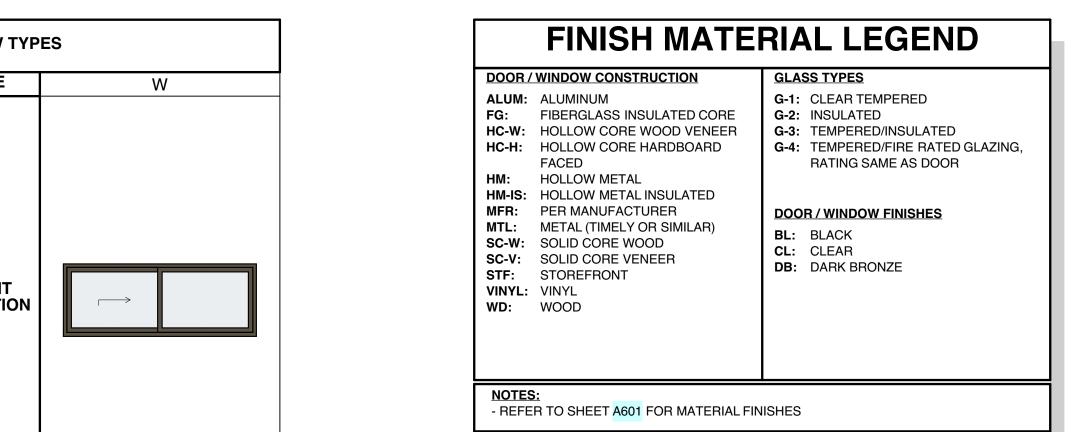
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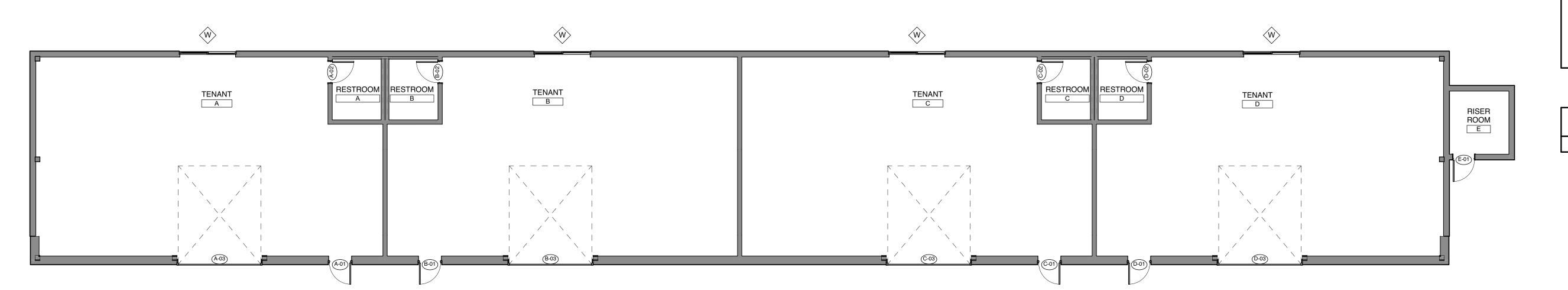
G

Α

G |

| TYPE | W |
|--------------------|---|
| FRONT ELEVATION | |





OPENING REFERENCE PLAN



PROJECT NUMBER: 2519

PREPARED FOR: FRANCIS SENGER

LAND-USE SUBMITTAL LA PINE BUSINESS INCUBATOR 16628 ASSEMBLY WAY, LA PINE, OREGON 97739

PROJECT ARCHITECT : TRAVIS SMITH , AIA

STAMP A602 DOOR & WINDOW SCHEDULES

WINDOW SCHEDULE NOTES

WINDOW GENERAL NOTES

A. WINDOW SCHEDULE AND WINDOW ELEVATIONS ARE FOR DESIGN REFERENCE ONLY. GENERAL CONTRACTOR TO VERIFY ALL QUANTITIES AND WINDOW SIZES

PRIOR TO CONSTRUCTION. GENERAL CONTRACTOR TO VERIFY ALL QUANTITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL PROVIDE "SAFETY" GLASS FOR ALL WINDOWS WHERE

APPLIES IN ACCORDANCE WITH IBC. . ALL WINDOW UNITS SHALL BE PROPERLY AND SECURELY INSTALLED IN

ACCORDANCE TO MANUFACTURER'S DETAILS AND SPECIFICATIONS PROVIDING ALL NECESSARY ANCHORS, SHIMS, INSULATION, FLASHING, CAULKING, ETC. WITH FINISH TRIMS AS REQUIRED PROVIDING A COMPLETE AND FULLY WEATHER TIGHT, CERTIFIED AND TOTALLY FINISHED INSTALLATION. R and/or U-VALUES SHALL BE LABELED AS "CERTIFIED."

. CONTRACTOR SHALL VERIFY ALL ACTUAL SIZES OF ROUGH OPENINGS PRIOR TO COMMENCING FABRICATION, TYPICAL.

PROVIDE ALL NECESSARY BLOCKING, NAILERS, FASTENERS, ETC., AS REQUIRED FOR A SECURE AND CRAFTSMAN-LIKE INSTALLATION. 3. CONTRACTOR SHALL VERIFY AND INSURE THAT ALL WINDOW SIZES

CONSTRUCTION, AND FEATURES SHALL MEET ALL EGRESS REQUIREMENTS, OPENING SIZES AND MAXIMUM SILL HEIGHTS AS REQUIRED BY APPLICABLE BUILDING CODE REQUIREMENTS AND SHALL MODIFY ACCORDINGLY. INSECT SCREENS SHALL BE PROVIDED FOR ALL OPERABLE SASH UNITS WITH

REMOVABLE TYPE SCREEN AND FRAME TO PROPERLY MATCH WINDOW UNIT. SHOP DRAWINGS ARE REQUIRED FOR ALL ASSEMBLIES, CORRECTED, AND SUBMITTED BY THE CONTRACTOR FOR REVIEW AND APPROVAL BY THE

FOR ALL OPENINGS WITH MULTIPLE WINDOW UNITS, IT IS THE RESPONSIBILITY OF THE WINDOW MANUFACTURER TO VERIFY RESPECTIVE WIND LOADING REQUIREMENTS AND PROVIDE FULL AND PROPER REINFORCEMENT AND OTHER PROVISIONS AS REQUIRED.

. CONTRACTOR SHALL VERIFY AND COORDINATE PROPER FRAME SIZE WITH APPROPRIATE PARTITION AND WALL TYPE, SIZE AND CONSTRUCTION.

DOOR AND FRAME SCHEDULE

A-01 3'-0"

A-02 3'-0"

A-03 | 12'-0"

B-01 3'-0"

B-02 3'-0"

B-03 12'-0"

C-01 3'-0"

C-02 3'-0"

C-03 | 12'-0"

D-01 3'-0"

D-02 3'-0"

E-01 3'-0"

D-03 12'-0" 14'-0"

WIDTH | HEIGHT | TYPE

7'-0"

7'-0"

14'-0"

7'-0"

7'-0"

14'-0"

7'-0"

7'-0"

14'-0"

7'-0"

7'-0"

7'-0"

City of La Pine Site Plan Review

Burden of Proof Narrative - La Pine Small Business Incubator Facility

Applicant: Central Oregon Intergovernmental Council (COIC)

334 NE Hawthorne Avenue

Bend, OR 97701

Applicant's Mission Building and Renovation LLC

Agent: Francis Senger

479 NE Alden Avenue

Bend, OR 97701

Owner: City of La Pine

PO Box 2460

La Pine, OR 97739

Architect: Collaborative Design Architects

Travis Smith

2280 Grant Road - Suite C

Billings, MT 59102

Civil Engineer: HWA

Grant Hardgrave

62930 OB Riley Road, Suite 100

Bend, OR 97701

Planner: Blackmore Planning and Development Services, LLC

Greg Blackmore 19454 Sunshine Way Bend, OR 97702

Location: The property is located in the southern area of the City of La Pine,

on the south side of Reed Road, approximately 1,500 feet east of Hwy 97. The property has been assigned the address of 16628 Assembly Way and it is identified by the Deschutes County Tax

Assessor as 221014AB00108.

Request: Type II Site Plan review for a single story, 6,087 square foot

industrial building, associated with the La Pine Business Small Business Incubator Program. Consistent with the RFP for the project, the design includes 4-suites with individual restrooms, adjacent parking lot, landscaping, drainage swales, street trees, decorative lighting, a 6' sidewalk, sidewalk aesthetics, and driveway

from Assembly Way.

I. Applicable Criteria and Procedures:

- 1. City of La Pine Development Code
- ARTICLE 3 ZONING DISTRICTS
 - Chapter 15.24 Industrial and Public Facility Zones
- ARTICLE 5 DEVELOPMENT STANDARDS
 - Chapter 15.80 Development Standards, Generally
 - Chapter 15.82 Landscaping, Buffering and Fences
 - o Chapter 15.86 Parking and Loading
 - o Chapter 15.88 Access and Circulation
 - o Chapter 15.90 Public Facilities
 - Chapter 15.94 Improvement Procedures and Guarantees
- ARTICLE 7 PROCEDURES
- ARTICLE 8 APPLICATIONS AND REVIEWS
 - Chapter 15.312 Site Plan Review

II. General Facts:

1. **LOCATION:** The property is located in the southern area of the City of La Pine, on the south side of Reed Road, approximately 1,500 feet east of Hwy 97. The property has been assigned the address of 16628 Assembly Way and it is identified by the Deschutes County Tax Assessor as 221014AB00108.



2. EXISTING ZONING AND COMPREHENSIVE PLAN DESIGNATIONS: The property is zoned and designed Industrial (I).

Surrounding Zoning -

An excerpt from the La Pine Zoning Map is included below:

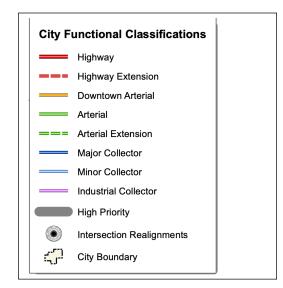


3. SITE DESCRIPTION & SURROUNDING USES: The property is .57 acres in size and rectangular in shape. The property is vacant and vegetated with native vegetation including antelope bitterbrush and Idaho fescue, along with lodge pole pine trees. Soils consist of NRCS 115A Soil – Shananan loamy coarse sand. The property generally level and unimproved. The property is located outside of any FEMA designated floodway and/or floodplain.

The property to the north (which is owned by the City of La Pine) is zoned Public Facilities (PF) and the properties to the east, south and west (which are privately owned) are zoned Industrial (I). There is a limited amount of industrial development in the area, the majority of the surrounding properties are vacant.

The La Pine Transportation System Plan (TSP) identifies Reed Road (to the north) as an Industrial Collector and Assembly Way (to the south) as a local street:





Reed Road is developed with 2 vehicular travels lanes within 80 feet of right-of-way and Assembly Way is developed with 2 vehicular travel lanes and on-street parking within 60 feet of right-of-way. An 8-inch water sewer main, a 4-inch pressure sewer main, and franchise utilities are located in the Assembly Way right-of-way.

- 4. **PROPOSAL:** Type II Site Plan review for a single story, 6,087 square foot industrial building, associated with the La Pine Business Small Business Incubator Program. Consistent with the RFP for the project, the design includes 4-suites with individual restrooms, adjacent parking lot, landscaping, drainage swales, street trees, decorative lighting, a 6' sidewalk, sidewalk aesthetics, and driveway from Assembly Way.
- **5**. **EXHIBITS:** In addition to this burden of proof statement, the applicant submits the following exhibits in support of this proposal:
 - Application Form and Fee
 - Ownership Deed
 - Title Report
 - City of La Pine Small Business Incubator Facility RFP
 - Architectural Plan Set
 - Cover Sheet
 - o Site Plans
 - Floor Plans
 - Roof Plan
 - Elevations
 - Color and Material Details
 - Trash Enclosure Details
 - Engineering Plan Set
 - o Existing Conditions and Demo Plan
 - Site Plan
 - Grading and Drainage Plan
 - Utility Plan

- Landscape Plan
- Traffic Generation Letter
- **6. LOT LEGALITY:** The property was platted as Lot 9, Block 1 of the La Pine Meadows No. subdivision and is therefore a lot of record.

III. Conformance with Procedures, Standards, and Approval Criteria

A. Article 3 – Zoning Districts:

Chapter 15.24 – Industrial and Public Facilities Zones

Sec. 15.24.100. - Purpose.

<u>Chapter 15.24</u> regulates allowed land uses ("uses") and sets forth lot and development standards, including without limitation minimum dimensions, area, density, coverage, structure height, and other provisions that control the intensity, scale, and location of development in the industrial and public facility zones. The regulations of this chapter are intended to implement the City of La Pine Comprehensive Plan.

Applicant Response: This section is a purpose statement; it does not establish any measurable development standards or approval criteria. It is understood that a project that conforms to the standards of this and other sections of the Code will carry out this stated purpose.

Sec. 15.24.200. - Characteristics of the industrial and public facility zones.

Industrial and public facility zones accommodate a mix of intensive and less intensive uses engaged in manufacturing, processing, warehousing, distribution, and similar activities. Two industrial zoning districts, one for light industrial uses and one for general industrial uses, provide for the full range of planned industrial land uses within the city. Both districts are intended to provide for efficient use of land and public services, provide a high-quality environment for business, offer a range of parcel sizes and locations for industrial site selection, avoid encroachment by incompatible uses, provide transportation options for employees and customers, and facilitate compatibility between dissimilar uses.

- A. Light Industrial Zone (LI). The LI zone is intended to allow for a mix of industrial and manufacturing businesses alongside industrial services, research and development, and small-scale retail and professional services.
- B. Industrial Zone (I). The I zone allows for the same uses as the LI zone, but also provides suitable locations for more intensive industrial uses, such as those with processing, manufacturing, assembly, packaging, distribution, or other activities.

Applicant Response: While the Development Code indicates that the City has both an Industrial (I) Zone and a Light Industrial (LI) Zone, the City of La Pine Zoning Map and Comprehensive Plan Map do not contain a Light Industrial label or any Light Industrial identified areas. The property is zoned I. As detailed below and documented on the attached Exhibits, the existing lot size, dimensions, proximity to other zones, and public facilities can accommodate the proposed improvements. The proposal adds new use areas and a new building, as well as associated site improvements and parking. As documented on the Plan Set, the property's location allows for an efficient and functional development; the design will allow the establishment of needed small business uses in an industrial area; the proposal is consistent with the desired characteristics of the LZone.

C. Public Facility Zone (PF). The PF zone is intended to provide areas for large-scale public facility and utility uses that require separation from residential and commercial uses. Additionally, the PF zone accommodates industrial uses that are compatible with large-scale public facilities.

Applicant Response: The property is not zoned PF. The provisions / characteristics of this section are not relevant.

Sec. 15.24.300. - Use regulations.

Uses may be designated as permitted, limited, conditional, or prohibited in the industrial and public facility zones. As noted in Table 15.24-1, a use may also be subject to special use standards of <u>article 6</u>.

Applicant Response: The project is for the La Pine Small Business Incubator Facility, sponsored by the City of La Pine and COIC. Specific uses and users have not been established. The development is for a spec building that will be able to provide the greatest flexibility (for future users / uses) as possible.

- A. *Permitted uses (P).* Uses allowed outright in the industrial and public facility zones are listed in Table 15.24-1 with a "P."
- B. Limited uses (L). Uses allowed in the industrial and public facility zones subject to limitations are listed in Table 15.24-1 with an "L." The limitations are defined below and correspond with the footnote numbers in Table 15.24-1.
 - 1. Eating and drinking establishments in the LI zone. Eating and drinking establishments in the LI zone are limited to 2,500 square feet of gross floor area.

Applicant Response: The property is not located within a LI Zone; this provision does not apply.

2. Offices in the LI, I, and PF zones. Offices as a primary use are limited to industrial offices (as defined in section 15.14.235) and government

offices that do not include a point-of-service facility. All other office uses must be accessory to a permitted industrial use.

Applicant Response: Section 15.14.235 established industrial offices as follows:

2. Industrial office. Software and internet content development and publishing; computer systems design and programming; graphic and industrial design; engineers; architects; telecommunication service providers; data processing; television, video, radio, and internet studios and broadcasting; scientific and technical services; customer call centers; research and development laboratories; and medical and dental laboratories.

The applicant understands that the above referenced office types are permitted in the Zone and in order to allow the greatest amount of flexibility associated with the La Pine Small Business Incubator Facility, the proposal includes this use as a potential use within the suites of the building.

3. Retail sales and services in the LI zone...

Applicant Response: The property is not located within the LI Zone, this section does not apply.

4. Automotive wrecking, salvage, and junk yards. The storage or sale of junk requires a special license, see section 15.108.040.

Applicant Response: Automotive wrecking, salvage and junk yards require a conditional use permit. The proposal does not include automotive wrecking, salvage, or a junk yard and a conditional use permit is not included with this proposal. This reference is not applicable.

- 5. General manufacturing and production in the LI zone...
- 6. Warehouse and freight movement in the LI zones...
- 7. Community services in the LI zone...
- 8. Agriculture in the LI zone...

Applicant Response: The property is not located within a LI Zone, therefore these standards do not apply.

9. *Marijuana facilities in the I zone.* Marijuana testing laboratories are permitted outright. Marijuana processing facilities, production facilities, or wholesalers are allowed with a conditional use permit.

Applicant Response: The applicant understands that marijuana testing laboratories are permitted in the I Zone, however that marijuana processing, production or wholesaling would require a conditional use permit. Marijuana testing requires

additional details, which are not included at this time. No marijuana uses are proposed.

10. Retail sales and services in the I and PF zones. Retail sales and services in the I and PF zones are limited to mobile food unit sites.

Applicant Response: The design does not include any mobile food units; thus this use is not proposed at this time. In the event that the Applicant / Owner desired to place a mobile food unit on the property in the future, it is understood that they would need to submit a Site Plan that provides details of the mobile food unit(s) and that additional Planning review would likely be needed.

11. Self-service storage. Self-service storage uses are required to have a minimum lot size of five acres. The expansion of existing self-storage facilities must meet the minimum acreage requirement. All areas within 30 feet of storage unit access points or doors shall be paved with an asphalt surface.

Applicant Response: The property is not at least 5 acres in size, a self-storage use is not proposed.

- C. Conditional uses (CU). Uses which are allowed if approved through the conditional use review process are listed in Table 15.24-1 with a "CU." These uses are allowed provided they comply with the conditional use requirements of chapter 15.316, conditional uses. Uses listed with a "CU" that also have a footnote number in the table are subject to the regulations cited in the footnote.
- D. *Prohibited uses (N).* Uses listed in Table 15.24-1 with an "N" are prohibited. Existing uses in categories listed as prohibited may be subject to the regulations of chapter 15.08, non-conforming uses and structures.

Applicant Response: This Site Plan application supports the La Pine Small Business Incubator Facility. Users and/or uses have not been identified as this time. The proposal is for a spec building and it is intended to allow for flexibility for future users. The application requests approval of Permitted and Limited uses, but does not include a Conditional Use Permit and it does not include any prohibited uses.

| Table 15.24-1. Use Regulations in the Industrial and Public Facility Zones | | | | | | | | |
|--|---------|-----------|-----------|---|--|--|--|--|
| Use Category LI I PF Special Use Standards | | | | | | | | |
| Residential Use Categories - None Permitted | | | | | | | | |
| | Commerc | ial Use (| Categorie | s | | | | |

| Table 15.24-1. Use Regulations in the Industrial and Public Facility Zones | | | | | | | |
|--|-------------|----------|-------------|--|--|--|--|
| Use Category | LI | I | PF | Special Use Standards | | | |
| Campgrounds and recreational vehicle parks | N | N | N | _ | | | |
| Commercial lodging | N | N | N | _ | | | |
| Commercial parking | N | N | N | _ | | | |
| Commercial recreation | N | N | N | _ | | | |
| Eating and drinking establishments | L (1) | L (10) | N L (10) | Mobile food unit sites subject to <u>Section</u> <u>15.108.070</u> . | | | |
| Marijuana dispensary | N | N | N | _ | | | |
| Quick vehicle servicing | Р | N | N | _ | | | |
| Office | L (2) | L (2) | L (2) | _ | | | |
| Retail sales and service | L (3) | N | N | _ | | | |
| Vehicle repair | Р | N | N | _ | | | |
| Self-service storage | CU(11) | P(11) | P(11) | _ | | | |
| li | ndustrial l | Jse Cate | gories | | | | |
| Artisanal and light manufacturing | Р | Р | Р | Section 15.108.010 | | | |
| Automotive wrecking, salvage, and junk yards | L (4) | N | L/CU (4) | Section 15.108.040 | | | |
| Industrial service | Р | N | N | _ | | | |
| General manufacturing and | L/CU | Р | Р | | | | |

| Table 15.24-1. Use Regulation | ns in the l | Industria | al and Pu | blic Facility Zones |
|---|---------------------|-------------------|------------------------|-----------------------|
| Use Category | LI | 1 | PF | Special Use Standards |
| Marijuana facilities | N | L/CU (9) | N | Section 15.108.050 |
| Wholesale sales | Р | Р | Р | _ |
| Warehouse and freight movement | L/CU (6) | P | P | _ |
| Waste treatment and recycling | N | N | CU | Section 15.108.100 |
| Ins | stitutional | Use Ca | tegories | |
| Basic utilities | Р | Р | Р | _ |
| Colleges | N | N | N | _ |
| | | | | |
| Community services | L (7) | Р | N | _ |
| Community services Daycare centers | L (7) | P N | N N | _ |
| | | | | |
| Daycare centers | Р | N | N | |
| Daycare centers Medical centers | P N | N N | N N | |
| Daycare centers Medical centers Parks and open areas | P N CU | N N CU | N N CU | |
| Daycare centers Medical centers Parks and open areas Religious institutions | P N CU N | N N CU N | N N CU N | |
| Daycare centers Medical centers Parks and open areas Religious institutions | P N CU N | N N CU N | N N CU N | |
| Daycare centers Medical centers Parks and open areas Religious institutions Schools | P N CU N N Other Us | N CU N N e Catego | N N CU N N | |

a Pine – Small Business Incubator Facility age 10 of 65

| Table 15.24-1. Use Regula | ations in th | e Indust | rial and P | Public Facility Zones |
|---|--------------|----------|------------|-----------------------|
| Use Category | LI | 1 | PF | Special Use Standards |
| Wireless telecommunication facilities | Р | P | Р | Section 15.108.110 |

Applicant Response: The proposal is for a 4-suite, flex-space building that is being developed as the La Pine Small Business Incubator Facility. This Site Plan is being submitted to allow the greatest amount of flexibility, without including a Conditional Use Permit. Potential Uses (without requiring additional review) are highlighted above. All of the noted uses are permitted outright or with the "Limited" designation. The proposal conforms to the use requirements of this section.

15.24.400. - Development standards.

A. Purpose. The development standards for industrial and public facility zones allow development flexibility, within parameters, that supports the intended characteristics of the specific zone. In addition, the regulations provide guidance to property owners, developers, and neighbors about the limits of what is allowed.

Applicant Response: This section provides a purpose, but does not contain any measurable development standards or approval criteria. It is understood that a project that conforms to the standards of this section will carry out the purpose noted herein.

B. Development standards. The development standards for industrial and public facility zones are presented in Table 15.24-2. Development standards may be modified as provided by <u>chapter 15.320</u>, variances. Additional standards may apply to specific zones or uses, see <u>section 15.24.500</u>.

| Table 15.24-2. Development Standards in the Industrial and Public Facility Zones | | | |
|--|---------|------|------|
| Standard | LI | I | PF |
| Minimum setbacks | _ | _ | _ |
| - Front or street-side yard | 20 feet | None | None |

| Table 15.24-2. Development Standards in the Industrial and Public Facility Zones | | | |
|--|--|---------|---------|
| Standard | LI | I | PF |
| - Side yard | 10 feet | None | None |
| - Rear yard | 10 feet | None | None |
| Maximum building height | 45 feet | 75 feet | 75 feet |
| Maximum lot coverage | 60% | 80% | 80% |
| Minimum landscaped area | See 15.24.500.A and <u>chapter 15.82</u> | | |

Applicant Response: As detailed in the table, the building site has no minimum or maximum setback requirements. The I zone has a maximum height allowance of 75 feet and the proposed building is less than 25 feet, which is well less than the maximum allowed 75 feet; the lot coverage is proposed to be 24.5%, which is well less than the allowed 80%; and the 30.9% landscaping coverage is well in excess of the minimum required 10%. A further review of 15.24.500.A and chapter 15.82 is included below. The proposed design conforms to the standards of this table.

Sec. 15.24.500. - Additional standards.

A. Landscaping standard. Any portion of a lot developed for industrial uses which are not used for buildings, other structures, parking or loading spaces, or aisles, driveways, sidewalks, and designated storage areas shall be planted and maintained with grass or other all-season groundcover vegetation. Grass shall be kept neatly mowed. Landscaping with trees and shrubs is permitted and encouraged. See additional landscaping standards in article.5.

Applicant Response: As detailed on the Site Plan, the site is proposed to be fully developed, including the buildings, walkways, parking areas, and/or landscaping. No area are proposed to be retained in an undeveloped state and/or being kept in a state that is otherwise prohibited by this section. The proposed design conforms to the standards of this section.

B. Screening requirements. All accessory storage of junk, waste, discarded or salvaged material, machinery, or equipment shall not be permitted except within a completely enclosed structure. Or if the lot area devoted to such use is over 200 square feet in area, the owner may have the

alternative of enclosing it on all sides, except for an exit and entrance not over 25 feet in width, by a solid fence or wall at least six feet in height and maintained in good condition or by a cyclone or equal-wire fence at least six feet in height and surrounded, except for an exit and entrance not over 25 feet in width, by evergreens at least six feet in height and planted not further apart than six feet so as to form a solid screen. See also chapter 15.82 for additional screening requirements. See additional buffering and fence standards in article 5.

Applicant Response: The proposal does not include the accessory storage of junk, waste discarded or salvaged material, machinery or equipment; therefore the design and proposal conforms to the standards of this section. It is understood/expected that this requirements may be perpetually ensured through the imposition of a condition of approval.

D. [C.] Vehicle access. Access driveways and entrances shall be permitted in a number and locations in which sight distance is adequate to allow safe movement of traffic in or out of the driveway or entrance, the free movement of normal highway traffic is not impaired, and the driveway or entrance will not create a hazard or an area of undue traffic congestion on highways to which it has access. The city may require the permit applicant to submit engineering data and/or traffic analyses to support its proposed plan of access driveways and entrances. See additional access and circulation standards in article 5.

Applicant Response: The proposal includes a single access point that will be located on the south side of the property, onto Assembly Way, which is the lowest classification street abutting the property. The access point is located and oriented to allow efficient development on the site. Neighboring properties are undeveloped and the proposed location does not conflict with any nearby streets or driveways. The proposal was coordinated with the City and Deschutes County Road Department and conforms to the requirements of this section.

E. [D.] *Emissions*. Industrial uses shall comply with all applicable pollution control regulations enacted by the federal and state government and other governmental authorities.

Applicant Response: The proposal is for the La Pine Small Business Incubator Facility. Final users are not known at this time. In the event that an industrial user occupies any suite of the facility, it is understood that they would need to comply with this standard. It is understood and expected that a condition of approval may be added to ensure on-going conformance with this standard.

F. [E.]. *Noise.* Industrial uses shall provide necessary shielding or other protective measures against interference caused by mechanical and nuclear equipment, or uses or processes with electrical apparatus, to nearby residences.

Applicant Response: There are no residences nearby the site; therefore this standard does not apply.

G. [F.] *Lighting*. All exterior lighting shall be so placed and shielded so as not to create a nuisance for adjacent properties.

Applicant Response: The applicant plans to install exterior lighting. All lighting is proposed to be full-cut off and will be placed such that it does not negatively impact any adjacent properties. Final lighting details are planned to be provided with the Building Permit submittal.

B. Article 5 – Development Standards

Chapter 15.80 - Development Standards, Generally

15.80.010 Purpose

Article 5 contains development and design standards for the built environment. The standards are intended to protect the public health, safety, and welfare through the provision of landscaping and buffering, parking and loading facilities, multimodal accessibility and interconnectivity, and adequate public facilities.

In interpreting and applying this title, the provisions herein shall be held to be the minimum requirements adopted for the promotion of the public health, safety, comfort, convenience, and general welfare.

Applicant Response: This section establishes the purpose of these General Development Standards. This section does not include any approval criteria or development standards to measure compliance with. Compliance with the standards of this section will ensure that the Purpose stated herein is implemented.

15.80.020 Applicability

Any land division or development, and the improvements required therefore, shall be in compliance with the development, design and improvement standards and requirements set forth in this Article. Other provisions of this Code, other city ordinances, or state statutes or administrative rules may also apply.

Applicant Response: The proposal includes new development; therefore this section applies.

15.80.030 Exemption - Lot Size Requirements...
15.80.040 Exemption - Yard or Setback Requirements...

Applicant Response: The proposal does not request an exception to lot size or setbacks. Therefore these sections do not apply.

Sec. 15.80.050. - Supplementary height regulations.

The maximum height limitations shall not apply to:...

Applicant Response: The proposed building height is well below the 75 foot height limit of the I Zone; the supplementary height regulations of this section are not applicable to the current proposal.

15.80.060 Restrictions on the Use of Metal Shipping Containers...

Applicant Response: The design does not include metal shipping containers. This section does not apply.

Chapter 15.82 - Landscaping, Buffering and Fences

15.82.010 Landscaping and Buffering Requirements

The following minimum landscape requirements are established for all developments subject to site plan approval, unless approved otherwise by the reviewing authority.

A. Exemption....

Applicant Response: No exceptions to this section are being requested.

- B. Area required. Except as approved otherwise by the City, the following minimum percent of a parcel area shall be landscaped for the following uses:...
 - 3. Commercial uses including mixed use commercial (CMX): 15 percent.
 - 4. Industrial uses. A minimum five-foot landscaped buffer along any adjoining public right-of-way of a collector or arterial street or highway, which may be computed toward an overall requirement of ten percent.
 - 5. Minimum area requirements may include landscaping around buildings, in parking and loading areas, outdoor recreational use areas, screening and buffering areas, and surface water drainage areas.

Applicant Response: As noted throughout this application, the proposal is for the La Pine Small Business Incubator Facility. Final uses / users have not been identified. As detailed on the Plan Set, the design provides 7,686 square feet of landscaping on the 24,899 square foot site, which equates to 30.9% of the site. The amount of landscaping that is proposed exceeds the required minimum; therefore the proposal conforms to these standards.

C. Landscaping defined. Required landscaping may include, but is not limited to, a combination of any of the following materials: living plant material such as trees, shrubs, groundcover, flowers and lawn (including native vegetation); and nonliving materials such as benches, walkways and courtyards, consisting of brick, decorative rock or other decorative materials. The total amount of nonliving materials (including bark dust, chips, aggregate, or other non-plant ground covers) shall not exceed more than 50 percent of the required landscape area.

Applicant Response: The Applicant proposes landscaping to be irrigated and to include living plant material such as trees, shrubs, groundcover, flowers and grasses (including native vegetation). The amount of non-plant ground cover is not proposed to exceed 50% of the landscaping; therefore the proposal conforms to these standards. The applicant proposes to provide the City with a final landscape plan prior to installation and to install all landscaping prior to initiation of use in the new area.

D. Existing vegetation. Existing site vegetation may be utilized to the maximum extent possible consistent with building placement and the applicable proposed landscape plan.

Applicant Response: As detailed on the Plan Set, in order to accommodate the planned development, the trees on the site will need to be removed.

E. Parking lots. Parking lots with space for ten or more vehicles must be landscaped in accordance with the following minimum requirements...

Applicant Response: As detailed on the Plan Set, the design includes parking stalls for 16 spaces on the west side of the site/building; therefore the standards of this section apply.

- 1. In commercial and residential developments, parking areas shall be divided into bays, and between or at the end of each parking bay a curbed planter containing at least 16 square feet may be required.
- 2. If required, each planter shall contain at least one tree or shrub and ground cover.
- 3. The areas shall be designed to be protected from being damaged by vehicles using the parking area.

Applicant Response: As detailed on the Landscape Plan, the parking area will be curbed and surrounded by landscaping. The proposed design can and will prevent the landscaped areas from being damaged and conform to the requirements of this section.

4. Unless sidewalks are provided adjacent to a structure, customer or resident parking areas should be separated from the exterior wall of

a commercial or residential structure by a minimum five-foot strip of landscaping.

Applicant Response: As detailed on the Plan Set, a walkway separates the parking spaces from the building; therefore a 5-foot landscape strip is not needed.

5. Where a parking, loading or driveway area serving a multi-family, commercial, industrial or government use abuts a public right-of-way of a collector or arterial street or a local street across from a residential zone, or abuts a residential zone, a screen planting or other approved landscaped planter strip may be required between the parking area and the right-of-way without encroaching into a clear vision area or sidewalk.

Applicant Response: Reed Road, to the north of the site is identified as an Industrial Collector and requires screening in accordance with the standards of this section. As detailed on the Landscape Plan, the parking area is separated by well over 20 feet of landscaping in accordance with the standards of this section.

F. Buffering and screening.

- 1. Purpose. The purpose of buffering and screening requirements are to reduce the impacts of a proposed use on adjacent uses and zones which provide for different types of uses. The City may waive or reduce the requirements where existing topography or vegetation is appropriate or otherwise negates the effectiveness or intended purpose or benefits of the buffering and screening.
- 2. Where any permitted principal and/or accessory use in a Commercial or Industrial zone abuts any land zoned RSF, RMF, RMP, or TA the following buffer and screening shall be required. These requirements shall apply in instances where such use is being newly developed on vacant land, expanded in floor area by 50% or greater, or removed and a new use developed.
- 3. Within Commercial Zones...

Applicant Response: The property is not within a Commercial Zone; this section does not apply.

4. Within Industrial Zones. A buffer strip at least 30 feet wide shall be provided and maintained along the entire length of a side or rear yard where it abuts any RSF, RMF, RMP, or TA zoned land. Buffer strips shall not be used for parking, storage of vehicles, equipment, or materials, nor for any other use incompatible with their purpose as a visual, noise, dust, and pollution barrier. The buffer shall meet the following standards:

Applicant Response: The property does not abut a RSF, RMF, RMP or TA Zone; therefore additional buffering or screening is not required.

5. A buffer or screening area may only be occupied by screening utilities and landscaping materials, but the same may be located within the required yard or setback requirements provided vision clearance requirements are complied with.

Applicant Response: Buffering or screening areas are not proposed or required; therefore this section does not apply.

6. In lieu of the foregoing requirements, an applicant may provide for landscaping and screening, including plantings, fences, walls, walks and other features designed to afford the same degree of buffering as the standards above. A plan and specifications for an alternative shall be reviewed and approved by the review authority.

Applicant Response: The design provides for all required landscaping, buffering and screening; an alternative is not needed.

- G. Plant material installation standards. Except as otherwise approved by the city, the following standards shall apply to plant materials and the installation thereof as provided in accordance with the provisions of this section.
 - 1. Landscape plant materials shall be properly guyed and staked, and shall not interfere with vehicular or pedestrian traffic or parking and loading.
 - 2. Trees shall be a minimum size of six feet in height and be fully branched at the time of planting. 5-6
 - 3. Shrubs shall be supplied in one-gallon containers or six-inch burlap balls with a minimum spread of 12 inches.
 - 4. Rows of plants should be staggered to provide for more effective coverage.

Applicant Response: The applicant plans to abide by the installation provisions of this section, which are specified on the Landscape Plan.

H. Maintenance and plant survival. All landscaping approved or required as a part of a development plan shall be continuously maintained, including necessary watering, weeding, pruning and replacement of plant materials. Except where the applicant proposes landscaping consisting of drought- resistant plantings and materials that can be maintained and can survive without irrigation, landscaped areas shall be irrigated. If plantings fail to survive, it is the responsibility of the property owner to replace them.

Applicant Response: The applicant plans to continuously maintain the landscaping. In the event that any plants fail to survive, the applicant will replace them.

I. Retention of trees. As part of a landscaping plan, the developer shall also provide a tree plan identifying the location and diameter breast height (DBH) of all trees on the property. Existing mature trees with at least a 10-inch DBH shall be retained to the furthest extent possible. Development shall conform to the natural environment to incorporate existing mature trees. Where trees must be removed for proposed development, they shall be replaced at a 1:1 ratio on the subject lot or parcel with trees of similar species that are native to the region, and with a DBH of at least five inches or larger. Replacement trees shall meet the maintenance and plant survival criteria of this code section. Any trees planted must maintain a 20-foot radius for spacing. The 20-foot radius spacing shall be the determining factor in how many trees are replaced on the subject property. If more trees must be planted than what the 20-foot radius spacing would allow, then the number of trees replaced shall be reduced to meet the spacing standard.

Applicant Response: As detailed on the Plan Set, the site includes 5 trees that are 10" DBH or greater. To accommodate the La Pine Small Business Incubator Facility, all of the trees on the site need to be removed. The 5 trees that are 10" DBH or greater are proposed to be replaced with 5 trees; which conforms to the requirements of this standard.

15.82.020 Fences and Walls...

Applicant Response: No fences or walls are proposed. The standards of this section do not apply.

Chapter 15.86 – Parking and Loading

15.86.0101 Applicability

Off-street loading and vehicle and bicycle parking spaces shall be provided in accordance with the specifications of this Chapter in all zones whenever any new use is established, an existing use is enlarged, or an existing use of land or structure is changed to a new use. Such new, enlarged, or changed use shall fully comply with the specifications of this Chapter prior to being given a certificate of use and occupancy.

Applicant Response: The proposal includes new development; therefore this Chapter applies.

15.86.020 Off-Street Loading

A. Every commercial and industrial use which requires the receipt or distribution of material or merchandise by trucks with a 40-foot or longer wheelbase at a frequency of one or more vehicles per week shall provide off- street loading spaces in sufficient number to adequately serve the number and frequency of vehicle shipping and receiving projected for the use. The applicant shall

provide supporting evidence of the projected shipping and receiving and how the number of spaces to be provided will be adequate.

Applicant Response: Users that will need receive or distribute material or merchandise by trucks with a 40-foot or longer base are not anticipated. This section does not apply.

B. Where an off-street loading space is required, it shall be large enough to accommodate the largest vehicle that is expected to serve the use without obstructing vehicles or pedestrian traffic on adjacent streets and driveways. Each off-street loading space shall not be less than 12 feet wide by 55 feet long unless otherwise approved by the City through Site Design Review.

Applicant Response: As noted above, users that will need to receive or distribute material or merchandise by trucks with a 40-foot or longer base are not anticipated; therefore an off-street loading space is not required.

C. Off-street loading space(s) shall also have adequate adjacent area for vehicle maneuvering so that vehicles using the space(s) are not required to back-up onto or back-up from a public street or alley to use the space. Where parking areas are prohibited between a building and the street, loading areas are also prohibited.

Applicant Response: As noted above, users that will need receive or distribute material or merchandise by trucks with a 40-foot or longer base are not anticipated; therefore an off-street loading space is not required and this associated standard does not apply.

D. Exceptions and Adjustments. The City, through Site Design Review, may approve a loading area adjacent to or within a street right-of-way where it finds that loading and unloading operations are short in duration (i.e., less than one hour), infrequent, do not obstruct traffic during peak traffic hours, do not interfere with emergency response services, and are acceptable to the applicable roadway authority.

Applicant Response: An off-street loading space is neither required nor proposed. Furthermore, an exception is not being requested at this time, however if a future user needs a loading space; this exception could be employed / utilized.

15.86.030 Off-Street Parking – Required

A. Location of off-street loading and parking spaces. Except as otherwise permitted by this Code, required off- street loading and parking spaces shall be located on the same lot with the principal use they are intended to serve. In no case shall a required loading space be part of the area used to satisfy the parking requirements and vice versa. Also, in no case shall the required

loading or parking space(s) of one use be used to satisfy the loading or parking space requirements of another use.

Applicant Response: As shown on the Site Plan, all new required parking spaces for the use will be on the property and will not encroach upon any loading area. The design conforms to this standard.

B. Encroachment or reduction. A required loading or parking space shall not be encroached upon by a structure, storage, or other use, nor shall the number of spaces be reduced without replacement of a commensurate number of spaces in accordance with this section unless a special exception or variance has been approved.

Applicant Response: As documented on the Plan Set, parking spaces are proposed solely for that use and will not be encroached upon for another purpose. As designed, the proposal conforms to this standard.

- C. Calculations of Amounts of Required and Allowed Parking.
 - 1. When computing parking spaces based on floor area, parking structures and non-leasable floor spaces, such as storage closets, mechanical equipment rooms, and similar spaces, are not counted.
 - 2. The number of parking spaces is computed based on the primary uses on the site except as stated in subsection 3, below. When there are two or more separate primary uses on a site, the minimum and maximum parking for the site is the sum of the required or allowed parking for the individual primary uses. For shared parking, see subsection (I) below.
 - 3. When more than 20 percent of the floor area on a site is in an accessory use, the required or allowed parking is calculated separately for the accessory use. An example would be a 10,000 square foot building with a 7,000 square foot warehouse and a 3,000 square foot accessory retail area. The minimum and maximum parking would be computed separately for the retail and warehouse uses.

Applicant Response: This section provides methods for calculation, but no specific standards. The methods identified in this section have been utilized in the preparation of this application.

D. Use of Required Parking Spaces. Except as otherwise provided by this section, required parking spaces must be available for residents, customers, or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for shared parking pursuant to subsection (I).

Applicant Response: Parking spaces are planned for the proposed development, consistent with the requirements of this section.

E. Improvement of Parking Areas. Motorized vehicle parking is allowed only on streets with an improved shoulder of sufficient width; within garages, carports, and other approved structures; and on driveways or parking lots that have been developed in conformance with this Code.

Applicant Response: The proposed parking spaces are planned within a parking lot. The parking lot and spaces conform to the requirements of the Code and thus with this provision.

- F. Minimum Number of Off-Street Automobile Parking Spaces. Except as required for Americans with Disabilities Act compliance under subsection (L), off-street parking shall be provided pursuant to one of the following three standards:
 - 1. The standards in Table 15.86-1;
 - 2. A standard from Table 15.86-1 for a use that the Planning Official determines is similar to the proposed use. For uses not specified in the table, the City shall determine parking based on submission of technical data from applicant or City sources; or
 - 3. Subsection (H), Parking Exceptions, which includes a Parking Demand Analysis option...

| Table 15.86-1. Automobile Parking Space | ces by Use |
|--|--|
| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) |
| Residen | tial Categories |
| Single-family dwelling, including manufactured dwellings on lots or in parks | One space per dwelling |
| Duplex | Two spaces per duplex (one space per dwelling unit) |

| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) |
|--|---|
| Accessory dwelling (second dwelling on a single-family lot) | Two spaces total for primary dwelling and accessory dwelling |
| Multi-family | One space per dwelling unit |
| Group living, such as nursing or convalescent homes, rest homes, assisted living, congregate care, and similar special needs housing | 0.5 space per four bedrooms |
| Commerci | ial Categories |
| Commercial outdoor recreation | One space for each three persons maximum occupancy; or per conditional use permit review whichever is less |
| Bed and breakfast inn | One space per use, plus 0.5 space(s) for each bedroom offered as lodging |
| Educational services, not a school (e.g., tutoring or similar services) | One space per 300 sq. ft. floor area |
| Entertainment, major event | One space for each three persons maximum occupancy; or per conditional use permit review whichever is less |
| Hotels, motels, and similar uses | 0.75 space per guest room. See also, parking requirements for associated uses such as restaurants, entertainment uses, drinking establishments, assembly facilities |
| Mortuary or funeral home | One space per 300 sq. ft. floor area |
| Offi ces | General office: one space per 500 sq. ft. floor area |

a Pine – Small Business Incubator Facility

| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) | | | |
|--|--|--|--|--|
| | Medical or dental office: one space per 500 sq. ft. floor area | | | |
| Outdoor recreation, commercial | Per conditional use permit review | | | |
| Surface parking lot, when not accessory to a permitted use | Per conditional use permit review | | | |
| Quick vehicle servicing or vehicle repair | Two spaces, excluding vehicle service or queuing area, or per conditional use permit review | | | |
| | Bank: one space per 300 sq. ft. floor area | | | |
| | Retail: one space per 400 sq. ft. floor area except one space per 1,000 sq. ft. for bulk retail (e.g., auto sales, nurseries, lumber and construction materials, furniture, appliances, and similar sales) | | | |
| Retail sales and commercial service | Restaurants and bars: one space per 200 sq. ft. floor area | | | |
| | Health clubs, gyms, continuous entertainment (e.g., roller rinks): one space per 500 sq. ft. floor area | | | |
| | Bowling alleys: five spaces for each lane | | | |
| | Theaters and cinemas: one space per six seats | | | |
| | Trailer and monument sales: one space per 2,500 sq. ft. of gross area | | | |
| Self-service storage | Two spaces, plus adequate space for loading and unloading | | | |

| Table 15.86-1. Automobile Parking Spaces by Use | | | | | | |
|---|---|--|--|--|--|--|
| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) | | | | | |
| Industrial Categories ¹ | | | | | | |
| Industrial service | One space per employee on the largest shift plus one space for each 10,000 sq. ft. for visitors up to ten additional spaces | | | | | |
| Manufacturing and production | One space per 1,000 sq. ft. of floor area; or as required by conditional use permit review | | | | | |
| Warehouse and freight movement | 0.5 space per 1,000 sq. ft. of floor area; or as required by conditional use permit review | | | | | |
| Waste-related | Per conditional use permit review | | | | | |
| Wholesale sales, e.g., building materials, heavy equipment, agricultural supplies, etc. | One space per 1,000 sq. ft. | | | | | |
| Marijuana wholesaler/production facility/processing facility/testing laboratories | Four plus one additional space per 2,000 sq. ft. gross floor area | | | | | |
| Institutional Categories | | | | | | |
| Basic utilities | Parking based on applicant's projected parking demand, subject to city approval | | | | | |
| Community service, including government offices and services | Parking based on applicant's projected parking demand, subject to city approval, except as specifically required elsewhere in this table for individual uses (See public assembly, office, retail, housing, etc.) | | | | | |
| Daycare | Family daycare: 1 space, plus required parking for dwelling | | | | | |

| Table 15.86-1. Automobile Parking Space | es by Use | | | |
|--|---|--|--|--|
| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) | | | |
| | Daycare center: 1 space per 400 sq. ft. of floor area | | | |
| Medical center or hospital | One space per 300 sq. ft. floor area or one for each bed, whichever is less | | | |
| Parks and open space | Parking based on projected parking demand for planned uses | | | |
| Public assembly | One space per 75 sq. ft. of public assembly area; or as required by conditional use permit | | | |
| Religious institutions and houses of worship | One space for each four members but not less than 15% of total occupancy permitted by fire marshal | | | |
| | Pre-school through middle-school: one space per classroom | | | |
| | High school/college: 1.5 spaces per classroom, plus 1 space per 10 students | | | |
| Schools | If the school is designed to accommodate related uses such as auditoriums, stadiums, theatres, and gymnasiums, additional parking shall be provided at a rate of 1 space per 4 seats. In the alternative, the city may accept a parking management plan to determine required parking | | | |
| Other Categories ² | | | | |
| Accessory uses | Parking standards for accessory uses are the same as for primary uses, but are pro rated based on the percentage of | | | |

| Table 15.86-1. Automobile Parking Spaces by Use | | | | |
|--|--|--|--|--|
| Use Categories | Minimum Parking per Land Use (Fractions are rounded down to the closest whole number.) | | | |
| | estimated overall parking demand, subject to city review and approval | | | |
| Agriculture | None, except as required for accessory uses | | | |
| Radio frequency transmission facilities | None, except as required by conditional use permit | | | |
| Temporary uses | Parking standards for temporary uses are the same as for primary uses, except that the city may reduce or waive certain development and designs standards for temporary uses | | | |
| Transportation facilities (operation, maintenance, preservation, and construction) | None, except for park-and-ride facilities; and where temporary parking is required for construction staging areas | | | |

Applicant Response: As noted throughout this narrative, this Site Plan application is for the La Pine Small Business Incubator Facility. Final users and uses are not known at this time. Of the potential uses, which are identified in this table, office has the greatest parking requirement. If the entire 6,090 square foot building were developed with office uses, then 12.18 parking spaces would be required. As detailed on the Plan Set, 16 parking spaces are proposed, which exceed the required minimum under the worst case scenario. The proposed amount of parking is adequate for the site and conforms to the requirements of this section.

- G. Maximum Number of Off-Street Automobile Parking Spaces. The following standards for maximum number of automobile parking spaces promote efficient use of land and compact development patterns.
 - 1. Applicability. Developments subject to Site Plan Review must conform to the maximum parking standards.
 - 2. Standards. Unless otherwise approved by the City through Site Plan Review, the maximum number of off-street automobile parking spaces allowed per for a commercial development equals the minimum number of

required spaces, pursuant to Table 15.86-1 times a factor of 2.0. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements.

Applicant Response: As noted throughout this narrative, this Site Plan application is for the La Pine Small Business Incubator Facility. Final users and uses are not known. Of the potential uses, which are identified in this table, office has the greatest parking requirement. If the entire 6,090 square foot building were developed with office uses, then 12.18 parking spaces would be required, which would allow for up to 24.36 parking spaces. As detailed on the Plan Set, 16 parking spaces are proposed, which is less than the allowed maximum. The proposed amount of parking is adequate for the site and conforms to the requirements of this section.

H. Exceptions and Reductions to Off-Street Parking...

Applicant Response: The proposal does not include any exceptions to the parking standards; therefore these sections do not apply.

I. Shared parking. Required parking facilities for two or more uses, structures, or parcels of land may be satisfied by the same parking facilities used jointly, to the extent that the owners or operators show that the need for parking facilities does not materially overlap (e.g., uses primarily of a daytime versus nighttime nature; weekday uses versus weekend uses), and, provided that the right of joint use is evidenced by a recorded deed, lease, contract, or similar written instrument establishing the joint use. Shared parking requests shall be subject to review and approval through site plan review.

Applicant Response: The shared parking standards of this section addresses the joint uses of parking spaces by uses or buildings that would utilize a space (or spaces) at differing times. Shared parking as contemplated by this section is not needed to meet a minimum standard and shared parking is not proposed with this application. This section does not apply.

J. Parking Stall Design and Minimum Dimensions. Where a new off-street parking area is proposed, or an existing off-street parking area is proposed for expansion, the entire parking area shall be improved in conformance with this Code. At a minimum the parking spaces and drive aisles shall be paved with asphalt, concrete, or other City-approved materials, provided the Americans with Disabilities Act requirements are met, and shall conform to the minimum dimensions in Table 15-86-2 and the figures below. All off-street parking areas shall contain wheel stops, perimeter curbing, bollards, or other edging as required to prevent vehicles from damaging buildings or encroaching into walkways, landscapes, or the public right-of-way. Parking areas shall also provide for surface water management.

| 90° | 9'-0" | 20.0 | 24.0 | 9.0 | 64.0 |
|-----|--------|------|------|------|------|
| | 9'-6" | 20.0 | 24.0 | 9.5 | 64.0 |
| | 10'-0" | 20.0 | 24.0 | 10.0 | 64.0 |

Applicant Response: As documented on the Plan Set, all stalls are proposed to be 90 degree stalls and be 10' x 20'. Furthermore, the drive aisles are proposed to be at least 24 feet in width. Also, the submittal is accompanied by a grading and drainage plan. As proposed, the design conforms to the standards of this section.

K. Adjustments to Parking Area Dimensions....

Applicant Response: The applicant is not proposing any adjustments to parking standards; therefore this section does not apply.

L. Americans with Disabilities Act (ADA). Parking shall be provided consistent with ADA requirements, including, but not limited to, the minimum number of spaces for automobiles, van-accessible spaces, location of spaces relative to building entrances, accessible routes between parking areas and building entrances, identification signs, lighting, and other design and construction requirements.

Applicant Response: The planned improvements necessitate 2 ADA parking spaces, which are identified on the Plan Set. As proposed, the design conforms to the requirements of this section.

15.86.050 Bicycle Parking

A. Exemptions...

Applicant Response: The proposal does not include any exceptions to the bike parking standards; therefore this section does not apply.

B. Standards. Bicycle parking spaces shall be provided with new development and, where a change of use occurs, at a minimum, shall follow the standards in Table 15.86-3. Where an application is subject to Conditional Use Permit approval or the applicant has requested a reduction to an automobile-parking standard, the City may require bicycle parking spaces in addition to those in Table 15.86-3.

Table 15.86 -3 Minimum Required Bicycle Parking Spaces

| Use | Minimum Number of Spaces |
|--------------|--|
| Commercial | 2 bike spaces per primary use or 1 per 5 vehicle spaces, whichever is greater |
| IIndiietriai | 2 bike spaces per primary use or 1 per 10 vehicle spaces, whichever is greater |

Applicant Response: As noted throughout this narrative, this Site Plan application is for the La Pine Small Business Incubator Facility. Final users and uses are not known at this time. Of the potential uses, which are identified in this table, commercial uses have the greatest bike parking requirement. If the entire 6,090 square foot building were developed with commercial uses, then 3.6 bicycle parking spaces would be required. As detailed on the Plan Set, 2 racks (4 spaces) are proposed, which exceeds the required minimum and conforms to the requirements of this section.

C. Design. Bicycle parking shall consist of staple-design steel racks or other Cityapproved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle. At a minimum, bicycle parking facilities shall be consistent with the following design guidelines:

- 1. All bicycle parking shall be within one hundred (100) feet from a building entrance and located within a well-lit and clearly visible area;
- 2. Bicycle parking shall be convenient and easy to find. Where necessary, a sign shall be used to direct users to the parking facility;
- 3. Each bicycle parking space shall be at least two (2) feet by six (6) feet with a vertical clearance of six (6) feet;
- 4. An access aisle of at least five (5) feet shall be provided in each bicycle parking facility;
- 5. Bicycle parking facilities shall offer security in the form of either a lockable enclosure in which the bicycle can be stored or a stationary object, i.e., a "rack," upon which the bicycle can be locked. Structures that require a user-supplied lock shall accommodate both cables and U-shaped locks and shall permit the frame and both wheels to be secured (removing the front wheel may be necessary.) Note: businesses may provide long-term, employee parking by allowing access to a secure room within a building.

D. Hazards. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall be located so as to not conflict with the vision clearance standards of Section 15.88.040.

Applicant Response: As shown on the Plan Set, the bike parking spaces are planned on the south side of the building, in U racks. Furthermore the bicycle parking areas will be separated from other use area and they will be secure. The applicant is proposing to locate and install the racks prior to initiating the use. As designed, the proposal

complies with these standards.

15.86.060 Snow Storage Areas

- A. Purpose. The purpose of these standards is to ensure that adequate space is be provided within a development for storage of snow in winter months in order to accommodate space needed for access, circulation, and off-street parking.
- B. Applicability. Snow storage standards apply to all subdivisions and to developments subject to Site Plan review.

Applicant Response: The proposal is for Site Plan Review; therefore this section applies.

C. Standards.

- 1. Minimum Area. Snow storage areas must be designated on a site plan. The areas must total a minimum of 15% of the area to be cleared, including all access drives, parking areas, and walkways.
- 2. Location. Snow storage is not permitted on landscaped areas, except where these areas are limited to grass or rock cover. Snow storage may be permitted in parking areas provided that the site can still accommodate enough parking spaces to meet minimum off-street parking requirements in winter months. Parking spaces that are located in snow storage areas do not count toward the maximum parking space requirements. It is encouraged that snow storage areas be located away from public view and that additional impervious surface areas are not created for the sole purpose of snow storage.
- 3. Exceptions and Adjustments. The City may reduce or eliminate the required snow storage areas if a snow removal plan is presented which provides a continuous guarantee of removal.

Applicant Response: As detailed on the Site Plan, the design includes 10,205 square feet of parking and maneuvering area, thereby requiring at least 1,530 square feet of snow storage area. The proposed design accommodates snow storage on the north side of the site (north of the parking area). As detailed on the Plan Set, the proposed design conforms to the requirements of this section.

Chapter 15.88 - Access and Circulation

15.88.010 Purpose

Chapter 15.88 contains standards for vehicular and pedestrian access, circulation, and connectivity. The standards promote safe, reasonably direct, and convenient options for walking and bicycling, while accommodating vehicle access to individual properties, as needed.

Applicant Response: This section establishes the purpose of these Development Standards. This section does not include any approval criteria or development standards to measure compliance with. Compliance with the standards of this section will ensure that the Purpose stated herein is implemented.

15.88.020 Applicability

Chapter 15.88 applies to new development and changes in land use necessitating a new or modified street or highway connection. Except where the standards of a roadway authority other than the City supersede City standards, Chapter 15.88 applies to all connections to a street or highway, and to driveways and walkways.

Applicant Response: The proposal will necessitate a new access point to the south, onto Assembly Way. The new access point will be developed in conformance with City Standards; therefore this section applies.

15.88.030 Vehicular Access and Circulation

A. Purpose and Intent. Section 15.88.030 implements the street access guidelines of the City of La Pine Transportation System Plan. It is intended to promote safe vehicle access and egress to properties, while maintaining traffic operations in conformance with adopted standards. "Safety," for the purposes of this chapter, extends to all modes of transportation.

Applicant Response: This is a purpose statement and it does not provide any measurable development standards or approval criteria.

B. Permit Required. Vehicular access to a public street (e.g., a new or modified driveway connection to a street or highway) requires an approach permit approved by the applicable roadway authority.

Applicant Response: Upon approval of this Site Plan, the applicant would pursue any needed Access Permit(s).

C. Traffic Study Requirements. The City, in reviewing a development proposal or other action requiring an approach permit, may require a traffic impact analysis, pursuant to Section 15.90.080, to determine compliance with this Code.

Applicant Response: The proposal is supported by a Trip Generation Memo that was prepared by HWA.

D. Approach and Driveway Development Standards. Access management restrictions and limitations consist of provisions managing the number of access points and/or providing traffic and facility improvements that are designed to maximize the intended function of a particular street, road or highway. The intent is to achieve a balanced, comprehensive program which

provides reasonable access as new development occurs while maintaining the safety and efficiency of traffic movement. Intersections, approaches and driveways shall conform to access spacing guidelines in the City of La Pine Transportation System Plan and the roadway authority's engineering standards. In the review of all new development, the reviewing authority shall consider the following techniques or considerations in providing for or restricting access to certain transportation facilities.

- 1. Access points to arterials and collectors may be restricted through the use of the following techniques:
 - a. Restricting spacing between access points based on the type of development and the speed along the serving collector or arterial.
 - b. Sharing of access points between adjacent properties and developments.
 - c. Providing access via a local order of street; for example, using a collector for access to an arterial, and using a local street for access to a collector.
 - d. Constructing frontage or marginal access roads to separate local traffic from through traffic.
 - e. Providing service drives to prevent overflow of vehicle queues onto adjoining roadways.

Applicant Response: Access is proposed onto Assembly Way (a local road) and not Reed Road (an Industrial Collector). Assembly Way carries relatively low volumes. Based upon the results of the traffic report and pre-submittal correspondence with the City and County Road Department, it is not anticipated that any of the methods of this section (which *may* be employed) will be utilized.

- 2. Consideration of the following traffic and facility improvements for access management.
 - a. Providing of acceleration, deceleration and right-turn-only lanes.
 - b. Offsetting driveways to produce T-intersections to minimize the number of conflict points between traffic using the driveways and through traffic.
 - c. Installation of median barriers to control conflicts associated with left turn movements.
 - d. Installing side barriers to the property along the serving arterial or collector to restrict access width to a minimum.

Applicant Response: The proposal will add a single, primary access point onto Assembly Way, in an area that does not have heavy traffic levels and the access point is proposed to be installed in conformance with City Standards. The design has been

studied by the project Engineer and has been determined to be safe and efficient. Based upon the result of the Traffic Report and pre-submittal correspondence with City Staff and the County Road Department, it is not anticipated that any restrictions will be imposed upon the planned access point.

- E. ODOT Approval. Where a new approach onto a state highway or a change of use adjacent to a state highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The City may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case the City will work cooperatively with the applicant and ODOT to avoid unnecessary delays.
- F. Other Agency Approval. Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.

Applicant Response: The proposal does not include a new access point onto an ODOT facility, however the Deschutes County Road Department has jurisdiction in this area. The Deschutes County Road Department attended the Pre-Application Meeting and supported the access onto Assembly Way at the meeting. It is expected that the Deschutes County Road Department will review this formal submittal and confirm their support.

G. Exceptions and Adjustments. The City may approve adjustments to the spacing standards of subsections above, where an existing connection to a City street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance.

Applicant Response: No exceptions or adjustments are being requested; therefore this section does not apply.

H. Joint Use Access Easement and Maintenance Agreement. Where the City approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use of and cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed, defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the City for its records, but the City is not responsible for maintaining the driveway or resolving any dispute between property owners.

Applicant Response: The design does not include a joint use driveway; therefore an easement and/or maintenance agreement is not needed.

15.88.040 Clear Vision Areas (Visibility at Intersections)

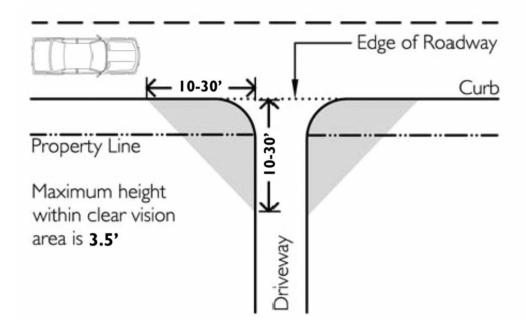
A. In all zones, a clear vision area shall be maintained on the corners of all

property at the intersection of two streets or a street and a railroad. A clear vision area shall contain no planting, wall, structure, private signage, or temporary or permanent obstruction exceeding three and one-half feet in height, measured from the top of the curb or, where no curb exists, from the established street centerline grade, except that trees exceeding this height may be located in this area provided all branches and foliage are removed to a height of eight feet above the grade.

- B. A clear vision area shall consist of a triangular area on the corner of a lot at the intersection of two streets or a street and a railroad (see Figure 18.88-1). Where lot lines have rounded corners, the specified distance is measured from a point determined by the extension of the lot lines to a point of intersection. The third side of the triangle is the line connecting the ends of the measured sections of the street lot lines. The following measurements shall establish clear vision areas within the City.
 - 1. In an agricultural, forestry or industrial zone, the minimum distance shall be 30 feet; or at intersections including an alley, 10 feet.
 - 2. In all other zones, the minimum distance shall be in relationship to street and road right of way widths as follows:

Right of way Width Clear vision 80 feet or more 20 feet Less than 80 feet 30 feet

Figure 15.88-1. Clear Vision Areas



Applicant Response: This section only references the intersections of streets or an intersection of a street and a railroad. The provisions of this section do not apply to the subject property.

15.88.050 Pedestrian Access and Circulation

A. Purpose and Intent. This section implements the pedestrian access and connectivity policies of City of La Pine Transportation System Plan and the requirements of the Transportation Planning Rule (OAR 660-012). It is intended to provide for safe, reasonably direct, and convenient pedestrian access and circulation.

Applicant Response: This is a purpose statement and it does not provide any measurable development standards or approval criteria.

- B. Standards. New subdivisions, multi-family developments, planned developments, commercial developments and institutional developments shall conform to all of the following standards for pedestrian access and circulation:
 - 1. Continuous Walkway System. A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.

Applicant Response: As documented on the Plan Set, the design includes a walkway that extends from the Assembly Way right-of-way to all of the entrances of the building. Furthermore, walkways connect parking areas to the building, to all use areas and throughout the site. The proposed design conforms to the standards of this section by making practical connections throughout the development area.

- 2. Safe, Direct, and Convenient. Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, recreational areas, playgrounds, and public rights-of-way conforming to the following standards:
 - a. The walkway is reasonably direct. A walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-ofdirection travel.

Applicant Response: As shown on the Site Plan, the proposed development provides walkable areas that extend in a relatively straight fashion, from the building entrances to and throughout the property, and to the sidewalks in the abutting street system, Assembly Way. As proposed, the design conforms to this standard.

b. The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct

route of travel between destinations. The City may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.

Applicant Response: As shown on the Site Plan, walkways are proposed to be maintained for pedestrians. Walkways are proposed to be constructed of concrete and they provide straight lines of travel between site elements, such as from doors to parking areas and/or to the abutting sidewalks. As designed, the proposal conforms to this standard.

c. Vehicle/Walkway Separation. Except as required for crosswalks, per subsection 4, below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the City may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.

Applicant Response: The site has been designed to avoid pedestrian drive aisle crossings. Also, the design includes raised curbs and/or curb stops adjacent to the parking spaces and maneuvering areas. The design will separate the parking and maneuvering areas from the walkways. As designed, the proposal conforms to these standards.

d. Crosswalks. Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrasting material). The crosswalk may be part of a speed table to improve driver- visibility of pedestrians.

Applicant Response: The proposed design does not necessitate any crosswalks.

e. Walkway Construction. Walkway surfaces may be concrete, asphalt, brick or masonry pavers, or other City-approved durable surface meeting ADA requirements. Walkways shall be not less than four feet in width, except that the City may require five- foot wide, or wider, sidewalks in developments where pedestrian traffic warrants walkways wider than four feet.

Applicant Response: As shown on the Site Plan, the walkways are proposed to be constructed of concrete, which conforms to the construction standards of this section.

f. Multi-Use Pathways. Multi-use pathways, where approved, shall be 10 feet wide and constructed of asphalt, concrete or other City-approved durable surface meeting ADA requirements consistent with the applicable City engineering standards.

Applicant Response: Multi-use paths are neither proposed nor required; therefore this section does not apply.

Chapter 15.90 Public Facilities

15.90.010 Public Facilities Improvement

Minor betterment, improvements, replacement or reconstruction of existing public facilities such as sewer and water lines, storm water drainage facilities, sidewalks and other pedestrian ways or facilities, bikeways and similar public facilities within rights-of-ways and easements for the purposes existing on or before the effective date of this ordinance, or on contiguous publicly-owned property designated, intended or utilized to support the facilities, or the facilities that are set forth within an adopted public facilities plan or other capital improvement plan duly adopted on or before the effective date of this ordinance, are exempt from permit requirements, unless specifically set forth otherwise.

Applicant Response: The proposal does not include the review of public facility improvements outside of a land use process; therefore this section does not apply.

15.90.020 Developer Responsibility for Streets and Other Public Facilities

A. Duties of developer. It shall be the responsibility of the developer to construct all streets, curbs, sidewalks, sanitary sewers, storm sewers, water mains, electric, telephone and cable television lines necessary to serve the use or development in accordance with the specifications of the city and/or the serving entity.

Applicant Response: Water and sewer mains exist within the Assembly Way right-of-way. As detailed on the Plan Set, the design extends water and sewer laterals, along with franchise utilities to the site. Furthermore, the design includes sidewalk and roadway improvements within Assembly Way right-of-way. As detailed on the Plan Set, the design upgrades Assembly Way to the extent needed to accommodate the proposed development and the planned improvements will be constructed to City Standards.

B. Over-Sizing. The City may require as a condition of development approval that sewer, water, or storm drainage systems serving new development be sized to accommodate future development within the area as projected by the applicable facility master plan, and the City may authorize other cost-recovery or cost- sharing methods as provided under state law.

Applicant Response: An 8-inch water main and a 4-inch pressure sewer main exists within the abutting right-of-way. The mains have adequate capacity to accommodate the proposed development, which will be reviewed and confirmed with this Site Plan review process. It is not anticipated that any additional upgrades to the mains will be

necessary.

C. Inadequate existing streets. Whenever existing streets, adjacent to, within a tract or providing access to and/or from a tract, are of inadequate width and/or improvement standards, additional right-of-way and/or improvements to the existing streets may be required.

Applicant Response: The abutting Assembly Way right-of-way is 60 feet in width, which does not meet the 64 foot width requirement of the TSP. At the pre-application meeting, the applicant was not informed of a need to dedicate any additional right-of-way. Also, the property abuts Reed Road to the north, with is an Industrial Collector Street and developed within 80 feet of right-of-way and conforms to City Standards. As proposed, the project enhances the right-of-way abutting the subject property and conforms to the standards of this section.

D. Half streets. Half streets, while generally not acceptable, may be approved where essential to the reasonable development of a proposed land development, and when the City finds it will be practical to require dedication and improvement of the other half of the street when the adjoining property is developed. Whenever a half street exists adjacent to a tract of land proposed for development, the other half of the street shall be dedicated and improved.

Applicant Response: The property abuts full streets; the half street provisions of this section are not applicable.

15.90.030 Sewer and Water

A. Sewer and Water Plan Approval. Development permits for sewer and water improvements shall not be issued until the Public Works Director has approved all sanitary sewer and water plans in conformance with City standards.

Applicant Response: Water and sewer mains exist within the vicinity (in the Assembly Way right-of-way). Furthermore, the site is proposed to be developed with water and sewer lateral connections that conform to City Standards and will serve the planned development. The applicant plans to obtain all needed approvals.

B. Inadequate Facilities. Development permits may be restricted or rationed by the City where a deficiency exists in the existing water or sewer system that cannot be rectified by the development and which, if not rectified, will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of domestic water and sewerage treatment systems. The City may require water booster pumps, sanitary sewer lift stations, and other critical facilities be installed with backup power.

Applicant Response: As detailed on the Engineering Plan Set, water and sewer mains exist within the abutting Assembly Way right-of-way. Also, water and sewer laterals will be extended to the site in conformance with City Standards. As designed, the planned facilities will have adequate capacity to accommodate the proposed development. It is not anticipated that development permits will be restricted or rationed due to water or sewer capacity.

15.90.040 Stormwater.

- Accommodation of Upstream Drainage. Culverts and other drainage facilities shall be large enough to accommodate existing and potential future runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the City Engineer.
- 2. Effect on Downstream Drainage. Where it is anticipated by the City Engineer that the additional runoff resulting from the development will overload an existing drainage facility, the City shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with City standards.

Applicant Response: Public and private improvements are considered in this review. The Assembly Way improvements are proposed in accordance with City Standards, which will ensure that public stormwater is adequately addressed in the right-of-way. Regarding on-site improvements, the proposal is supported by a Drainage Plan that documents how drainage will be maintained on-site, through the use of drainage chambers. As proposed, the design conforms to these standards.

15.90.050 Utilities

- A. General Provision. The developer of a property is responsible for coordinating the development plan with the applicable utility providers and paying for the extension and installation of utilities not otherwise available to the subject property.
- B. Underground Utilities. All new electrical, telephone or other utility lines shall be underground unless otherwise approved by the city.

Applicant Response: The applicant has coordinated with all utility providers and they have confirmation that the providers can and will serve the new development. All new utilities are planned to be extended underground, in conformance with these standards.

C. Subdivisions...

Applicant Response: The proposal does not include a subdivision, therefore this section does not apply.

D. Exception to Undergrounding Requirement. The City may grant exceptions to the undergrounding standard where existing physical constraints, such as geologic conditions, streams, or existing development conditions make underground placement impractical.

Applicant Response: The proposal does not include an exception to providing new utilities underground; therefore this standard does not apply.

15.90.060 Public Street/Highway Improvement.

The following public streets and highway improvement activities are permitted outright in all zones and are exempt from the permit requirements of this Code.

- A. Installation of additional and/or passing lanes, including pedestrian ways and/or bikeways, within a public street or highway right-of-way existing as of the effective date of this chapter, unless such adversely impacts on-street parking capacities and patterns.
- B. Reconstruction or modification of public roads and highways, not including the addition of travel lanes, where no removal or displacement of buildings would occur, and/or no new land parcels result.
- C. Temporary public road and highway detours that will be abandoned and restored to original condition or use at such time when no longer needed.
- D. Minor betterment of existing public roads and highway related facilities such as maintenance yards, weigh stations, waysides, and, rest areas within a right-of-way existing as of the effective date of this Code. In addition, also exempt are contiguous public-owned property utilized to support the operation and maintenance of public roads and highways provided such is not located within a duly designated Residential Zone, or adjacent to or across the street from a lot or parcel within such a zone.
- E. The construction, reconstruction, or modification of a public street or highway that is identified as a priority project in a transportation system plan (TSP) or the State Transportation Improvement Plan (STIP) that was duly adopted on or before the effective date of this chapter.
- F. The design, construction, operation, and maintenance of a tourist-oriented or public wayside.

Applicant Response: The proposal includes improvements in the Assembly Way right-of-way. Pursuant to this section, the noted improvements are permitted.

15.90.070 Design of Streets and Other Public Facilities.

A. Traffic circulation system. The overall street system shall assure an adequate traffic circulation system with intersection angles, grades, tangents and curves appropriate for the traffic to be carried considering the terrain of the development and the area. An analysis of the proposed traffic circulation system within the land division, and as such system and traffic generated there from affects the overall City of La Pine transportation, will be required

to be submitted with the initial land division review application. The location, width and grade of streets shall be considered in their relationship to existing and planned streets, to topographical conditions, to public convenience and safety and to the proposed use or development to be served thereby.

Applicant Response: The property is within a relatively recently developed subdivision that was established in with a transportation system that conforms to City Standards. All needed surrounding rights-of-way exist and with this development improvements are proposed within the existing Assembly Way right-of-way. The location, width and grade of the planned improvements that are noted on the Plan Set are consistent with Development Code requirements and City Public Works Standards; therefore the improvements are in proper relationship with the surrounding properties and developed rights-of-way. The proposed design ensures added convenience and safety to the surrounding properties and the community at-large. The proposal, including planned improvements, will ensure conformance with this standard.

- B. Street location and pattern. The proposed street location and pattern shall be shown on the development plan, and the arrangement of streets shall:
 - 1. Provide for the continuation or appropriate projection of existing principal streets in surrounding areas; or
 - 2. Conform to a plan for the general area of the development approved by the City to meet a particular situation where topographical or other conditions make continuance or conformance to existing streets impractical; and
 - 3. Conform to the adopted La Pine Transportation System Plan as may be amended.

Applicant Response: As documented on the Plan Set, the proposed sidewalk improvements will be within the existing Assembly Way right-of-way. The sidewalk design conforms to the La Pine Transportation System Plan and the topography that exists on the site and surrounding area. As designed, the proposal conforms to these standards.

C. Access Ways. The City, in approving a land use application with conditions, may require a developer to provide an access way where the creation of a cul-de-sac or dead-end street is unavoidable and the access way connects the end of the street to another street, a park, or a public access way. Where an access way is required, it shall be not less than 10 feet wide and shall contain a minimum six-foot-wide paved surface or other all-weather surface approved by the City. Access ways shall be contained within a public right-of-way or public access easement, as required by the City.

Applicant Response: The proposal enhances the existing development in the area and does not necessitate additional pedestrian access ways.

D. Future street extensions...

Applicant Response: The proposal includes improvements in existing right-of-way amongst an established grid. Street extensions are not anticipated in this area; therefore this section does not apply.

E. Minimum right-of-way and roadway widths. Unless otherwise approved in the tentative development plan, street, sidewalk and bike rights-of-way and surfacing widths shall not be less than the minimum widths in feet set forth in the La Pine Transportation System Plan, and shall be constructed in conformance with applicable standards and specifications set forth by the city.

Applicant Response: The TSP establishes the following standards:

Roadway Cross Section Standards

Table 4-4 presents the dimensional standards for the five proposed functional classifications in La Pine.

| | Features/Dimensions (Each Direction) | | | | | | | Tabal |
|------------------------------|--------------------------------------|--------------|--------------------------|----------|-------------------|---------------------------------------|--------------------------|------------------------------------|
| Functional Classification | Travel Lane | Bike Lane | On- Street Parking | Sidewalk | Plante r Strip | Left Turn Lane/ Median | Total Paved Width | Total Right- of-Way Width |
| Arterial | 12' | 6' | None | 6′ | 8' | Left-Turn Lanes, 14' | 36' to 50' | 78' |
| Major Collector | 11' | 6′¹ | 7′² | 6′ | 8' | None | 34 ¹ - 48' | 76' |
| Local Street | 11' | None | 7' | 6' | 8' | None | 36' | 64' |
| Downtown Arterial | 12' | 6' | Optional, | 8' | 8′ | Optional Landscaped Median, 14' | 50' | 82 |
| Minor Collector | 11' | 6′ | None | 6′ | 8′ | None | 34' | 62′ |
| Industrial Collector | 14' | 6′ | None | 6′ | None | None | 40′ | 52' |

Table 4-4 Roadway Cross-Section Standards

Reed Road (to the north) is an Industrial Collector street, which is not constructed in conformance with the minimum standards of this section, it does not contain a bike lane or sidewalk. Assembly Way (to the south) is a local street that is not constructed in conformance with the minimum standards of this section, it is constructed within 60 feet of right-of-way, it does not have a sidewalk. Both of the abutting roadways are managed by Deschutes County and at the pre-application meeting, the County Road Department did not indicate the need for any right-of-way dedication or street improvements. The proposal does include the installation of a 6 foot wide sidewalk in the Assembly Way right-of-way. The proposed design improves the abutting right-of-

¹On low volume, low speed (>30 mph) facilities, alternative bicycle facilities can be considered at the discretion of the City

²On-street parking provide adjacent to commercially zoned properties

way to an extent commensurate with the proposed development and as expressed by the manager of the abutting right-of-way (Deschutes County Road Department). The proposed improvements will bring this street into conformance with the standards of this section along the property frontage to the extent required.

F. Sidewalks. Unless otherwise required in this chapter or other city ordinances or other regulations, or as otherwise approved by the Commission, sidewalks shall be required as specified in the La Pine Transportation System Plan. In lieu of these requirements, however, the City may approve a development without sidewalks if alternative pedestrian routes and facilities are provided.

Applicant Response: As documented on the Plan Set, a 6 foot wide sidewalk is planned in the Assembly Way right-of-way, which is consistent with the City of La Pine TSP. At the Pre-Application Meeting, the applicant was not informed of a need to install sidewalks along Reed Road (no sidewalks exist in the vicinity, along that right-of-way).

G. Bike lanes. Unless otherwise required in this chapter or other city ordinances or other regulations, bike lanes shall be required as specified in the La Pine Transportation System Plan, except that the Planning Commission may approve a development without bike lanes if it is found that the requirement is not appropriate to or necessary for the extension of bicycle routes, existing or planned, and may also approve a development without bike lanes in the streets if alternative bicycle routes and facilities are provided.

Applicant Response: The abutting road (Reed Road) is an Industrial Collector street and bike lanes are do not currently exits thereupon. Bike lanes are most efficiently established by the City, so that they can serve a usable area (as opposed to randomly placed along newly developed properties). Rather than establishing bicycle lanes at this time (only adjacent to this property), it is anticipated that the City will take on an area-wide project in the future.

H. Cul-de-sacs...

Applicant Response: The design does not include any cul-de-sacs; therefore this section does not apply.

I. Marginal access streets. Where a land development abuts or contains an existing or proposed arterial street, the city may require marginal access streets, reverse frontage lots with suitable depth, screen- plantings contained in a non-access reservation strip along the rear or side property line or other treatments deemed necessary for adequate protection of residential properties and the intended functions of the bordering street, and to afford separation of through and local traffic.

Applicant Response: The site does not contain or abut an Arterial Street; therefore

the marginal street access provisions of this section do not apply.

J. Streets adjacent to railroad right-of-way...

Applicant Response: The property is not adjacent to a railroad right-of-way; therefore this section does not apply.

K. Reserve Strips. Reserve strips or street plugs controlling access to streets will not be approved unless deemed necessary for the protection of public safety and welfare and may be used in the case of a dead-end street planned for future extension, and in the case of a half street planned for future development as a standard, full street.

Applicant Response: The property is not abutted by a reserve strip and no reserve strips are proposed; therefore this section does not apply.

- L. Alignment...
- M. Intersection angles...
- N. Curves...
- O. Street grades...
- P. Street names...

Applicant Response: The proposal includes development adjacent to existing street rights-of-way; the proposal does not modify the existing street alignment, intersection angles, curves, grades or names; therefore the standards of these sections do not apply.

Q. Street name signs. Street name signs shall be installed at all street intersections by the developer in accordance with applicable city, county or state requirements. One street sign shall be provided at the intersection of each street, and two street signs shall be provided at four-way intersections.

Applicant Response: Street signs exist in the vicinity of the property and no new streets are proposed. Therefore, the installation of new street signs is not applicable.

R. Traffic control signs. Traffic control signs shall be provided for and installed by the developer as required and approved by the appropriate city, county and/or state agency or department.

Applicant Response: The applicant understands that it would be their responsibility to provide and install any required traffic control signs and plans as needed to facilitate the planned improvements. Based upon the traffic report and pre-submittal correspondence, it is not anticipated that any new traffic control signs will be needed.

S. Alleys. Alleys are not necessary in residential developments, but may be required in commercial and industrial developments unless other permanent provisions for access to off-street parking and loading facilities are approved

by the city.

Applicant Response: Development is proposed in an area with an established street grid. Access drives will be provided to serve the development site in a manner that allows for access to off-street parking facilities. Alleys are not appropriate or necessary in this area.

T. Curbs. Curbs shall be required on all streets in all developments, and shall be installed by the developer in accordance with standards set forth by the city unless otherwise approved by the city. Approval of streets without curbs shall be at the discretion of the City Engineer, and shall be so determined during the tentative plan land division review process on the basis of special circumstances to the development.

Applicant Response: The existing street and surrounding area is developed without curbs. Given the existing design and configuration, and the fact that this section allows the City Engineer to determine if and when curbs are required, it is expected that curbs will not be required at this location.

U. Street lights. Street lights may be required and, if so required, shall be installed by the developer in accordance with standards set forth by the city and the serving utility company. Streets lights, if required, shall include one (1) fixture and be located at the intersection of streets.

Applicant Response: The property is not located at the intersection of 2 streets; therefore street light installation is not applicable.

V. Utilities. The developer shall make necessary arrangements with the serving utility companies for the installation of all proposed or required utilities, which may include electrical power, natural gas, telephone, cable television and the like.

Applicant Response: The applicant has coordinated with all utility providers and has confirmation that they can serve the property. All new utilities are planned to be extended underground, in conformance with these standards.

W. Drainage facilities. Drainage facilities shall be provided as required by the City in accordance with all applicable City and Oregon Department of Environmental Quality standards.

Applicant Response: New public impervious are reviewed with this application, including the sidewalk and road improvements in the Assembly Way right-of-way. The planned design conforms to City Standards. The design, will accommodate all existing and future run-off from the public facilities within the right-of-way, the design of which will be reviewed by the City Engineer prior to construction. Regarding on-site drainage, as shown on the Drainage Plan, all on-site drainage will be accommodated in drainage chambers on-site. As designed the proposal conforms to these standards.

X. Gates. Except where approved as part of a Master Planned Development, private streets and gated drives serving more than two dwellings (i.e., where a gate limits access to a development from a public street), are prohibited.

Applicant Response: Neither private streets nor gates are proposed. This section does not apply.

15.90.080 Traffic Impact Analysis

- A. Purpose. The purpose of this subsection is coordinate the review of land use applications with roadway authorities and to implement Section 660-012-0045(2)(e) of the state Transportation Planning Rule, which requires the City to adopt a process to apply conditions to development proposals in order to minimize impacts and protect transportation facilities. The following provisions also establish when a proposal must be reviewed for potential traffic impacts; when a Traffic Impact Analysis must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; the required contents of a Traffic Impact Analysis; and who is qualified to prepare the analysis.
- B. When a Traffic Impact Analysis is Required. The City or other road authority with jurisdiction may require a Traffic Impact Analysis (TIA) as part of an application for development, a change in use, or a change in access. A TIA shall be required where a change of use or a development would involve one or more of the following:...
 - 1. A change in zoning or a plan amendment designation;
 - 2. Operational or safety concerns documented in writing by a road authority;
 - 3. An increase in site traffic volume generation by [300] Average Daily Trips (ADT) or more;
 - 4. An increase in peak hour volume of a particular movement to and from a street or highway by [20] percent or more;
 - 5. An increase in the use of adjacent streets by vehicles exceeding the 20,000 pound gross vehicle weights by 10 vehicles or more per day;
 - 6. Existing or proposed approaches or access connections that do not meet minimum spacing or sight distance requirements or are located where vehicles entering or leaving the property are restricted, or such vehicles are likely to queue or hesitate at an approach or access connection, creating a safety hazard;

- 7. A change in internal traffic patterns that may cause safety concerns; or
- 8. A TIA required by ODOT pursuant to OAR 734-051.

Applicant Response: The submittal packet is supported by a Trip Generation Letter that has been prepared by HWA. The Trip Generation Letter is sufficient for the size/scale of the proposed development.

C. Traffic Impact Analysis Preparation. A professional engineer registered by the State of Oregon, in accordance with the requirements of the road authority, shall prepare the Traffic Impact Analysis.

Applicant Response: The submittal packet is supported by a Trip Generation Letter that has been prepared by Grant Hardgrave of HWA, professional engineer registered by the State of Oregon, as depicted by the stamp on the report. The proposal complies with this requirement.

D. Waiver or Deferral. The City may waive or allow deferral of standard street improvements, including sidewalk, roadway, bicycle lane, undergrounding of utilities, and landscaping, as applicable, where one or more of the following conditions in (1) through (4) is met. Where the City agrees to defer a street improvement, it shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future...

Applicant Response: Waiver or deferment is not proposed at this time. These provisions do not apply.

C. Article 6 - Special Use Standards:

Sec. 15.102.010. - Purpose.

Special uses included in <u>article 6</u> are uses which, due to their effect on surrounding properties, must be developed in accordance with special conditions and standards. These special use standards may differ from the development standards established for other uses in the same zoning district.

Applicant Response: This section establishes a purpose, but no measurable development standards or approval criteria.

Sec. 15.102.020. - Applicability.

All uses listed in <u>chapter 15.104</u> and <u>chapter 15.108</u> are subject to the standards of <u>article 6</u>. Special use standards may apply to an entire use category, as described in <u>chapter 15.14</u>, use categories, or a specific use within a use category. The standards of this chapter supplement the other requirements of

this article. When a dimensional standard for a special use differs from that of the underlying district, the standard for the special use shall apply...

Applicant Response: One of the potential uses (Artisanal and light manufacturing uses) has special standards (established in Section 15.108.010); this section applies and the referenced section is addressed below.

Sec. 15.108.010. - Artisanal and light manufacturing uses.

A. Applicability. The following provisions are intended to encourage mixed-use development, including cottage industries and business incubators, by integrating small-scale manufacturing with commercial uses. Artisanal and light manufacturing uses are permitted in the C and CN zones as a primary use with a conditional use permit. Artisanal and light manufacturing uses may also be permitted as an accessory to a permitted use in a commercial or mixed-use zones. Whether a primary or accessory use, all artisanal and light manufacturing uses in commercial or mixed-use zones must meet the standards of this section.

B. Standards.

- 1. Where an artisanal and light manufacturing use is allowed in a commercial or mixed-use zone, it shall be wholly enclosed in a building.
- 2. Where an artisanal and light manufacturing use is allowed in a commercial or mixed-use zone and the subject site is located within 100 feet of a residential zone, the city may limit the hours of operation of the use to between 7:00 a.m. and 10:00 p.m. where it has identified concerns about noise, parking, or other impacts related to the use.

Applicant Response: The standards of this section apply to the placement of artisanal and light industrial uses in commercial or mixed-use zones. The property is located within the I Zone. These standards do not apply to the referenced use within the I Zone.

D. Article 7 - Procedures:

15.202.0010 Purpose and Applicability

A. Purpose. The purpose of this chapter is to establish decision-making procedures that will enable the City, the applicant, and the public to reasonably review applications and participate in the local decision-making process in a timely and effective way. Table 15.202-1 provides a key for determining the review procedure and the decision-making body for particular applications.

Applicant Response: This section addresses the procedures that will be utilized in the review of this application packet. The procedures do not include any development standards or approval criteria that the applicant needs to document conformance with, instead this sections details how the applications are to be reviewed by the City. The majority of the provisions of this Article direct City Staff, however there are a few sections that identify applicant specific required procedural provisions. The applicant anticipates that the applicable procedures of this section will be followed by City Staff. The procedures of this section that are requirements of the applicant (and those which could use further analysis), are addressed below.

- B. Applicability of Review Procedures. All land use and development permit applications, except building permits, shall be decided by using the procedures contained in this article as modified by any applicable application-specific procedures identified in Articles 8 and 9. The procedure "type" assigned to each application governs the decision-making process for that application. There are four types of review procedures as described in subsections 1-4 below. Table 15.202-1 lists the City's land use and development applications and corresponding review procedure(s)...
 - 2. Type II Procedure (Administrative/Staff Review with Notice). Type II decisions are made by the City Planning Official, with public notice and an opportunity for appeal to the Planning Commission. Alternatively, the City Planning Official may refer a Type II application to the Planning Commission for its review and decision in a public meeting.

Applicant Response: The proposal is a Site Plan. Based upon Table 15.202-1, the applicant anticipates that the application will be reviewed via the Type II procedure.

15.202.020 Time Limit and Consolidated Review.

C. Consolidated Review of Applications. When an applicant applies for more than one type of application for the same one or more contiguous parcels of land, the proceedings shall be consolidated for review and decision. When proceedings are consolidated, required notices may be consolidated, provided the notice shall identify each application to be decided. When more than one application is reviewed in a hearing, separate findings and decisions shall be made on each application.

Applicant Response: The applicant has submitted a Site Plan application only; therefore, there is no need to consolidate applications.

15.202.040 Pre-application conference

A. A pre-application conference is encouraged for complex applications or for applicants who are unfamiliar with the land use process and is required for all Type III applications. The purpose of the conference shall be to acquaint the applicant with the substantive and procedural requirements of the

applicable land use codes, to provide for an exchange of information regarding applicable requirements of the comprehensive plan, zoning code or land division code and to identify issues likely to arise in processing an application. The applicable zoning code may require that a pre-application conference be held for particular types of applications.

B. Required pre-application conferences must be held no more than one year prior to the submittal of a Type III land use application. Requests for pre-application conferences shall be made on a form provided by the City.

Applicant Response: While the application is not complex, a pre-application was held; the events that occurred are consistent with this provision.

15.202.050 Neighborhood Contact

A. Purpose and Applicability. Unless waived by the City Planning Official, applicants for master plans, subdivisions with more than 10 lots, major variances and property owner-initiated for zone changes are required to contact neighboring property owners and offer to a hold meeting with them prior to submitting an application...

Applicant Response: The proposal is for a Site Plan. It is not for a master plan, subdivisions with more than 10 lots, major variance or property owner-initiated for zone change. Therefore Neighborhood Contact is not required.

E. Title 8 - Applications and Reviews

Chapter 15.312 – Site Plan Review

15.312.010 Purpose

- A. The purpose of the site plan review provisions of this section is to ensure that development within the City complies with standards and limitations set forth within the applicable zone, by other city standards and requirements and by applicable county, state and federal regulations.
- B. This broad purpose is furthered by the following specific purposes of site plan review.
 - 1. To implement the goals and policies of the Comprehensive Plan.
 - 2. To foster development that is designed, arranged and constructed in a manner that provides a safe, efficient and aesthetically pleasing community asset.
 - 3. To encourage originality and creativity in site design, architecture and landscape design.

- 4. To ensure that the arrangement of all functions, uses and improvements of a development reflect the natural amenities, capabilities and limitations of its site and adjacent areas.
- 5. To encourage development where the various structures, use areas and site elements are integrated in a manner that is visually harmonious within the development and the surrounding area.
- 6. To encourage development and landscape design that complements the natural landscape and setting, improves the general appearance of the community and enhances specific elements of the manmade environment, both presently and historically.

Applicant Response: This is a purpose statement and it does not provide any measurable development standards or approval criteria.

15.312.020 Applicability

The following uses and development shall be subject to the provisions of this section.

A. All new construction or new development except for: single-family residences (including manufactured dwellings, mobile homes, modular homes), duplexes, accessory dwelling units and related accessory structures unless provided otherwise in this chapter....

Applicant Response: The proposal includes new development and is a development other than a single-family home, duplex, or accessory dwelling; therefore Site Plan Review is required.

15.312.025 Site Plan approval required

Site Plan review and approval, as specified by this chapter, shall be required prior to the following:

- A. Site clearance activities such as grading, excavation or filling for any use or development requiring a permit pursuant to this Code.
- B. The issuance of a building or development permit for any use or development requiring City approval pursuant to this Code.

Applicant Response: The applicant plans to receive all required approvals prior to initiating any of the items listed in this section.

15.312.030 Procedure Type

B. Site Plan Review applications are subject to Type II Review in accordance with

the procedures in Article 7 unless elevated to a Type III review at the discretion of the Planning Official.

Applicant Response: This section establishes that a Site Plan is reviewed via the Type II procedures, it is expected that the application will be reviewed accordingly.

- C. Pre-application conference. Prior to applying for site plan approval, applicants should and may meet with the City Planning Official, Building Official and Public Works Director, or designees thereof, and present a preliminary plan which shall contain, in an approximate manner, the information required on a site plan review application.
 - 1. The purpose of the preliminary site plan review is to enable the applicant to obtain advice from the City as to the intent, standards, criteria and provisions of this Chapter, this Code, other city ordinances, standards and regulations, and state and federal rules and regulations which may be pertinent to the proposal.
 - 2. Information presented for preliminary discussion shall be considered confidential if so requested by the applicant.

Applicant Response: The applicant held a pre-application meeting with City Staff prior to this submittal. The pre-application meeting that was held conforms to these requirements.

15.312.040 Submittal Requirements

A property owner or authorized representative thereof may initiate a request for site plan review by filing an application with the City using forms prescribed by the City together with the required filing fee in accordance with the Type II application requirements in Article 7. In addition to the information required for a Type II review (see Article 7), the applicant shall submit that which is listed below.

A. Requirements for information to be submitted. Information provided on the site plan shall conform to the following. The number of copies required shall be as specified on the application form.

- 1. Drawings depicting the proposal shall be presented on sheets not larger than 24 inches by 36 inches in the number of copies directed by the City.
- 2. To facilitate public reviews and notice, at least one copy of the proposal shall be provided on a sheet of paper not larger than 11 inches by 17 inches.
- 3. Drawings shall be at a scale sufficiently large enough to enable all features of the design to be clearly discerned and shall include a north arrow and scale.

- 4. The City may require that the drawing, development plan or other information be provided to the City on computer disk in a format adaptable to the city's computer systems.
- B. Site analysis diagram. If required by the City Planning Official, this element of the site plan, which may be in schematic or free hand form to scale, shall indicate the following site characteristics.
 - 1. Location and species of existing trees greater than six inches in diameter when measured four feet above the natural grade, and an indication of which trees are proposed to be removed.
 - 2. On sites that contain steep slopes, potential geological hazard or unique natural features that may affect the proposed development, the city may require contours mapped at two-foot intervals.
 - 3. Natural drainage ways, depths of any ground water tables less than 12 feet, any areas of surface water accumulations and any other significant natural features.
 - 4. The location and width of all public and private streets, drives, sidewalks, pathways, rights-of-way, and easements on the site and adjoining the site, and all buildings, utilities, retaining walls, and other manmade features, both existing and proposed.
 - 5. Natural features, including trees, riparian habitat and stream channels and structures on-site or on adjoining properties that have or may have a visual or other significant relationship with the site and the proposed development thereon.
- C. Site photographs. Photographs depicting the site and its relationship to adjoining sites and the general area are extremely valuable, should be provided, and may be required by the City Planning Official.
- D. Site development plan. The site plan shall indicate the following.
 - 1. Legal description of the property.
 - 2. Boundary dimensions and site area.
 - 3. Location and sizes of existing and proposed utilities, including water lines, sewer lines, hydrants, etc.
 - 4. Location of all existing and proposed structures, including distances from the property lines.
 - 5. Area of the site to be covered by structures, existing and proposed, and the percentage of site coverage thereby.

- 6. All external dimensions of existing and proposed buildings and structures, 8-9
- 7. Location of building entrances and exits.
- 8. Access drives, parking and circulation areas, including their dimensions.
- 9. Service areas and delivery circulation plan for such uses as the loading and delivery of goods.
- 10. Locations, descriptions and dimensions of easements as may be applicable.
- 11. Grading and drainage plans and calculations, including spot elevations and contours at intervals close enough to convey their meaning.
- 12. Location of areas to be landscaped, including designated landscape materials/plants types and sizes.
- 13. Outdoor recreation and/or play areas.
- 14. Pedestrian and bicycle circulation, including existing and proposed onsite and offsite sidewalks.
- 15. Location of mechanical equipment not enclosed within a building, garbage disposal areas, utility appurtenances and similar structures.
- 16. Exterior lighting and fencing.
- 17. Location, size and method of illumination of signs.
- 18. Provisions for handicapped persons.
- 19. Other site elements which will assist in the evaluation of site development.
- 20. Location, names, surface and right-of-way widths and improvement standards of all existing and proposed streets within or adjacent to the proposed development.
- 21. Location of areas designated for snow storage, in accordance with the requirements of Section 18.86.060, and calculations of the area required by the minimum standard and the proposed area.
- 22. Information necessary to demonstrate compliance with Fire Code including, but not limited to, fire flow, apparatus access, and hydrant spacing.

Applicant Response: As documented on the attached Exhibits, the submittal packet includes plans that document all elements detailed herein that are necessary to determine conformance with the applicable development standards and approval criteria.

- E. Accompanying written summary. In addition to the foregoing site development plan requirements, a written summary of the proposal should be provided and may be required showing the following, (unless such is shown on the site development plan).
 - 1. Commercial and nonresidential development. For commercial and nonresidential development:
 - a. The square footage contained in the site area to be developed.
 - b. The percentage of the area to be covered by structures when developed.
 - c. The percentage of the area to be covered by parking areas and the total number of parking spaces.
 - d. The total square footage of all landscaped areas, including the percentage consisting of natural materials and the percentage of hard surfaced areas such as courtyards.
 - e. Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the City Planning Official). A Traffic Impact Analysis may be required in accordance with Section 15.90.080.
 - 2. Residential development. For residential development:
 - a. The total square footage of the lot or parcel and in the structures in the development.
 - b. The number of dwelling units in the development (include the units by the number of bedrooms in each unit, for example, ten one-bedroom, 25 two-bedroom and the like).
 - c. Percentage of lot coverage by structures, way areas, recreation areas and landscaping.
 - d. Trip generation letter, signed by a professional engineer registered by the State of Oregon (unless waived by the City Planning Official). A Traffic Impact Analysis may be required in accordance with Section 15.90.080.

Applicant Response: As documented on the attached Exhibits, the submittal packet

includes plans that document all elements detailed herein that are necessary to determine conformance with the applicable development standards and approval criteria.

- F. Landscape plan. If required by the City Planning Official, a landscape plan shall be submitted and shall indicate the following.
 - 1. The size, species and locations of plant materials to be retained or placed on site.
 - 2. The layout of irrigation facilities.
 - 3. Location and design details of walkways, plazas, courtyards and similar areas.
 - 4. Location, type and intensity of outdoor lighting.
 - 5. Location and design details of proposed fencing, retaining walls and trash collection areas.
 - 6. Other information as deemed appropriate by the review authority. An arborist's report may be required for sites with mature trees that are to be retained and protected.

Applicant Response: The application is supported by Landscape Plan that contains all of the items noted in this section.

- G. Architectural drawings. This element of the site plan review, if required by the City Planning Official, shall indicate the following.
 - 1. A plan specifying the building footprint and dimensions, including all points of access. Floor plans of interior spaces to the extent required to clarify access functions and the relationship of the spaces to decks, porches, balconies and stairs or other features shown on the building elevations. The floor plans shall be provided for all building floors and shall include appropriate dimensions.
 - 2. Exterior elevations showing building heights, windows, doors, exterior light fixtures, stairways, balconies, decks and other architectural details. These elevations shall be provided for every exterior wall surface, including those which are completely or partially concealed from view by overlapping portions of the structure. Existing and finished grades at the center of all walls shall be shown with elevations of floors indicated and a dimension showing compliance with height limitations.
 - 3. Location and type of exterior light fixtures, including the lamp types and the levels of illumination that they provide.

4. Location, size and method of illumination of all exterior signs.

Applicant Response: The application is supported by Architectural Plans which include all items noted in this section.

H. Property survey. A survey of the property by a licensed land surveyor may be required, and if required the survey shall clearly delineate property boundaries, and show the location of the corners of proposed buildings and other significant features proposed for the site. The requirement for a survey of the exterior boundaries of a site may be waived where it is found that there is a recent survey that can be used to clearly establish the applicant's property boundaries.

Applicant Response: The Site Plan and supporting documents are based upon a survey of the site.

I. Deed restrictions. Copies of all existing and proposed restrictions or covenants, including those for roadway access control.

Applicant Response: A title report is included in the submittal packet, which references all restrictions on the property.

J. Narrative. A written narrative addressing the applicable criteria listed Section 15.312.050 for residential development and Sections 15.312.050 and 15.312.060 for non-residential development.

Applicant Response: This document is the written narrative addressing the applicable criteria.

K. Other information as determined by the City Planning Official. The City Planning Official may require studies or exhibits prepared by qualified professionals to address specific site features or project impacts (e.g., traffic, noise, environmental features, natural hazards, etc.), as necessary to determine a proposal's conformance with this Code.

Applicant Response: To date, no other information has been requested by the City.

15.312.050 Approval Criteria – All Residential and Non-Residential

To ensure that the stated purposes of the site plan review process are met, the review authority shall be governed by the criteria below as they evaluate and render a decision on a proposal.

A. Statement of intent.

1. The site plan review criteria are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the City with a means of reviewing proposed

plans.

- 2. These criteria provide a clear and objective means of evaluating residential development (and the residential components of a mixed use development) in accordance with ORS 197.
- 3. The review authority is not authorized as a part of the site plan review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this Code.

Applicant Response: This is a purpose statement and statement of intent. This section does not provide any measurable development standards or approval criteria.

- B. Site Plan Evaluation Criteria. The following criteria shall be used in evaluating all site development plans.
 - 1. The application is complete, in accordance with the applicable procedures in Article 7;

Applicant Response: A complete review of Article 7 – Procedures was noted above. As detailed therein, the proposal complies with the noted procedures and it is anticipated that City Staff will review the Application in accordance with the procedures that are established in that section. Therefore, the proposal complies with this approval criterion.

2. The application complies with all applicable provisions of the underlying Zoning District in Article 3, including, but not limited to, setbacks, lot dimensions, density, lot coverage, building height, and other applicable standards;

Applicant Response: A complete review of Article 3 –was noted above. As detailed therein, the proposal complies with all standards of the Industrial (I) Zone; therefore the proposal complies with this approval criterion.

3. The application complies with the provisions of the any applicable Overlay Zones in Article 4;

Applicant Response: The property is not located within an Overlay Zone; therefore this criterion does not apply.

4. The proposal complies with all applicable Development and Design Standards of Article 5;

Applicant Response: A complete review of Article 5 –was noted above. As detailed therein, the proposal complies with all standards of Article 5; therefore the proposal complies with this approval criterion.

5. The application complies with all applicable Special Use standards in Article 6;

Applicant Response: A complete review of Article 6 –was noted above. As detailed therein, the proposal complies with the Special Use Standards that apply to planned development; therefore the proposal complies with this approval criterion.

6. Adequate public facilities and utilities are available or can be made prior to occupancy to serve the proposed development;

Applicant Response: Water and sewer laterals will be extended to serve the site. As designed the proposal conforms to this approval criterion.

7. The proposed Site Plan conforms to the standards within the adopted La Pine Transportation System Plan (TSP), as may be amended from time to time, unless other design standards are specifically approved by the City.

Applicant Response: The application is supported by Traffic Report that was prepared by HWA. The submitted traffic report ensures conformance with this approval criterion.

8. The proposed Site Plan conforms to the La Pine sewer and water standards, as may be amended from time to time, unless other design standards are specifically approved by the City. All sewer improvements must comply with Oregon Administrative Rules Chapter 340 Division 52 requirements, including Appendix A - Sewer Pipelines.

Applicant Response: Water and sewer facilities will be extended to serve the site in conformance with City Standards. As designed, the proposal conforms to this approval criterion.

9. The proposed Site Plan conforms to the Central Oregon Stormwater Manual (COSM), as may be amended from time to time, unless other design standards are specifically approved by the City.

Applicant Response: The application is supported by a preliminary drainage plan that documents conformance with this approval criterion. Final drainage plans will be reviews through Building Permit review. The proposal design conforms to this approval criterion.

10. All utilities shall be installed underground, unless otherwise specifically approved by the City.

Applicant Response: As noted throughout this narrative, all new utilities are planned to be installed underground and therefore in conformance with this approval criterion.

11. The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.

Applicant Response: The site is vacant. There are no prior approvals or conditions of approval that apply.

Note: Compliance with other City codes and requirements, though not applicable land use criteria, may be required prior to issuance of building permits.

Applicant Response: This statement is informational only.

15.312.060 Additional Approval Criteria – Non-Residential Development.

In addition to the approval criteria in <u>section 15.312.050</u>, to ensure that the stated purposes of the site plan review process are met, the review authority shall also be governed by the criteria below as they evaluate and render a decision on a nonresidential development proposal.

A. Statement of intent.

- 1. The site plan review criteria for nonresidential development are intended to provide a frame of reference for the applicant in the development of a site, building and landscape plans, as well as providing the city with a means of reviewing proposed plans.
- 2. These criteria are not intended to be inflexible requirements, nor are they intended to discourage creativity. The specification of one or more architectural styles is not intended by these criteria.
- 3. The review authority is not authorized as a part of the design review process to approve projects which exceed specific development standards set forth by the applicable zone unless the exceptions are approved in accordance with specific variance or other provisions set forth in this chapter.

Applicant Response: This is a purpose statement and statement of intent. These sections do not provide any measurable development standards or approval criteria.

- B. Site plan evaluation criteria. In addition to the approval criteria in <u>section</u> <u>15.312.050</u>, the following criteria shall be used in evaluating nonresidential site development plans:
 - 1. The arrangement of all functions, uses and improvements has been designed so as to reflect and harmonize with the natural characteristics and limitations of the site and adjacent sites.

Applicant Response: The site is vacant. The proposal will enhance the existing uses in the area and provide a City sponsored Small Business Incubator Facility in the community. The proposed design will be an upgrade to the site and bring the site area into conformance with the standards of the Code, which will harmonize with the site and

surrounding area. Overall, the project conforms to this approval criterion.

2. In terms of setback from streets or sidewalks, the design creates a visually interesting and compatible relationship between the proposed structures and/or adjacent structures.

Applicant Response: The proposed design will be an upgrade to the site, bringing it into conformance with the standards of the Development Code. The design includes a variety of materials, and landscaping which will improve the street visibility of the site.

Overall, the proposed development will be a significant improvement in the area, it will enhance small business opportunities in the area, and it will add visual interest in the area, in accordance with this approval criterion.

3. The design incorporates existing features, such as streams, rocks, slopes, vegetation and the like, as part of the overall design.

Applicant Response: The site does not have any streams, rocks, or slopes. The proposal adds landscaping to the site, consistent with this criterion.

4. Where appropriate, the design relates or integrates the proposed landscaping/open space to the adjoining landscape/open space in order to create a pedestrian/bike pathway and/or open system that connects several properties or uses.

Applicant Response: There are not any adjoining open space areas, nor are there any pedestrian or bike facilities in the area (other than in the right-of-way). As proposed, the design conforms to this criterion to the maximum degree possible at this time.

5. The arrangement of the improvements on the site do not unreasonably degrade the scenic values of the community and the surrounding area in particular.

Applicant Response: There are not any established scenic corridors, elements, or protected features in the area. The proposed design includes development that conforms to all required setback, height and lot coverage standards; the proposed design is appropriately scaled for the site and use. The planned arrangement of improvements on the site is efficient and will accommodate the needs in the community. Given the proposed efficient design and the fact that there are no protected scenic elements on or around the site, the proposal will not unreasonably degrade any scenic values of the community or the surrounding area and the proposal conforms to this approval criterion.

6. Where appropriate, the design includes a parking and circulation system that encourages a pedestrian and/or bicycle rather than vehicular orientation, including a separate service area for delivery of goods.

Applicant Response: The design includes parking areas, it provides walkways and it provides bike parking. Furthermore, the layout will separate the use areas of the site. As designed, the proposal conforms to this approval criterion.

7. The design gives attention to the placement of storage, mechanical equipment, utilities or waste collection facilities so as to screen such from view, both from within and from outside the site.

Applicant Response: The proposal will adequately screen all site elements, which carries out the intent of this standard. As proposed no storage areas, mechanical equipment, utilities or waste collection elements will be visible from outside of the site.

- C. Landscape design evaluation criteria. The following criteria shall be used in evaluating landscape plans.
 - 1. The overall design substantially complements the natural environment of the city and the character of the site and the surrounding area.

Applicant Response: As detailed on the Landscape Plan, the proposed landscaping will improve the design of the property. It will complement the architectural character of the site. As designed, the proposal complies with this approval criterion.

2. The design acknowledges the growing conditions for this climatic zone, and the unique requirements that its specific site location makes upon plant selection.

Applicant Response: The vegetation selection was coordinated with local landscaping companies and has been selected based upon the climatic zone in La Pine and the requirements of the site. As designed, the proposal complies with this approval criterion.

3. Provision has been made for the survival and continuous maintenance of the landscape and its vegetation.

Applicant Response: The applicant plans to water the landscaping areas and to replace any vegetation that fails to survive.

4. The design contributes to the stabilization of slopes and the protection of other natural features and resources where applicable.

Applicant Response: The site does not include any slopes that require extraordinary stabilization and the applicant plans to protect all existing trees that will be maintained with protective fencing. As proposed, the design conforms to this approval criterion.

15.312.070 Conditions of Approval

In addition to the standards and conditions set forth in a specific zone, (if found to be necessary and supported with adequate findings) additional conditions may

be imposed by the City which are found to be necessary to avoid a detrimental impact on adjoining properties, the general area or the city as a whole, and to otherwise protect the general welfare and interests of the surrounding area. The conditions may include, but are not limited to, the following.

- 1. Limiting the manner in which the use is conducted, including restricting the time an activity may take place, and restrictions to minimize environmental impacts such as noise, vibration, air or water pollution, glare and odor.
- 2. Establishing a special setback or other open space requirements, and increasing the required lot size or other dimensional standards.
- 3. Limiting the height, size or location of a building or other structure or use.
- 4. Increasing street width and/or requiring improvements to public streets and other public facilities serving the proposed use, even including those off-site but necessary to serve the subject proposal.
- 5. Designating the size, number, improvements, location and nature of vehicle access points and routes, and requiring pedestrian and/or bicycle ways.
- 6. Limiting or otherwise designating the number, size, location, height and lighting of signs and outdoor or security lighting, and the intensity and/or direction thereof.
- 7. Requiring screening, fencing or other improvements or facilities deemed necessary to protect adjacent or nearby properties, and establishing requirements or standards for the installation and maintenance thereof.
- 8. Protecting and preserving existing trees, other vegetation and water, scenic, historic, archaeological, unique, landmark or other natural or manmade significant resources.

Applicant Response: While this section provides conditions that can be imposed on the development, based upon the size and scale of the proposal, along with presubmittal correspondence, the applicant does not anticipate that any of these conditions will be imposed.

15.312.080 Revision of Plans ...

Applicant Response: In the event that there is a need to revise the plans, the applicant understands that this section would apply.

15.312.090 Performance Assurance

1. Landscaping and other site improvements required pursuant to an approved design review plan shall be installed prior to the issuance of a

certificate of occupancy or final inspection, unless the property-owner and/or applicant submits a performance assurance device that is approved by the city committing the installation of landscaping and other site improvements within one year.

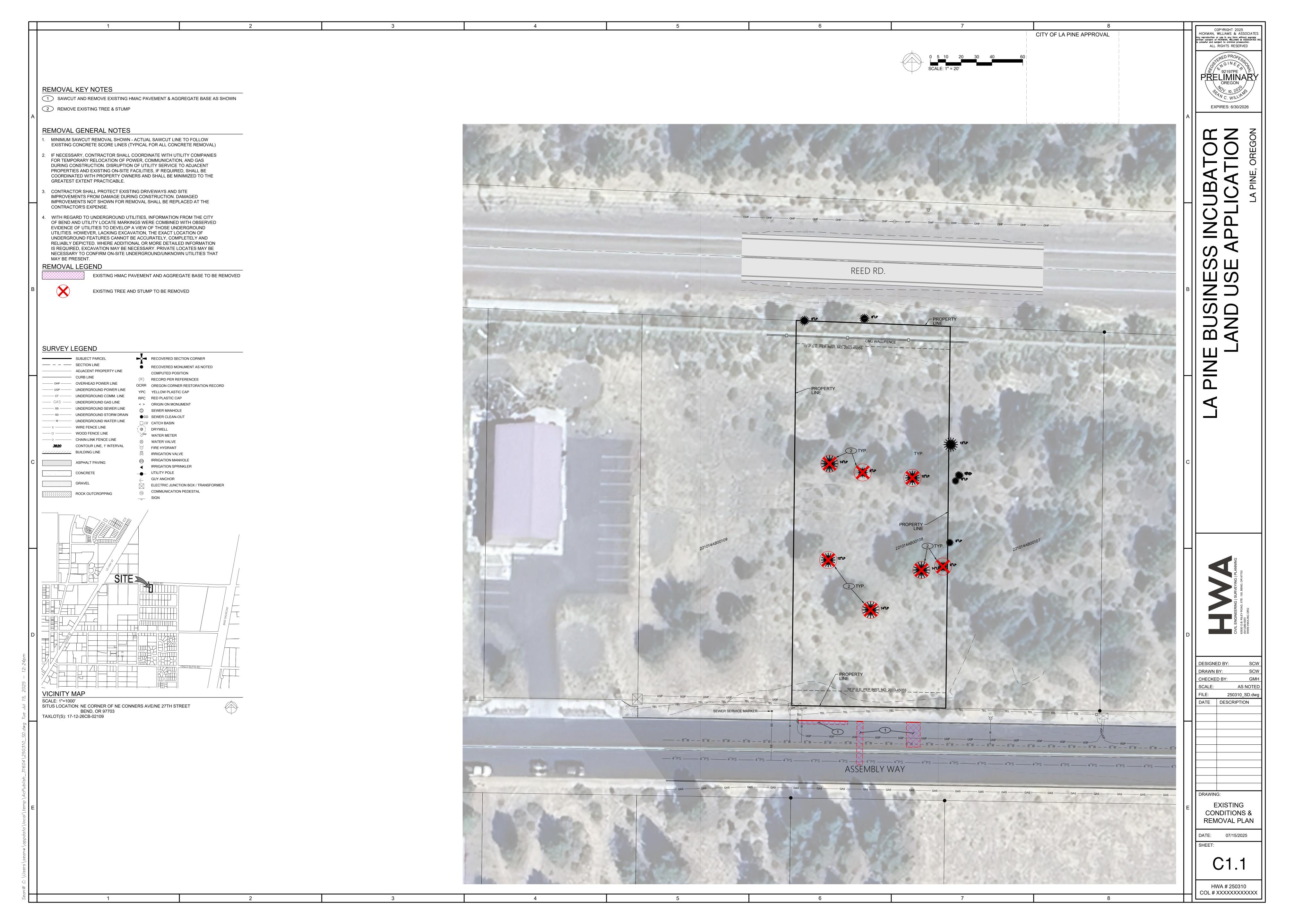
- 2. In no case shall the performance be delayed beyond the one-year period for more than six months unless approved otherwise by the City.

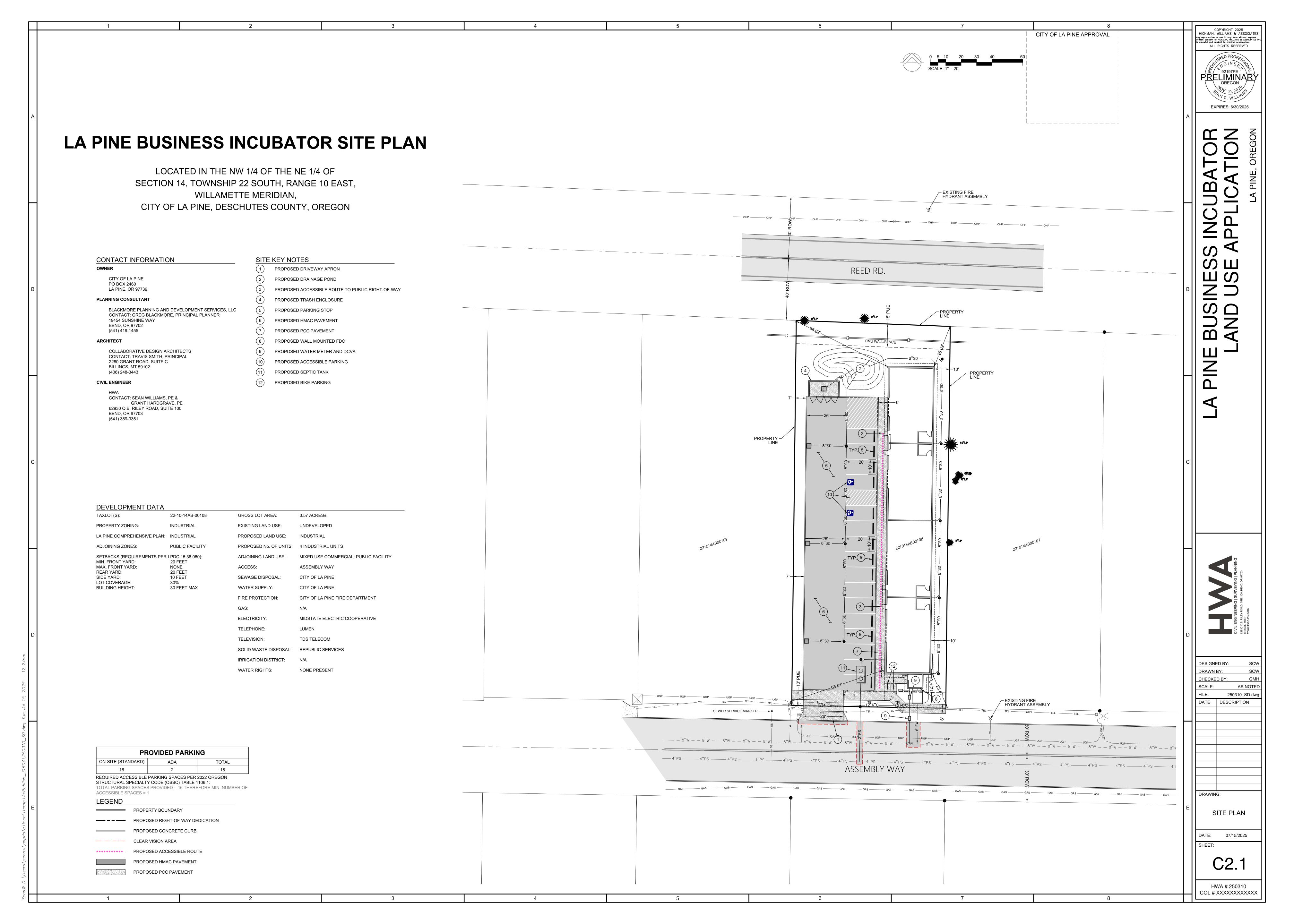
 Acceptable performance assurances shall be in compliance with the provisions of this chapter or as otherwise approved by the City.
- 3. Performance Guarantee Required for Infrastructure Improvements. The City at its discretion may allow a developer to delay installation of required public infrastructure improvements provided such infrastructure improvements must be complete and accepted by the City prior to the issuance of a certificate of occupancy, and provided that the applicant provides assurance for said improvements acceptable to the City. The applicant shall provide a bond issued by a surety authorized to do business in the state of Oregon, irrevocable letter of credit from a surety or financial institution acceptable to the City, cash, or other form of security acceptable to the City.

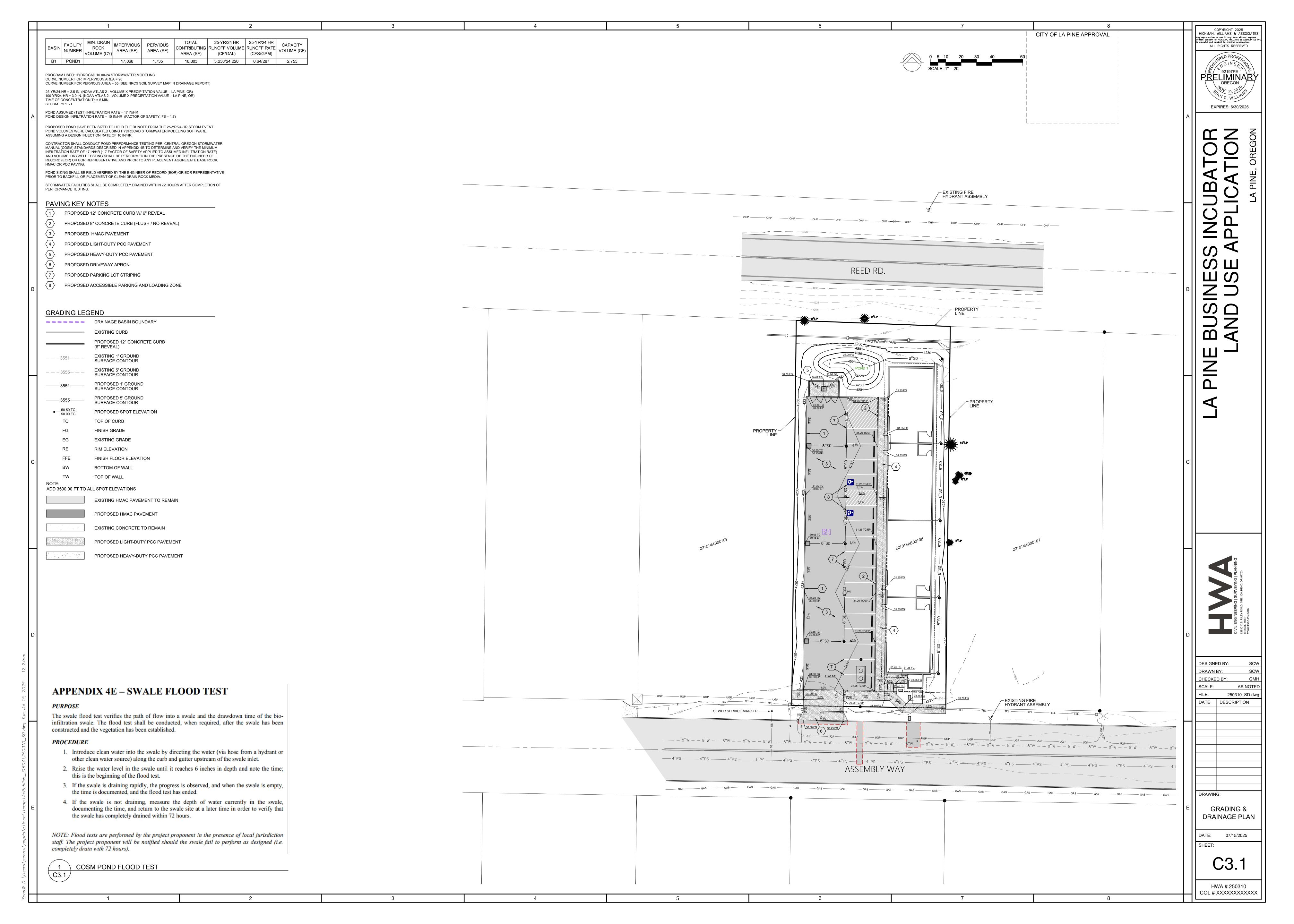
Applicant Response: In the event that performance assurances are needed, the applicant understands that this section would apply.

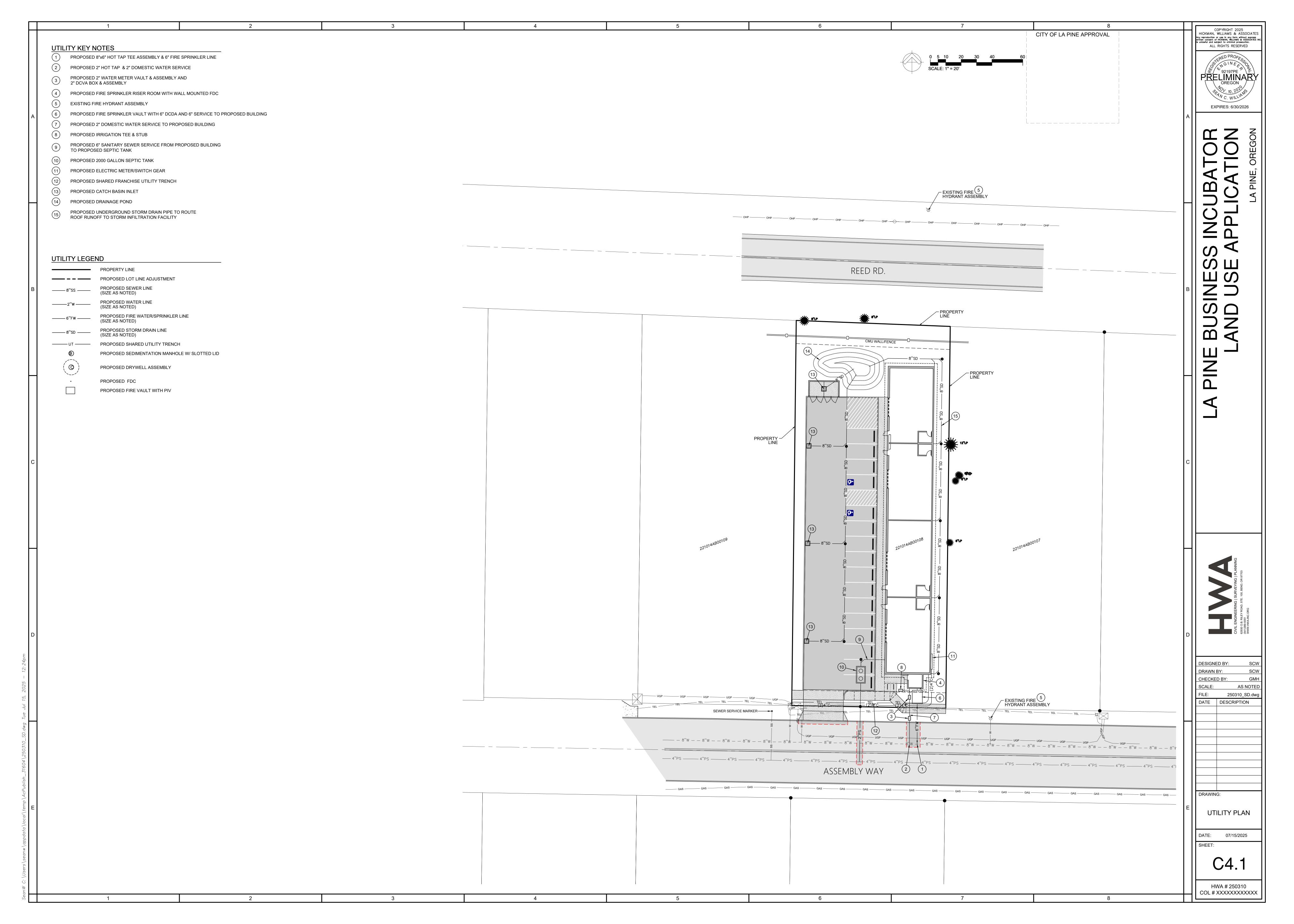
V. Summary and Conclusion:

Based on the discussion above, as well as the exhibits included with this application, the Applicant has documented that the Site Plan request meets the applicable approval criteria. Because the proposal conforms to all applicable criteria and standards, the applicant respectfully requests that the City approve the Site Plan as proposed.







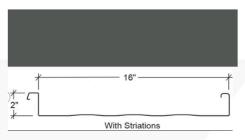




LA PINE BUSINESS INCUBATOR

7/14/25

16628 Assembly Way, La Pine, OR 97739



z

O

MTR - I CHARCOAL GREY - MBCI - BATTENLOK HS WITH STRIATIONS



MTS - I CHARCOAL GREY - MBCI - PBR WALL PANEL



<u>CMU - I SAND</u> – RCP BLOCK & BRICK – PRECISION BLOCK



EXTERIOR WALL LIGHT –

MANUFACTURER: HUBBELL LIGHTING

SERIES: SLING SG2-50-4K-PCU **FINISH:** TEXTURED BLACK





395 SW Bluff Drive, Suite 100 Bend, OR 97702 Phn - (541)382-4201 Fax - (866)524-9203

PUBLIC RECORD REPORT FOR NEW SUBDIVISION OR LAND PARTITION

THIS REPORT IS ISSUED BY THE ABOVE-NAMED COMPANY ("THE COMPANY") FOR THE EXCLUSIVE USE OF:

HWA Civil Engineering, Surveying, and Pl 62930 O B Riley Road Ste 100 Bend, OR 97701 Phone: (541)389-9351

Fax:

Date Prepared: June 02, 2025

Effective Date : 8:00 A.M on May 23, 2025

Order No. : 7069-4275646

Subdivision : NEWBERRY BUSINESS PARK

THIS IS NOT a title report since no examination has been made of the title to the above-described property. Our search for apparent encumbrances was limited to our tract Indices and therefore does not include additional matters which might have been disclosed by an examination of the record title, such as pending litigation. The charge for this service will not include supplemental reports, rechecks or other services.

REPORT

A. The Land referred to in this report is located in the County of Deschutes, State of Oregon, and is described as follows:

As fully set forth on Exhibit "A" attached hereto and by this reference made a part hereof.

B. As of the Effective Date, the tax account and map references pertinent to the Land are as follows:

As fully set forth on Exhibit "A" attached hereto and by this reference made a part hereof.

C. As of the Effective Date and according to the Public Records, we find title to the land apparently vested in:

As fully set forth on Exhibit "B" attached hereto and by this reference made a part hereof

D. As of the Effective Date and according to the Public Records, the Land is subject to the following liens and encumbrances, which are not necessarily shown in the order of priority:

As fully set forth on Exhibit "C" attached hereto and by this reference made a part hereof.

First American Title Insurance Company Public Record Report for New Subdivision or Land Partition Order No. 7069-4275646

EXHIBIT "A" (Land Description Map Tax and Account)

Lot 4, NEWBERRY BUSINESS PARK, Deschutes County, Oregon.

Map No.: 221014AB00108 Tax Account No.: 205413 First American Title Insurance Company Public Record Report for New Subdivision or Land Partition Order No. 7069-4275646

EXHIBIT "B" (Vesting)

City of La Pine, an Oregon Municipal Corporation

EXHIBIT "C" (Liens and Encumbrances)

- 1. Subject property is under public ownership and is tax exempt. Any change in ownership before delivery of assessment roll may result in tax liability. Account No. 205413.
- 2. City liens, if any, of the City of La Pine.
- 3. Subject to rules and regulations, including the levies, liens, assessments, rights of way, and easements of LaPine Special Sewer District.
- 4. An easement reserved in a deed, including the terms and provisions thereof; Recorded: September 11, 1981 as Book 347, Page 396

From: The United States of America To: Deschutes County, Oregon

For: Easement and Right-of-way for ditches or canals and for road

crossing

Affects: Underlying land

5. Memorandum of Contract for Line Extension Agreement and the terms and conditions thereof:

Between: Midstate Electric Cooperative, Inc.
And: Oregon Pacific Industries, Inc.

Recording Information: February 03, 1989 as Book 178, Page 0962

6. Covenants, conditions, restrictions and/or easements; but deleting any covenant, condition or restriction indicating a preference, limitation or discrimination based on race, color, religion, sex, handicap, family status, or national origin to the extent such covenants, conditions or restrictions violate Title 42, Section 3604(c), of the United States Codes:

Recording Information: December 06, 1993 as Book 321, Page 1574

7. License (Improvement Agreement), including terms and provisions thereof.

Recorded: January 11, 2002 as Instrument No. 2002-1911

Rerecorded January 18, 2002 as Instrument No. 2002-3339

8. Covenants, conditions, restrictions and/or easements; but deleting any covenant, condition or restriction indicating a preference, limitation or discrimination based on race, color, religion, sex, handicap, family status, or national origin to the extent such covenants, conditions or restrictions violate Title 42, Section 3604(c), of the United States Codes:

Recording Information: February 12, 2002 as Instrument No. 2002-08641

Modification and/or amendment by instrument:

Recording Information: September 18, 2017 as Instrument No. 2017-37409

- 9. Regulations and Assessments of Newberry Business Park Owners Committee, as set forth in Declaration recorded February 12, 2002 as Instrument No. 2002-08641.
- 10. Public Utility Easement, including terms and provisions thereof.

Recorded: July 03, 2003 as Instrument No. 2003-45055

First American Title Insurance Company Public Record Report for New Subdivision or Land Partition Order No. 7069-4275646

11. Restrictive Covenant, including terms and provisions thereof.

Recorded: November 12, 2024 as Instrument No. 2024-29616

NOTE: Notice of Written Order, Deschutes County Forestland Classification Committee, recorded January 17, 2019 as Instrument No. 2019-01721, Deschutes County, Oregon.

DEFINITIONS, CONDITIONS AND STIPULATIONS

- 1. **Definitions.** The following terms have the stated meaning when used in this report:
 - (a) "Customer": The person or persons named or shown as the addressee of this report.
 - (b) "Effective Date": The effective date stated in this report.
 - (c) "Land": The land specifically described in this report and improvements affixed thereto which by law constitute real property.
 - (d) "Public Records": Those records which by the laws of the state of Oregon impart constructive notice of matters relating to the Land.

Liability of the Company.

- (a) THIS REPORT IS NOT AN INSURED PRODUCT OR SERVICE OR A REPRESENTATION OF THE CONDITION OF TITLE TO REAL PROPERTY. IT IS NOT AN ABSTRACT, LEGAL OPINION, OPINION OF TITLE, TITLE INSURANCE COMMITMENT OR PRELIMINARY REPORT, OR ANY FORM OF TITLE INSURANCE OR GUARANTY. THIS REPORT IS ISSUED EXCLUSIVELY FOR THE BENEFIT OF THE APPLICANT THEREFOR, AND MAY NOT BE USED OR RELIED UPON BY ANY OTHER PERSON. THIS REPORT MAY NOT BE REPRODUCED IN ANY MANNER WITHOUT FIRST AMERICAN'S PRIOR WRITTEN CONSENT. FIRST AMERICAN DOES NOT REPRESENT OR WARRANT THAT THE INFORMATION HEREIN IS COMPLETE OR FREE FROM ERROR, AND THE INFORMATION HEREIN IS PROVIDED WITHOUT ANY WARRANTIES OF ANY KIND, AS-IS, AND WITH ALL FAULTS. AS A MATERIAL PART OF THE CONSIDERATION GIVEN IN EXCHANGE FOR THE ISSUANCE OF THIS REPORT, RECIPIENT AGREES THAT FIRST AMERICAN'S SOLE LIABILITY FOR ANY LOSS OR DAMAGE CAUSED BY AN ERROR OR OMISSION DUE TO INACCURATE INFORMATION OR NEGLIGENCE IN PREPARING THIS REPORT SHALL BE LIMITED TO THE FEE CHARGED FOR THE REPORT. RECIPIENT ACCEPTS THIS REPORT WITH THIS LIMITATION AND AGREES THAT FIRST AMERICAN WOULD NOT HAVE ISSUED THIS REPORT BUT FOR THE LIMITATION OF LIABILITY DESCRIBED ABOVE. FIRST AMERICAN MAKES NO REPRESENTATION OR WARRANTY AS TO THE LEGALITY OR PROPRIETY OF RECIPIENT'S USE OF THE INFORMATION HEREIN.
- (b) No costs (including, without limitation attorney fees and other expenses) of defense, or prosecution of any action, is afforded to the Customer.
- (c) In any event, the Company assumes no liability for loss or damage by reason of the following:
 - (1) Taxes or assessments which are not shown as existing liens by the records of any taxing authority that levies taxes or assessments on real property or by the Public Records.
 - (2) Any facts, rights, interests or claims which are not shown by the Public Records but which could be ascertained by an inspection of the land or by making inquiry of persons in possession thereof.
 - (3) Easements, liens or encumbrances, or claims thereof, which are not shown by the Public Records.
 - (4) Discrepancies, encroachments, shortage in area, conflicts in boundary lines or any other facts which a survey would disclose.
 - (5) (i) Unpatented mining claims; (ii) reservations or exceptions in patents or in Acts authorizing the issuance thereof, (iii) water rights or claims or title to water.
 - (6) Any right, title, interest, estate or easement in land beyond the lines of the area specifically described or referred to in this report, or in abutting streets, roads, avenues, alleys, lanes, ways or waterways.
 - (7) Any law, ordinance or governmental regulation (including but not limited to building and zoning laws, ordinances or regulations) restricting, regulating, prohibiting or relating to (i) the occupancy, use or enjoyment on the land; (ii) the character, dimensions or location of any improvement now or hereafter erected on the land; (iii) a separation in ownership or a change in the dimensions or area of the land or any parcel of which the land is or was a part; or (iv) environmental protection, or the effect of any violation of these laws, ordinances or governmental regulations, except to the extent that a notice of the enforcement thereof or a notice of a defect, lien or encumbrance resulting from a violation or alleged violation affecting the land has been recorded in the Public Records at the effective date hereof.
 - (8) Any governmental police power not excluded by 2(d)(7) above, except to the extent that notice of the exercise thereof or a notice of a defect, lien or encumbrance resulting from a violation or alleged violation affecting the land has been recorded in the Public Records at the effective date hereof.
 - (9) Defects, liens, encumbrances, adverse claims or other matters created, suffered, assumed, agreed to or actually known by the Customer.
- 3. **Charge.** The charge for this report does not include supplemental reports, updates or other additional services of the Company.



Illegal Restrictive Covenants

Please be advised that any provision contained in this document, or in a document that is attached, linked, or referenced in this document, that under applicable law illegally discriminates against a class of individuals based upon personal characteristics such as race, color, religion, sex, sexual orientation, gender identity, familial status, disability, national origin, or any other legally protected class, is illegal and unenforceable by law.



June 19, 2025

City of La Pine Community Development 16345 Sixth Street La Pine, OR 97739

RE: Trip Generation Estimate for the La Pine Business Incubator 16628 Assembly Way (tax lot 22 10 14AB – 00108)

The proposed development will create a 6,100 square foot light industrial building on a 0.6-acre site. The building will be separated into four separate tenant lease spaces of approximately 1,525 square feet each. The proposed development is consistent with Land Use 110 "General Light Industrial" in the ITE Trip Generation Manual, 10th Edition.

| <u>Land Use 110</u> | General Light Industrial |
|---------------------|------------------------------------|
| Average Daily Trips | 4.96 per 1,000 SF Gross Floor Area |
| PM Peak Hour Trips | 0.83 per 1,000 SF Gross Floor Area |

The proposed building will generate an estimated 30 ADT and 5 PM peak hour trips.

(6.1 ksf) x (4.96 ADT / ksf) = 30 ADT (6.1 ksf) x (0.83 PM Peak Hour / ksf) = 5 PM Peak Hour Trips

Since the estimated ADT is less than 300, a Traffic Impact Analysis will not be required per City of La Pine Development Code 15.90.080.B.3.

Sincerely,

Grant Hardgrave, PE